



**WOODMONT COMMONS
PLANNED UNIT DEVELOPMENT
MASTER PLAN
SEPTEMBER 2013**

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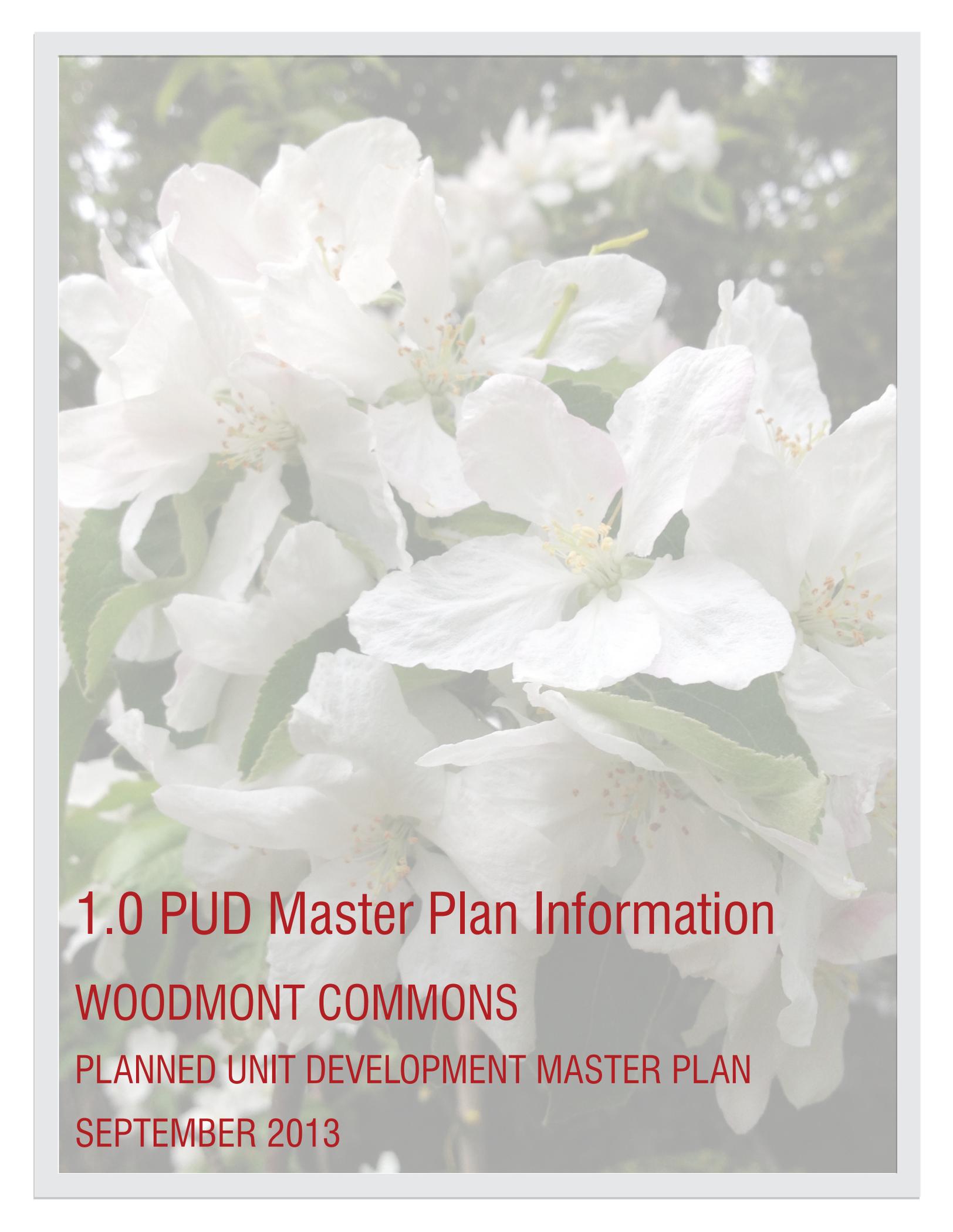
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1.0 PUD Master Plan Information

WOODMONT COMMONS

PLANNED UNIT DEVELOPMENT MASTER PLAN

SEPTEMBER 2013

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1.1 PURPOSE OF THE WOODMONT COMMONS PUD

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1.1.1 Intent of the Woodmont Commons PUD

The purpose of *Section 2.8 Planned Unit Development* of the Town of Londonderry, New Hampshire Zoning Ordinance (*Londonderry Zoning Ordinance*) is to create a process to allow owners to propose projects largely independent from current land use regulations.

The Woodmont Commons Planned Unit Development Master Plan (PUD Master Plan) was prepared in response to the purpose of the ordinance. Its conception, preparation and analysis has been integrated with the desires and aspirations of the community. Over the course of multiple years, and through dozens of meetings with citizens and public officials of the Town, a vision, a master plan and associated rules and regulations were crafted to accomplish the following goals:

- Accommodate development in an environmentally responsible manner;
- Create a development that will, upon completion, facilitate choice among citizens as to where and how they want to live, at the various stages of their lives and according to the resources available to them;
- Establish a framework for development that will deliver a walkable environment that comprises a variety of uses and building types;
- Develop in a manner that is in alignment with accepted standards and performance for public infrastructures;

- Allows for substantial commercial and mixed-use development to create a positive economic impact for the town;
- Create regulations and establish a process that allows flexibility in phasing and in the actual development plan.

1.1.2 PUD Planning Process

Woodmont Commons informally began its planning process in Spring, 2010 and included preliminary meetings with local and State officials and a preliminary design charrette.

A much larger and fully public week-long design charrette was held on the property in September, 2010. This charrette was led by the owners and the firm of Duany Plater-Zyberk & Company of Miami.

The first discussions with Londonderry Community Development Department staff and other Town staff pursuant to *Section 2.8.3.1.1* of the *Londonderry Zoning Ordinance* commenced in Spring 2010 and conceptual discussions with the Planning Board pursuant to *Section 2.8.3.1.2* of the *Londonderry Zoning Ordinance* soon followed.

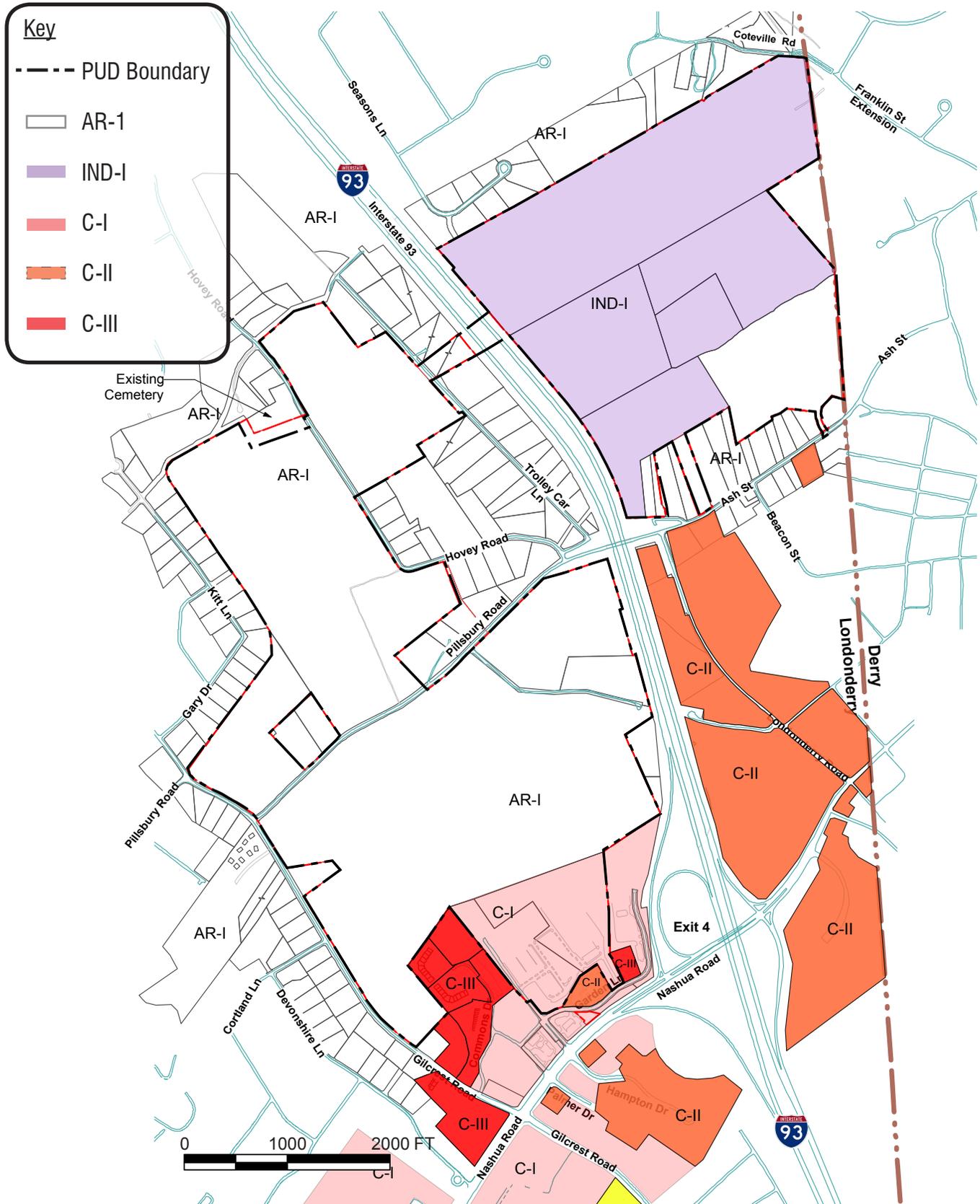
These conceptual discussions with the Planning Board continued into 2011, while at the same time Attorney John Michels held meetings at the site twice weekly for months. These on-site public meetings allowed discussions about the project in a less formal setting.

Over the course of several months, the concept of the Woodmont Commons PUD evolved and adapted to meet areas of concern and opportunities. Since this is the first PUD application to be submitted in Londonderry, new issues arose, which were discussed and addressed.

1.1.3 Location



1.1.4 Current Underlying Zoning



1.2 INFORMATION PLANS

1.2.1 Early Plans

As a component of the rezoning process prescribed in *Section 2.8 Planned Unit Development* of the *Londonderry Zoning Ordinance*, a series of concept plans were prepared during the planning process to demonstrate how projects may develop in accordance with the PUD Master Plan. Additional land was added to the project, but it remains approximately 600 acres on both sides of I-93.

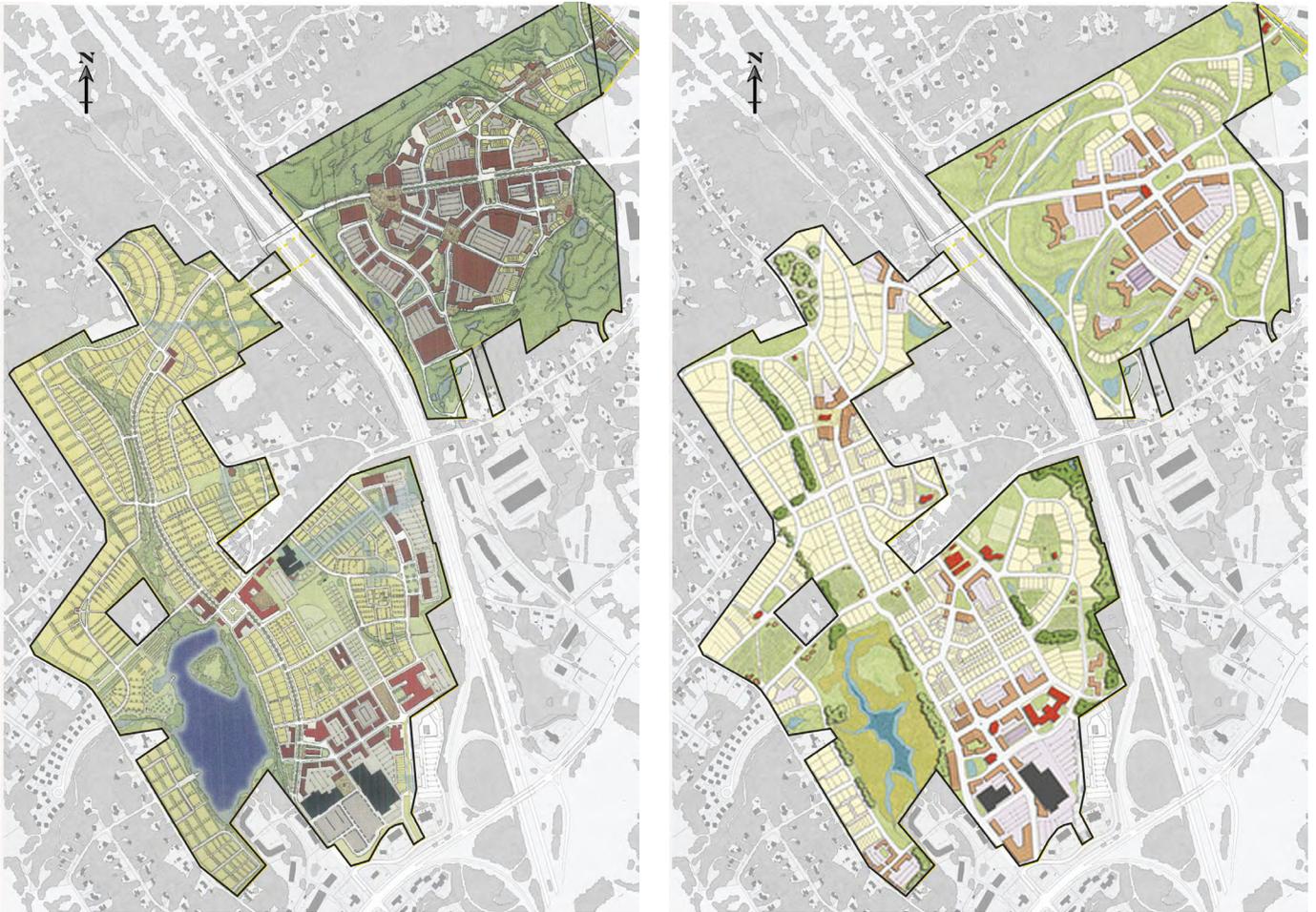
Conceptual discussions with the Planning Board developed the ideas of breaking the site down into Subareas and establishing ranges and limits of land uses around the PUD. The plans have evolved as a response to the comments received during these public discussions. The project submittal package was accepted by the Planning Board as a Complete Application in October, 2012.



(A) Examples of early plans from the charrette

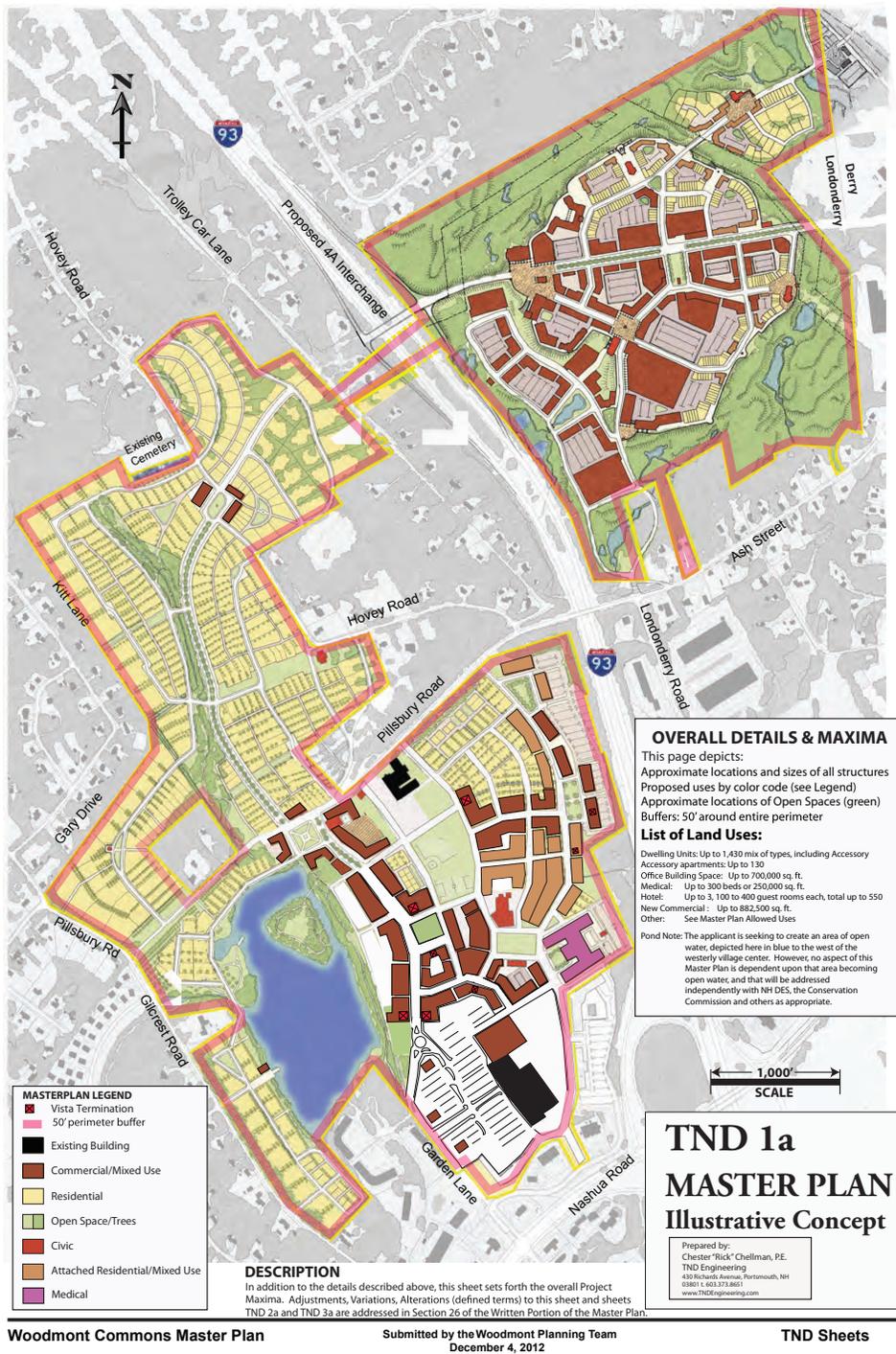
The charrette provided input for a number of plans that were incorporated into the plans submitted with the submission presented to the Planning Board in October, 2012.

1.2.1 Early Plans



(B) Examples of early plans from the charrette

1.2.1 Early Plans



Woodmont Commons Master Plan

Submitted by the Woodmont Planning Team
 December 4, 2012

TND Sheets

TND-1a

This was a concept plan from the revised project submittal package given to the Planning Board on October 3, 2012. The Planning Board accepted this document as a complete application in October, 2012.

1.2.1 Early Plans



Preliminary Versions, Information Plan

These two concept plans were created during the development of the Information Plan as part of the review process for the complete application.

1.2.2 Information Plan Overview

Overview of the Information Plan

The Information Plan in *Section 1.2.4 The Information Plan* represents four planning concepts:

- Understandings to accommodate abutters;
- Maximum yield profile for the development;
- A graphic example of the form of development required by the Master Plan;
- One of many possible development programs and therefore site plans that may occur in the future.

Information Plan by Subarea

The items shown on the Information Plan are presented as approximations or preliminary estimates subject to change. This includes the proposed general estimates of location, size, use(s) for each structure; the proposed general estimates of location and width of all streets, drives, sidewalks and paths; the proposed general estimates of location and number of spaces for each parking area; the proposed open space areas; and the natural and cultural resources proposed to be preserved. The Perimeter Buffers to adjoining property are shown on the Information Plan in *Section 1.2.4 The Information Plan* and in *Section 2.2.2 Land Use Plan*.

Open Space

The approach to Open Space and Conserved Green Space within the PUD Master Plan is based upon several key ideas:

- Perimeter Buffers – the Woodmont Commons PUD will include a Perimeter Buffer with landscaping to screen existing single-family residences from adjoining incompatible uses. Where uses on both sides are the same or are compatible, then the landscape treatment, site improvements and architecture may be planned to complement each other and enhance views to, from and across the sites.
- Preservation of apple trees – Three rows of apple trees, where existing, will remain along Gilcreast and the areas of Perimeter Buffer to preserve the existing rows as indicated on the Land Use Plan. When needed, apple trees will be replaced with apple trees, but using a staggered approach so that not all three rows are replaced at once. *Section 2.3.5 regarding PUD Perimeter Buffers* contains additional regulatory requirements for the preservation and maintenance of the apple trees.
- An enhanced agricultural impoundment as an open space feature – A large, natural and low-lying area exists near the southwest corner of the Woodmont Commons PUD in Subarea WC-3 that is an existing agricultural impoundment. To the extent that it may be approved by state and federal regulatory authorities, it is proposed that this area be conserved and become an enhanced open water impoundment with bordering open space and public access along its perimeter, connecting to local streets and paths. Should state and federal regulatory approval be denied, WC-3 may be conserved in a manner similar to the diagram on the next page;
- An accessible, ecological approach to major stormwater drainage features – Existing agricultural drainage areas on the west side of the Woodmont Commons PUD would be modified and enhanced to become a network of open spaces and stormwater drainage corridors that accomplish both environmental and public access goals. The drainage ways, which lead to the enhanced impoundment, would be designed to provide incremental levels of water quality improvement for stormwater before it enters the feature. The corridors would also provide pedestrian and bicycle connections for the community at the perimeter of the landscaped space;
- Conserved Green Space in WC-12 – Conserved Green Space will be provided in Subarea WC-

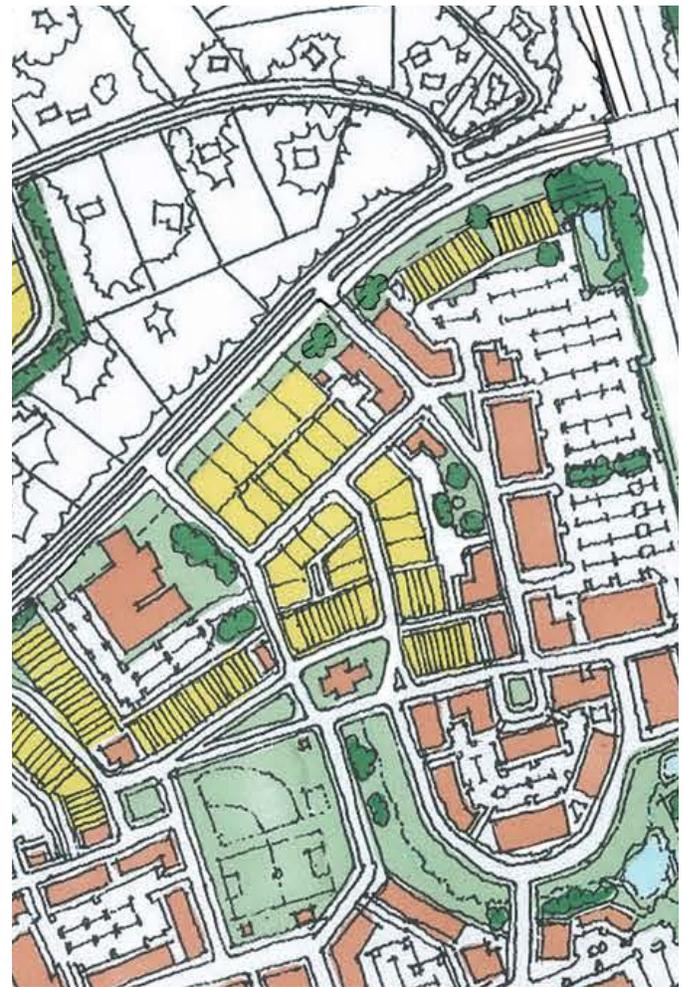
1.2.2 Information Plan Overview

12 and connected by a Perimeter Buffer along residential edges of the PUD boundary. This buffer will serve as links to existing natural corridors that reach to the borders of the Woodmont Commons PUD. It is proposed that this Conserved Green Space would take advantage of wetland preservation or enhancements that emerge from the state and federal review and approval processes;



Alternate Information Plan

If the proposed pond in WC-3 is not approved by State or Federal authorities having jurisdiction, then one possible alternative is shown above.



Alternate Information Plan

This alternate information plan provides Single-Family Attached houses in WC-2 along Pillsbury Road instead of commercial uses.

1.2.2 Information Plan Overview

- Provision and distribution of publicly accessible open space – Publicly accessible space must be connected to a street, sidewalk network or walking path. A variety of different types of Conserved Green Space and/or Shared Open Space would be created within or directly accessible to the development and neighborhood fabric.
- Traffic study updates to track the effect of background development unrelated to development within the Woodmont Commons PUD and other changing conditions may be required at the time of PUD Subdivision and/or PUD Site Plan submittal. These updates will vary in scope depending on the scale of the development;

Transportation

The Woodmont Commons PUD will be developed incrementally over a number of years. As a result, transportation accommodation will be required that matches the scale, type and pace of development in order to create a coordinated infrastructure that can safely and efficiently respond to the automotive trips generated by the development within the Woodmont Commons PUD.

In crafting the PUD Master Plan, several development scenarios, conceptualized mitigation areas, and transportation demand management measures were evaluated as options for enhancing the operating characteristics of the roadways and intersections and reducing the number of single-occupancy vehicle trips. The approach to transportation is founded on the following major principles and considerations:

- The PUD will generate some vehicle trips that will remain within the boundaries of the project (internal trip capture). Although the actual trend is expected to be higher, the conservative analysis prepared for the PUD Master Plan is based upon an estimated internal capture rate of 23%. This capture effect will be realized over time as a mixture of land uses are built and interconnected within the PUD;
- Traffic operations were analyzed over a 20-year horizon within the PUD Master Plan. The options within the model for future improvements account for an at-buildout condition during the traditional weekday morning and evening peak periods;

- The development potential within WC-12 will expand if the New Hampshire Department of Transportation (NHDOT) constructs Exit 4A and the ancillary exit-related infrastructure. Without Exit 4A, the potential to develop certain land uses within WC-12 will be scaled appropriately to preserve an acceptable level of service;
- The planned roadway and intersection improvements will be coordinated with the Town of Londonderry and, where applicable, NHDOT. The conceptual designs contemplate several elements from the *Route 102 Corridor Study*, which was prepared for the Town of Londonderry by the Southern New Hampshire Planning Commission (SNHPC) in 2002. In some cases the physical improvements will require participation from the Town of Londonderry to secure rights-of-way and/or other legal interests for temporary and permanent impacts to adjacent land.

1.2.3 Interpretation

The Information Plan reflects one of many possible plans that can be prepared that conform to the rules and regulations of this PUD Master Plan. The example illustrations as shown within *Section 1.0 PUD Master Plan Information* depict a few of other possible approaches that may reflect future PUD Subdivision and PUD Site Plans that also conform to the PUD Master Plan.

1.2.4 The Information Plan

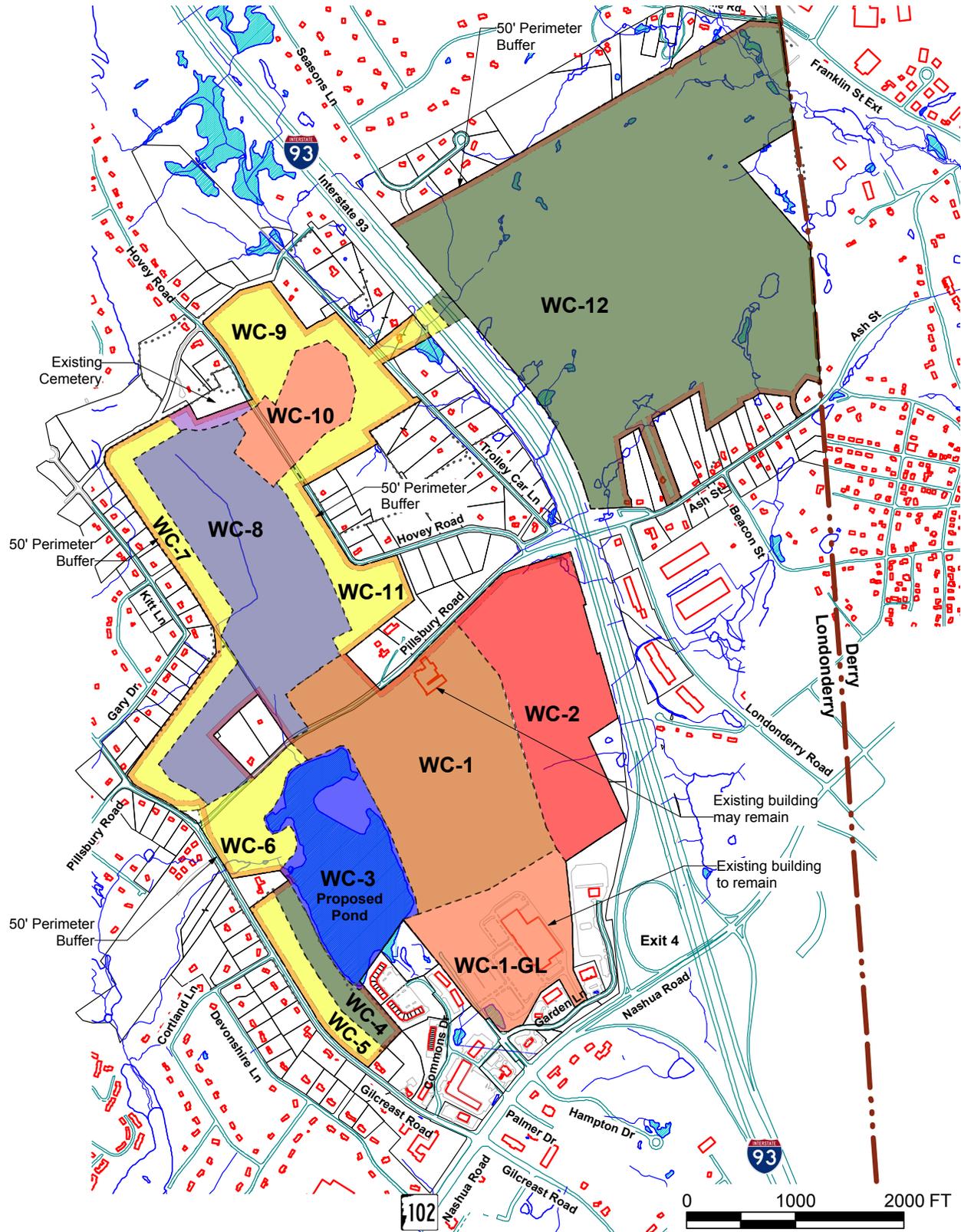
Illustration only. Refer to *Section 1.2.3 Interpretation*.



INFORMATION PLAN

1.2.4 The Information Plan

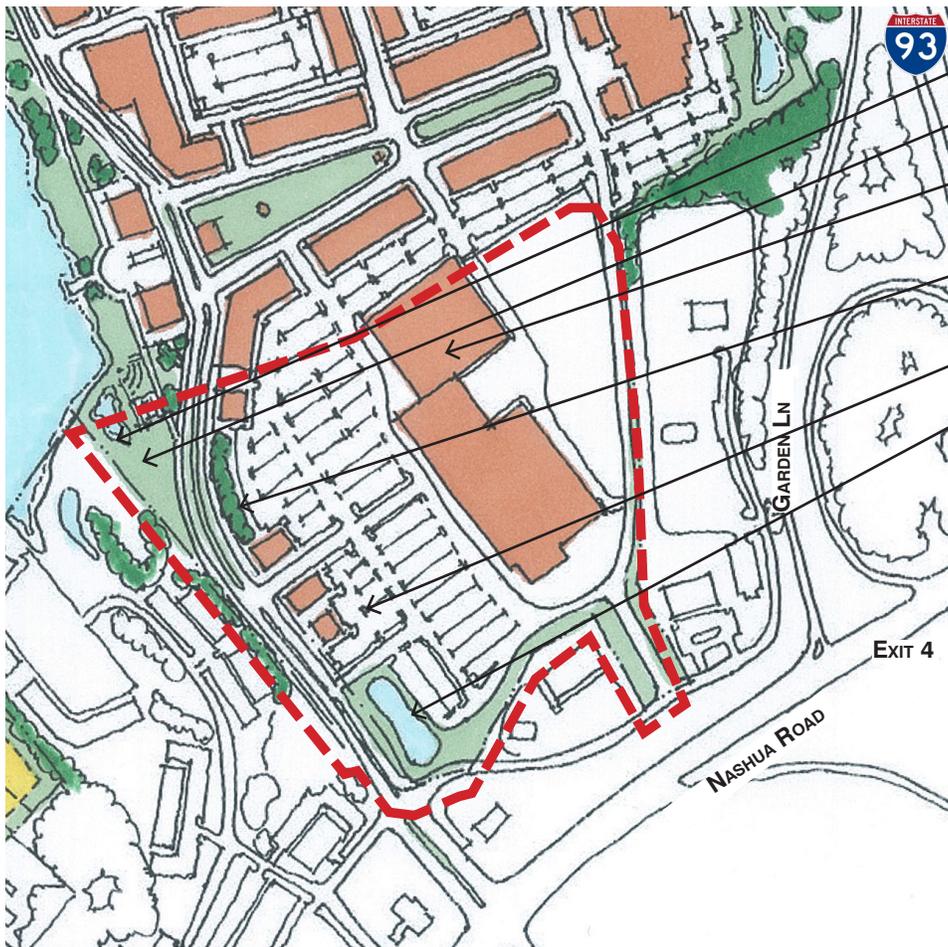
Outline of each Subarea.



SUBAREA PLAN

1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



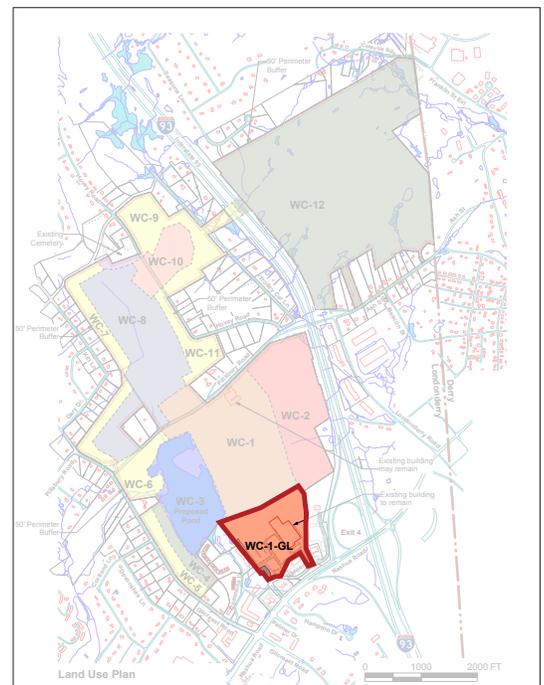
INFORMATION PLAN COMPONENTS

- Possible detention area
- Shared Open Space – Passive – Park
- New commercial uses with parking in front and rear for sites at the street
- New commercial uses or landscaping along entrance road
- Existing parking
- Conserved Green Space – with existing detention

SUBAREA WC-1-GL

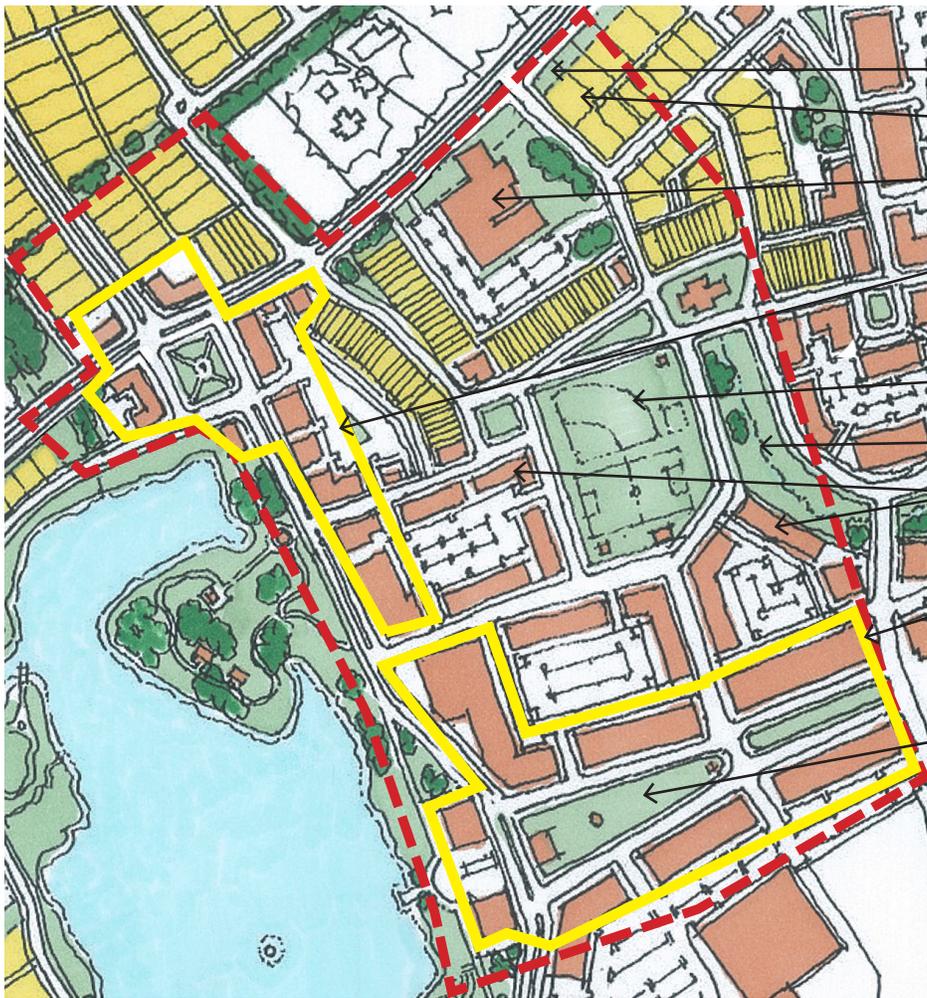
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--- Subarea Line



1.2.4 The Information Plan

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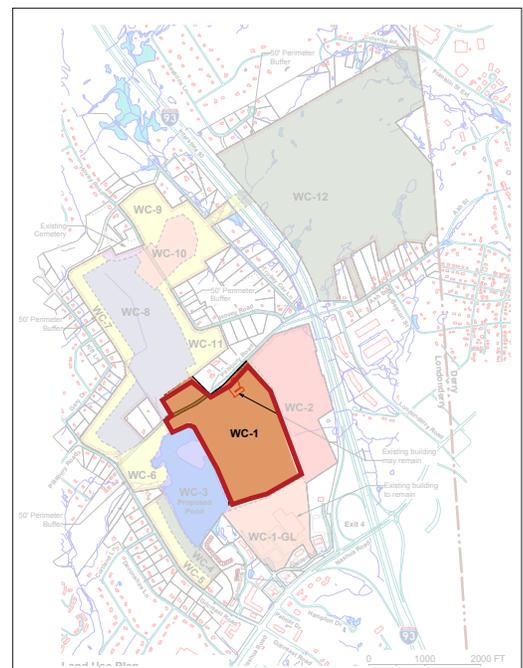
INFORMATION PLAN COMPONENTS

- Perimeter Buffer
- Residential uses along Pillsbury Road
- Existing building may be preserved and reused
- Commercial uses transition in scale to smaller building footprints as they approach the edge of the Subarea
- Shared Open Space – Active
- Conserved Green Space
- Multifamily uses with potential ground floor commercial uses near the Village Center
- Village Center core area with commercial first floors and possible mix of uses above and integrated into the plan
- Shared Open Space – Passive – Green

SUBAREA WC-1

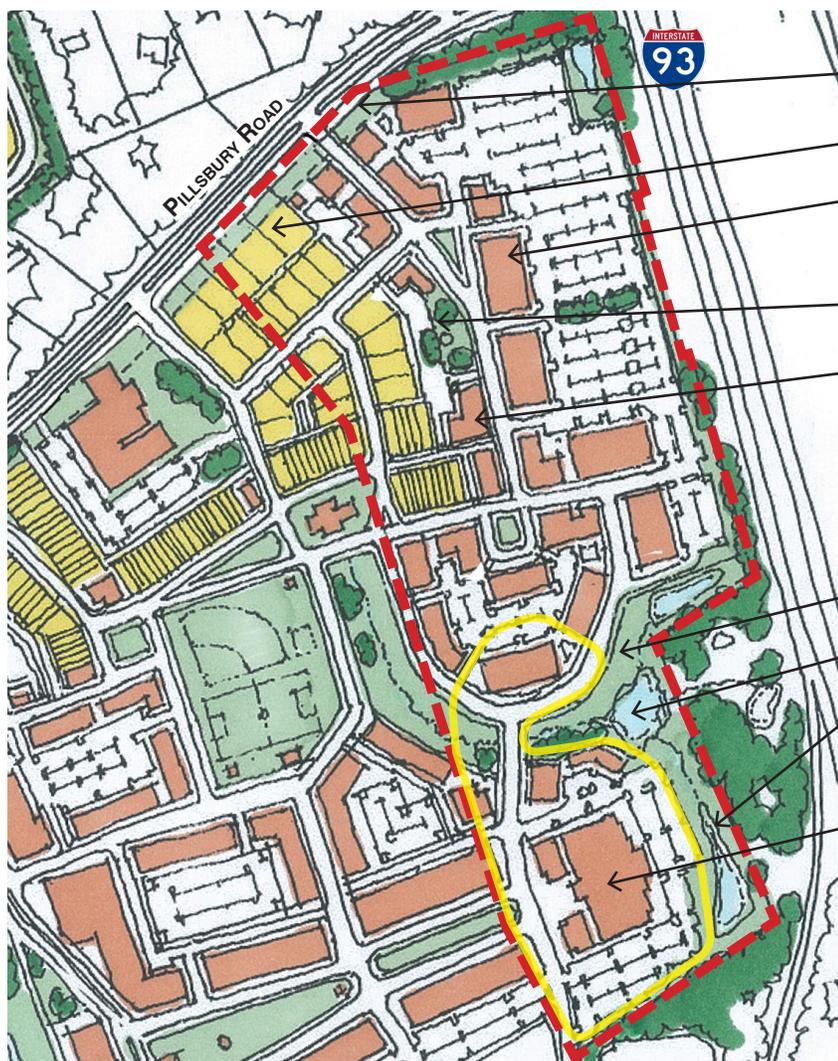
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Subarea Line



1.2.4 The Information Plan

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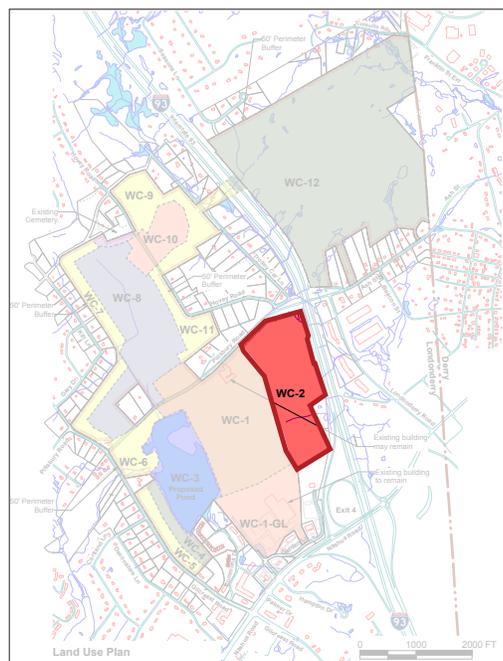
INFORMATION PLAN COMPONENTS

- Perimeter Buffer
- Residential uses along Pillsbury Road
- Dedicated Office Building zone along I-93 corridor; note that other uses are permitted
- Shared Open Space – Passive
- Commercial, Accommodation, Civic and Residential uses transition in scale to smaller building footprints as they approach the edge of the Subarea where adjacent Subarea has similarly scaled uses
- Conserved Green Space
- Detention areas
- Village Center core area with possible large commercial anchor; note that other uses are permitted

SUBAREA WC-2

LEGEND

----- Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



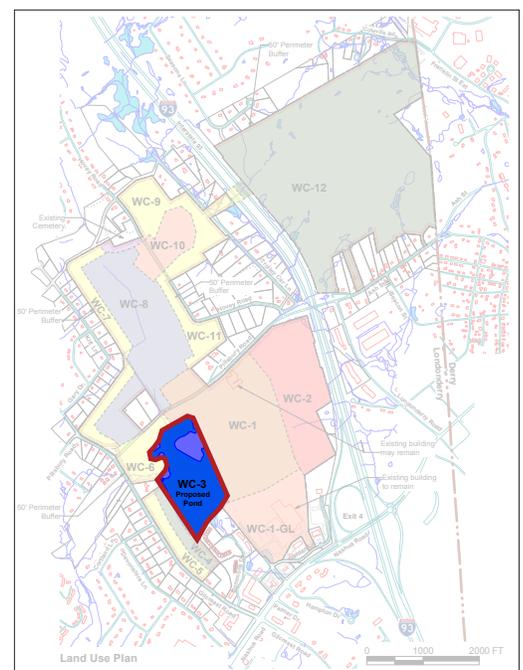
INFORMATION PLAN COMPONENTS

- Trail system
- Shared Open Space – Passive uses
- Trail system integrated with sidewalk at street
- Conserved Green Space – may include Active or Passive Shared Open Space with implementation of proposed pond, detention area
- Sidewalk or trail at perimeter of proposed pond

SUBAREA WC-3

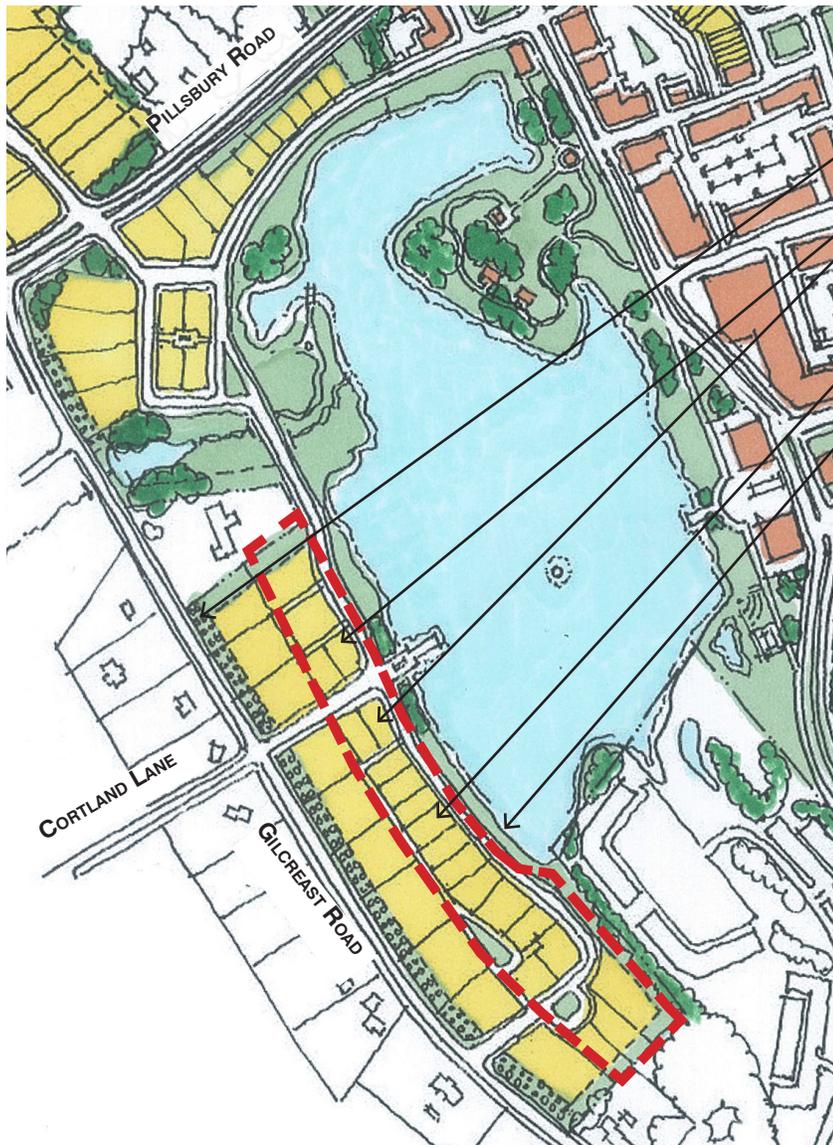
LEGEND

— Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



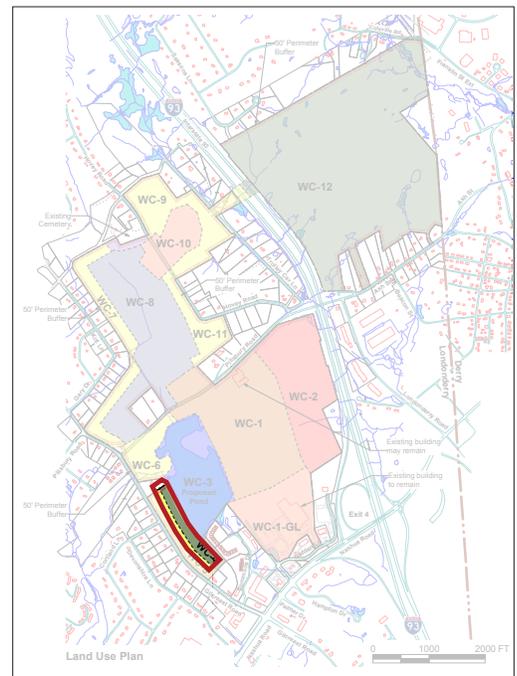
INFORMATION PLAN COMPONENTS

- Perimeter Buffer
- Residential uses or limited Civic, Accommodation or Commercial uses at connector street to Gilcrest Road
- Residential uses
- Sidewalk or trail at perimeter of proposed pond

SUBAREA WC-4

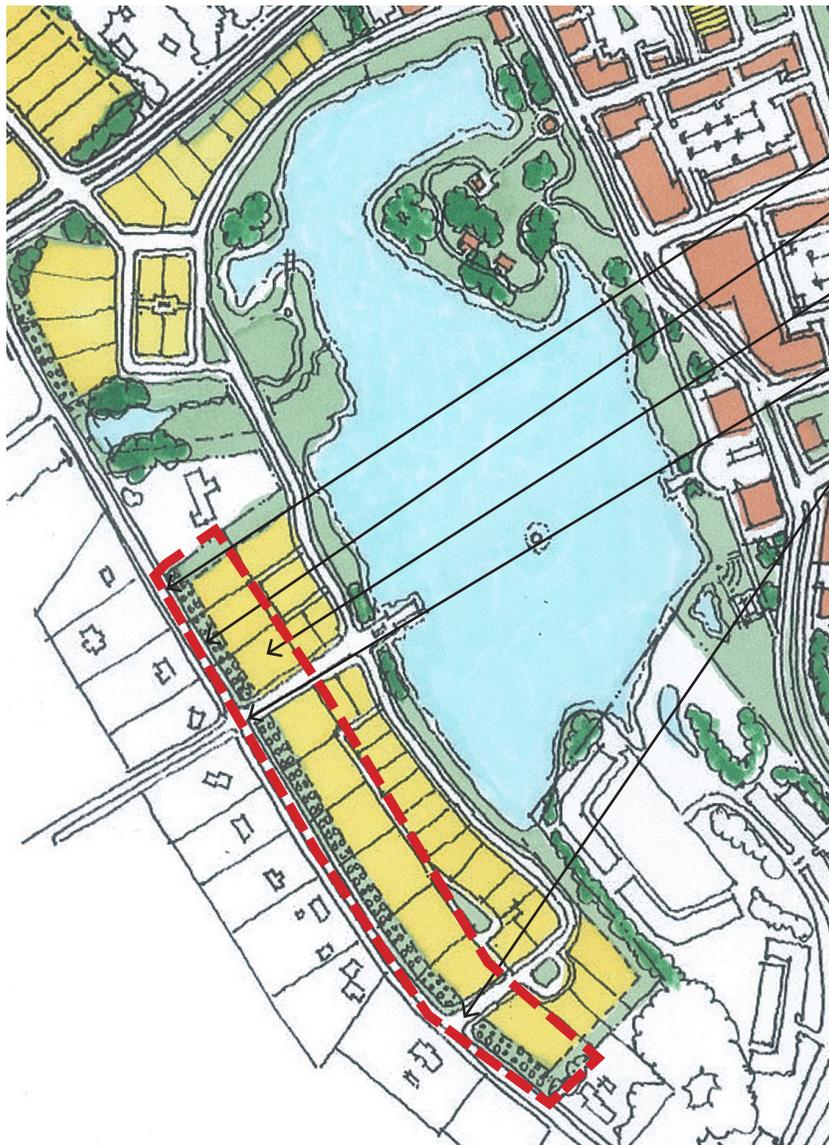
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1.2.4 The Information Plan

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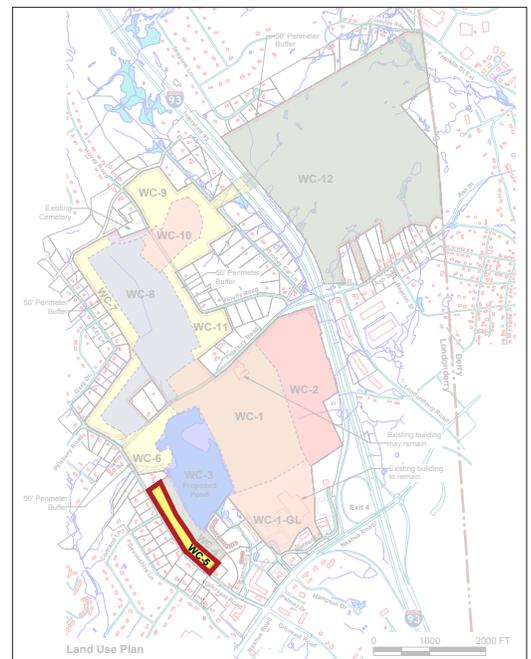
INFORMATION PLAN COMPONENTS

- Perimeter Buffer
- Perimeter Buffer with orchard-like planting
- Residential uses similar to abutting residential uses
- Possible connection to Gilcrest Road

SUBAREA WC-5

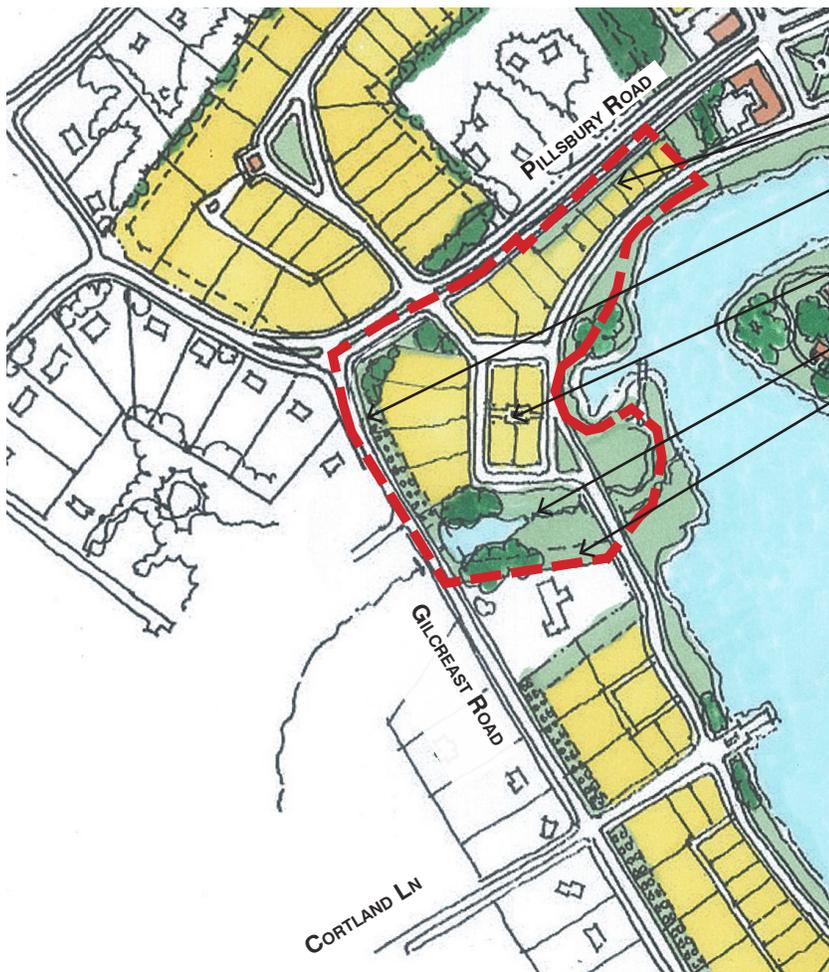
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 Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



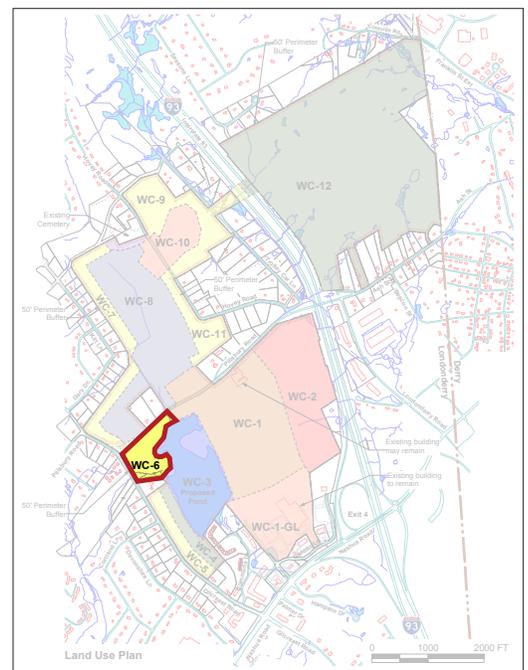
INFORMATION PLAN COMPONENTS

- Perimeter Buffer
- Perimeter Buffer with orchard like planting
- Residential or other uses
- Conserved Green Space
- Perimeter Buffer

SUBAREA WC-6

LEGEND

--- Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



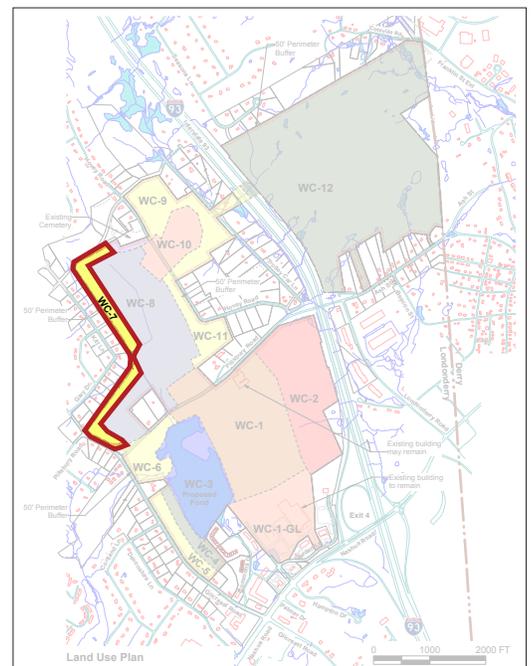
INFORMATION PLAN COMPONENTS

- Perimeter Buffer
- Residential uses similar to abutting residential uses
- Possible trail connection
- Conserved Green Space

SUBAREA WC-7

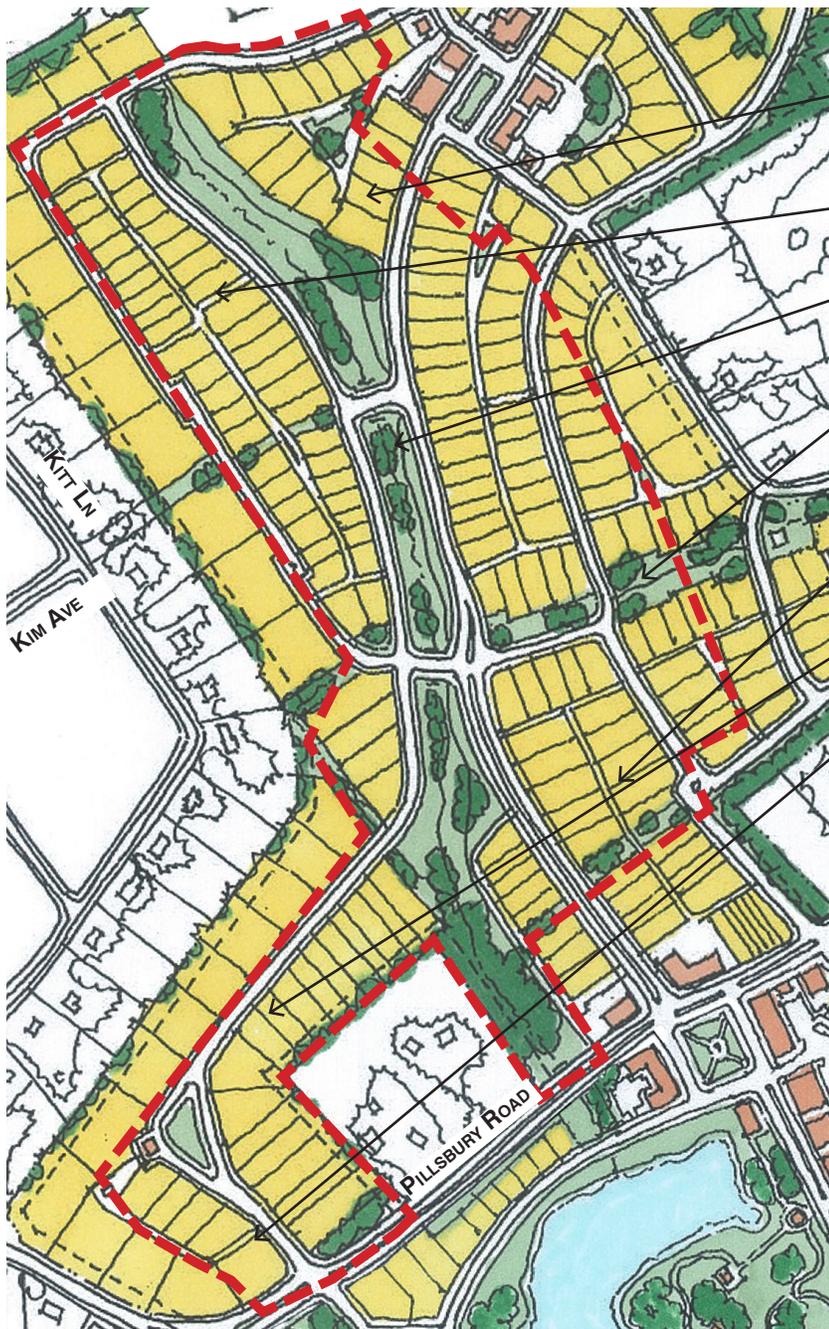
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--- Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



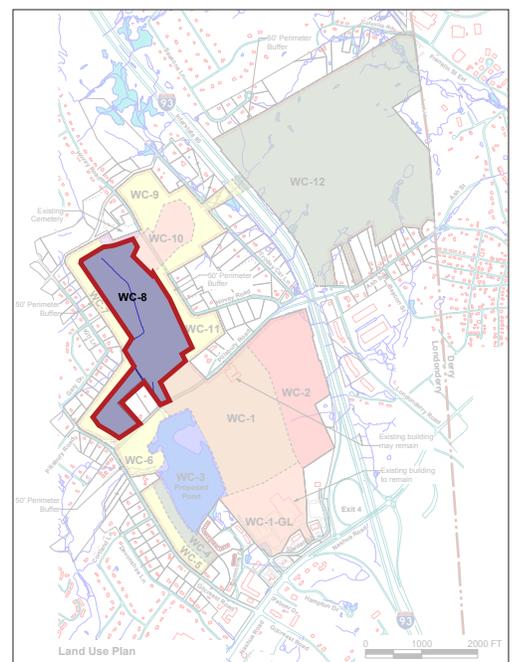
INFORMATION PLAN COMPONENTS

- Residential or other small neighborhood service type uses approaching Subarea WC-10
- Residential uses
- Conserved Green Space with trail system
- Shared Open Space – Passive with a trail system connecting to Conserved Green Space
- Residential or other small neighborhood service type uses approaching Subarea WC-1
- Residential uses
- Residential or other small neighborhood service type uses approaching Subarea WC-6

SUBAREA WC-8

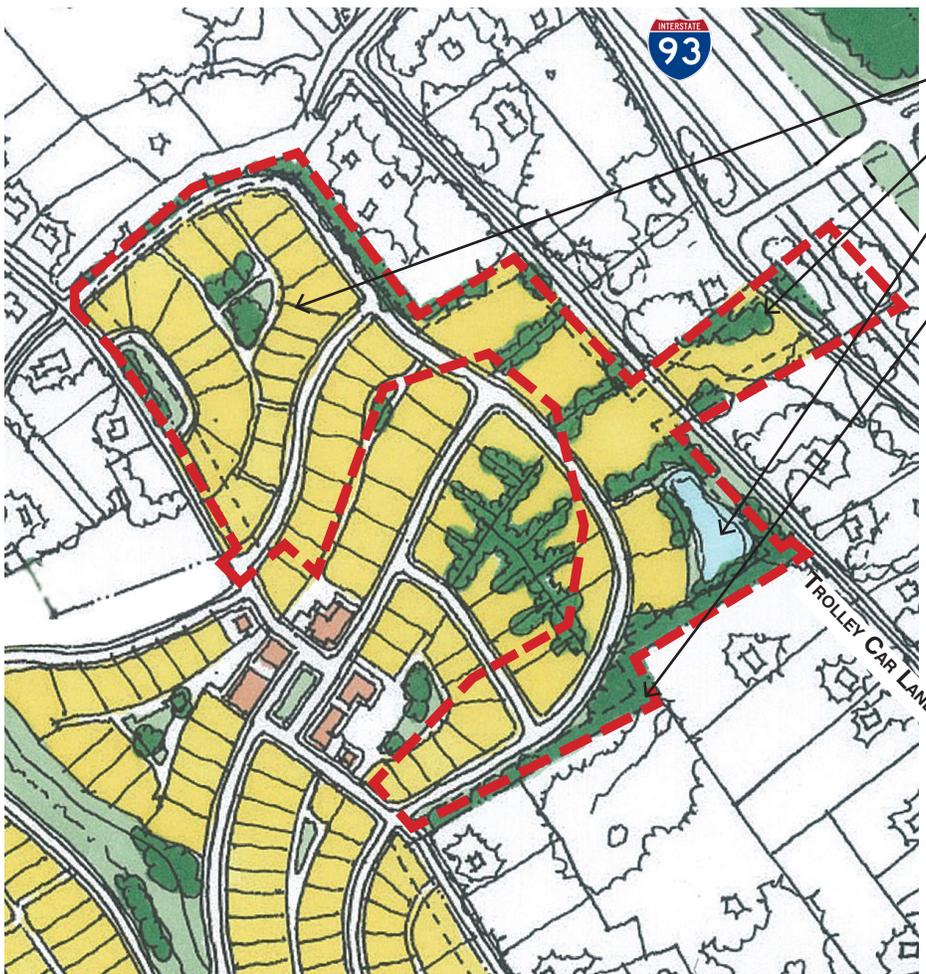
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Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



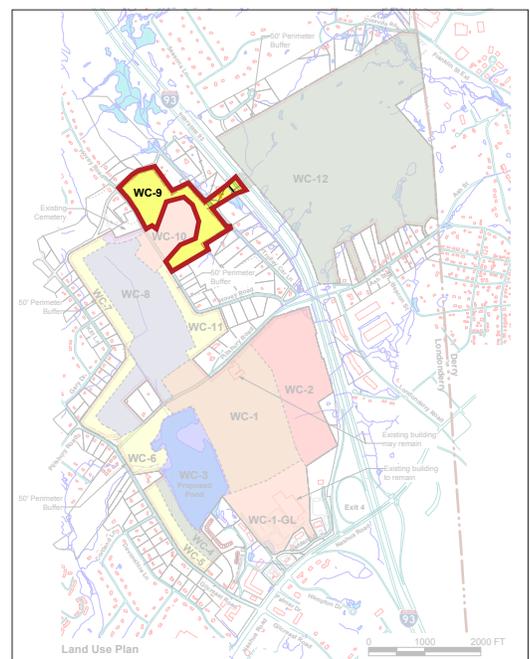
INFORMATION PLAN COMPONENTS

- Residential uses
- Conserved Green Space
- Conserved Green Space with possible detention area
- Perimeter Buffer

SUBAREA WC-9

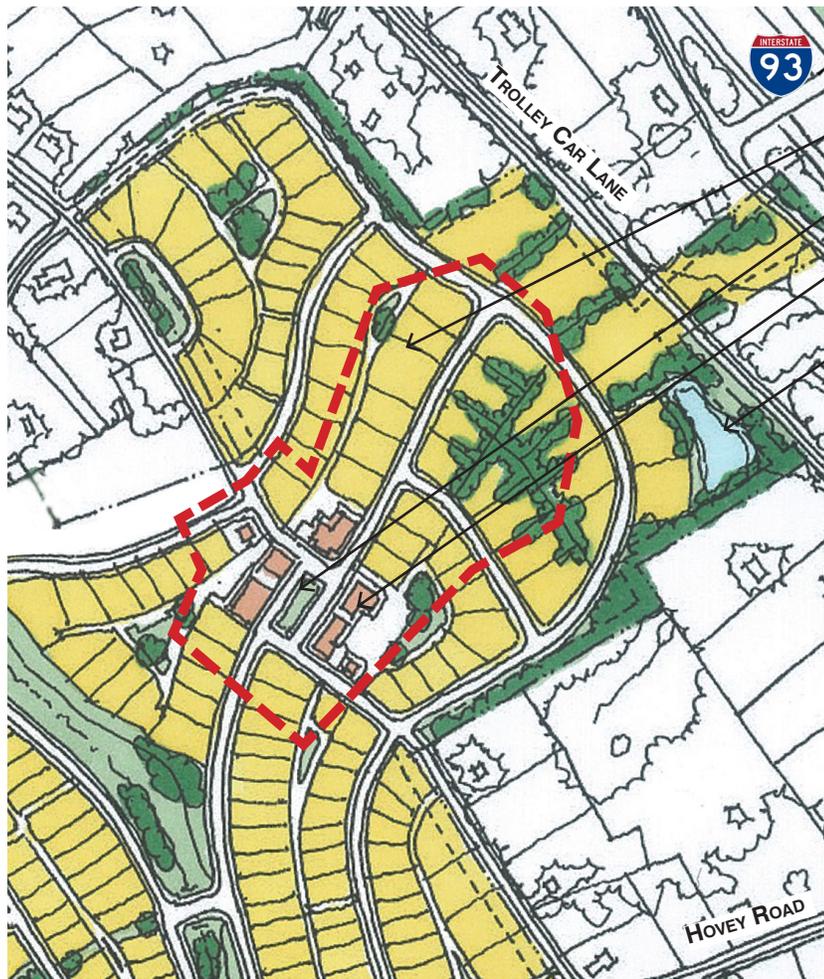
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- - - - - Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



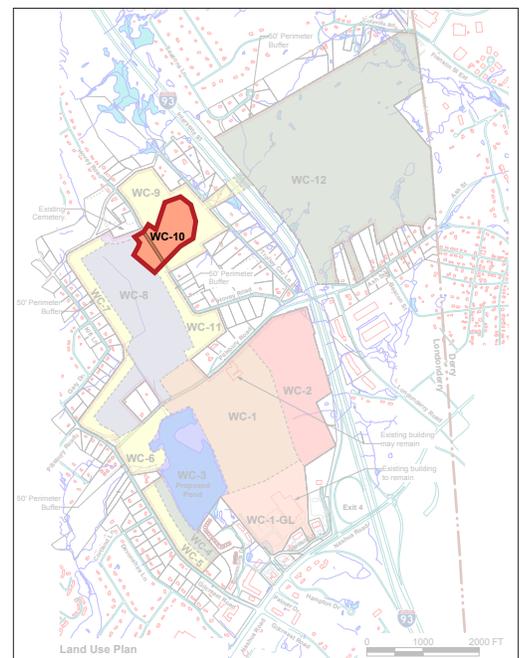
INFORMATION PLAN COMPONENTS

- Residential uses
- Shared Open Space – Passive – Square
- Commercial uses, small, neighborhood services type uses
- Possible detention area

SUBAREA WC-10

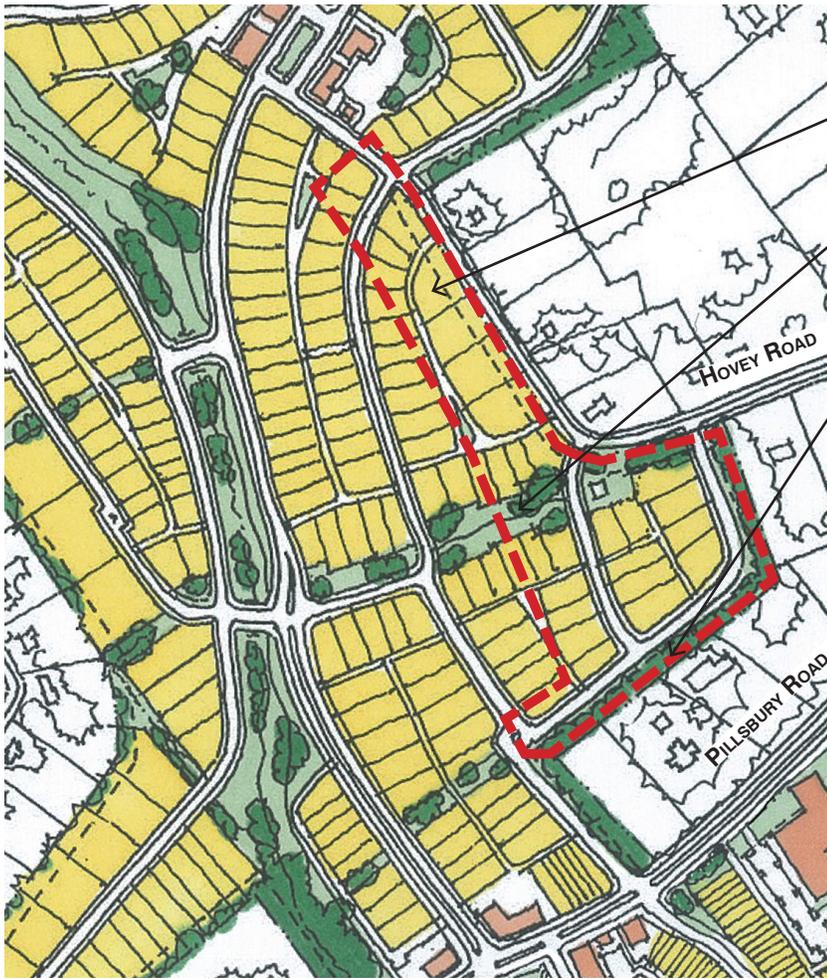
LEGEND

----- Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to *Section 1.2.3 Interpretation*.



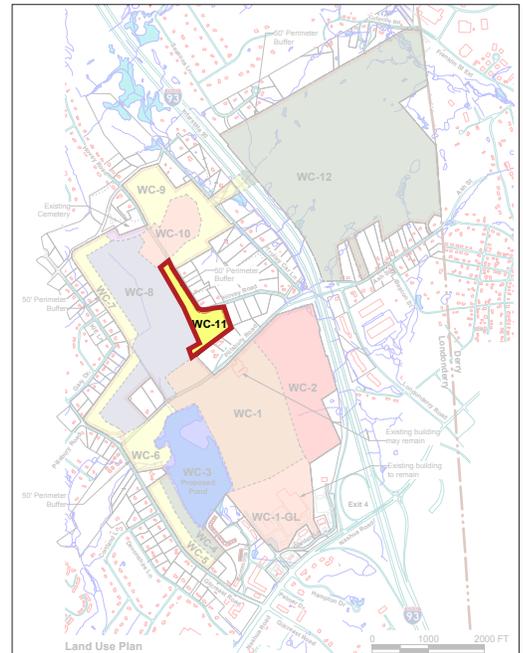
INFORMATION PLAN COMPONENTS

- Residential uses
- Shared Open Space – Passive – Green with trail connection to WC-8
- Perimeter Buffer

SUBAREA WC-11

LEGEND

 Subarea Line



1.2.4 The Information Plan

Illustration only. Refer to Section 1.2.3 Interpretation.



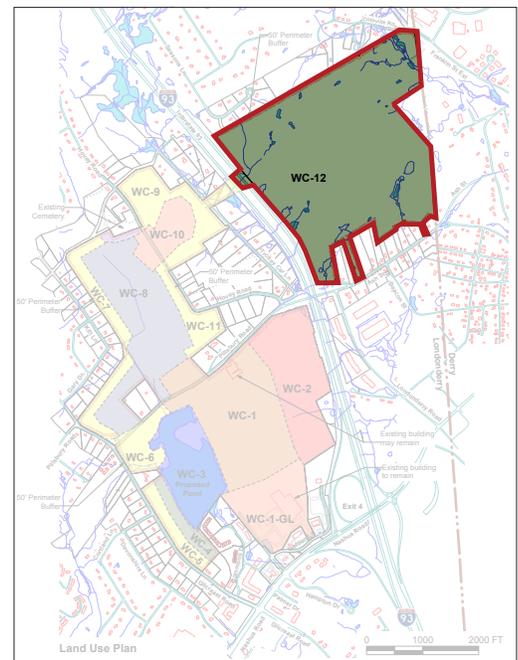
INFORMATION PLAN COMPONENTS

- Perimeter Buffer
- Exit 4A
- Residential uses
- Possible trail connection
- Mix of uses including Commercial uses with a regional draw, larger format retail and dedicated office uses
- Shared Open Space – Passive – Square
- Shared Open Space – Passive – Park with connected trail system
- Conserved Green Space
- Possible Institutional or Office use
- Possible detention area

SUBAREA WC-12

LEGEND

Subarea Line



1.3 OWNERSHIP ARRANGEMENT

1.3.1 Current Ownership

The approximately 600 acres of land within the Woodmont Commons PUD is owned by the following: Pillsbury Realty Development, LLC (Pillsbury or “Developer”), Hyrax Derry Partners, LLC, Demoulas Supermarkets, Inc., and Robert D. and Stephen R. Lievens. Pillsbury, on behalf of these Land Owners, serves as the primary applicant and principal liaison to the Town of Londonderry with respect to planning and land use approvals. Pillsbury will likewise serve as the initial Developer on behalf of the Land Owners, presiding over internal development controls and density accounting, conveyances of land subject to approved restrictive covenants, and the Developer’s collective obligations pursuant to the Woodmont Commons Development Agreement.

1.3.2 Ownership Organization

The Land Owners are obligated to each other through a series of written and informal joint venture agreements. The Developer is obligated to the Town of Londonderry through the Woodmont Commons Development Agreement. The Developer is obligated to their grantees, assignees, licensees and other future development entities (collectively, the “Future Site Developers”) through the PUD Master Plan.

1.3.3 Covenants, Restrictions and Easements

Prior to the Land Owners’ conveyance of any property within the Woodmont Commons PUD and subsequent to the approval of the PUD Master Plan, the Land Owners shall each record in the Rockingham County Registry of Deeds a Declaration of Restrictive Covenants (hereinafter, collectively, “Declarations”), duly executed by the record owner or owners of each

parcel included within the Woodmont Commons PUD, stating that the land is within the zoning jurisdiction of the Town of Londonderry and is zoned as a Planned Unit Development pursuant to the terms of the PUD Master Plan.

Said Declarations shall further state that the land and use of the land is subject to regulations applicable to the Woodmont Commons PUD as stated in the PUD Master Plan, a copy of which is available for public inspection at the Planning Department of the Town of Londonderry (“Planning Department”) at the Londonderry Town Hall, 268B Mammoth Road, Londonderry, New Hampshire 03053.

In addition, Declarations shall specify that: (i) each deed conveying land within Woodmont Commons recorded subsequent to the recordation of the Declarations shall state that the respective conveyance is subject to the Declarations, but that the failure to include such statement, or to include a sufficiently suitable statement in a deed shall not affect the enforceability of the Declarations with respect to the property conveyed or invalidate the respective conveyance; and (ii) no projects, rezoning petition, PUD Master Plan adjustment, variation, alteration or amendment, application for variance, special exception or conditional use approval shall be submitted to the Town, or its land use boards, or proposed in any manner, without the written consent of the Developer, which shall not be unreasonably withheld.

A form of restrictive covenant, to be executed and recorded by each of the Land Owners, is included in *Section 4.6 Sample Restrictive Covenant*.

1.3.4 Master Property Owners’ Association

The Developer, the Land Owners and Future Site Developers are subject to the terms and conditions of the PUD Master Plan. No master property owners’ association is planned at this time. However, one or more associations may be created in the future.

1.4 ORGANIZATION OF THE PUD MASTER PLAN DOCUMENTS

1.4.1 Documents and Relationships

The PUD Master Plan is composed of the sections listed in the Table of Contents on page 3. These sections serve a variety of purposes and have specific relationships to the Town's land use and development ordinances and regulations. There is also a specific relationship between components of the PUD Master Plan and the Woodmont Commons Development Agreement (Development Agreement). The assembled documents in the PUD Master Plan update and replace, thereby superseding, components of the previously completed Woodmont Commons PUD Master Plan application to the Planning Board (October 3, 2012).

Londonderry Zoning Ordinance

The PUD Master Plan has been established in compliance with the *Londonderry Zoning Ordinance* and its provisions for a special approach to land use management, as defined by the *Londonderry Zoning Ordinance Section 2.8 Planned Unit Development*. As stated in the PUD Ordinance, alternative regulatory standards can be substituted for typical standards if those standards are specifically waived and new provisions provided in the PUD Master Plan are approved by the Planning Board. The *Londonderry Zoning Ordinance* describes:

“In devising the PUD Master Plan, subject to specific limitations, guidelines and objectives stated elsewhere in this ordinance, there is flexibility in the selection of land uses, density, setbacks, buffers, building heights, lot sizes, lot dimensions, parking and road design requirements, and most site design and development standards contained in the *Londonderry Zoning Ordinance*, *Site Plan Regulations*, and *Subdivision Regulations*.” (Section 2.8.3.1)

The PUD Master Plan has been developed in accordance with these provisions.

The PUD Master Plan contains a number of alternative Zoning provisions and associated waivers. The alternative Zoning provisions are contained in various portions of *Sections 2.0, 3.0 and 4.0*, and the Zoning waivers are listed in *Section 2.1 Introduction*. Existing typical Zoning standards may still apply to future projects, if those standards have not been waived in the approved PUD Master Plan. In such instances, the *Londonderry Zoning Ordinance* provisions in effect on the date of the completed the Woodmont Commons PUD Master Plan application (October 3, 2012) will be applicable. Otherwise, the PUD Master Plan shall be the governing document for all development within the Woodmont Commons PUD.

Section 2.8 Planned Unit Development of The *Londonderry Zoning Ordinance* lists a series of formal submittal requirements and information that must be provided to the Planning Board for their review prior to an approval, but which do not become part of the alternative standards and regulations that will be applied to the Woodmont Commons PUD. Components satisfying formal submittal requirements are included *Sections 2.0 and 3.0*. Other components that provide information and technical evaluations associated with the Planning Board's review and approval process as required by the *Londonderry Zoning Ordinance* are included in this Section and *Section 4.0 Supplemental Documents*.

PUD Master Plan Information

The planning process leading to Planning Board approval of a PUD Master Plan requires the preparation and submittal of information to assist staff and third parties in the review and evaluation of the Woodmont Commons proposal before it is adopted as a PUD Master Plan. The *Londonderry Zoning Ordinance* recognizes that “Many of these items may be presented as approximations or preliminary estimates subject to

1.4.1 Documents and Relationships

change, where appropriate.” (Section 2.8.9.2). Section 1.0 PUD Master Plan Information includes some of the elements required by Section 2.8 Planned Unit Development of the Londonderry Zoning Ordinance. Specifically, this consists of these portions of the required submittals:

- Existing underlying zoning classification and classification of all adjoining lots (*Londonderry Zoning Ordinance Section 2.8.9.2.1*);
- Total acreage of the tract (*Londonderry Zoning Ordinance Section 2.8.9.2.3*).

Some of the required elements have been incorporated into the alternative regulations and standards for the PUD Master Plan, and are contained in Section 2.0 PUD Regulations and Standards. These components specifically include:

- Proposed total number of dwelling units and overall density for the tract;
- Proposed Conserved Green Space and Shared Open Space areas;
- Natural and cultural resources to be preserved;
- Proposed Perimeter Buffers, if appropriate, to adjoining property.

All other required elements have been prepared as approximations or preliminary estimates and are subject to change, and are not part of the regulations or standards that will be applied to the projects within the Woodmont Commons PUD. The PUD Master Plan documents providing this type of information are contained in Section 1.0 PUD Master Plan Information and Section 4.0 Supplemental Documents.

Thresholds, Mitigation and Improvement Requirements

In order to provide a properly planned development, projects may be eligible for Planning Board approval if they are within defined thresholds, provide appropriate mitigation of potential impacts, or are accompa-

nied by certain improvements. The PUD Master Plan incorporates such standards within the special regulatory framework for the Woodmont Commons PUD. All thresholds, mitigation and improvement requirements that will be integral to Planning Board project reviews and approvals are addressed and incorporated into Section 2.0 PUD Regulations and Standards and Section 3.0 Mitigation and Improvement Requirements.

PUD Regulations and Standards

The superseding regulations and standards that provide unique guidance for projects within the Woodmont Commons PUD have been assembled within a consolidated and coordinated portion of the PUD Master Plan, Section 2.0 PUD Regulations and Standards. Once approved, this Section will serve as the superseding regulatory framework for proposals requiring zoning approvals, PUD Subdivision approvals, or PUD Site Plan approvals within the Woodmont Commons PUD. This Section also describes the special administrative processes that will be used to ensure conformance of future projects with the PUD Master Plan.

PUD Subdivision Regulations

The *Londonderry Subdivision Regulations* provide for appropriate subdivision of land into individual parcels that are adequately served by streets or other infrastructure components. However, alternative subdivision regulations can be established within a PUD Master Plan that includes waivers of portions of the corresponding *Londonderry Subdivision Regulations*. The PUD Master Plan provides a number of special regulations and standards that will allow innovative subdivision of the land, streets and infrastructure; these alternative PUD Subdivision Regulations and Standards are contained in Section 2.3 PUD Subdivision Regulations and Standards. The specific list of Subdivision waivers is contained in Section 2.1 Introduction. If an existing provision of the *Londonderry*

1.4.1 Documents and Relationships

Subdivision Regulations is not waived in the PUD Master Plan, then future proposals will need to comply with the applicable provisions in effect on the date of the Woodmont Commons PUD Master Plan application (October 3, 2012) which was deemed complete by the Londonderry Planning Board.

PUD Site Plan Regulations

Multi-family and non-residential projects in Londonderry are subject to the *Londonderry Site Plan Regulations* and a related review and approval process. Similar to the special provisions associated with Zoning and subdivisions, a number of alternative regulations governing future PUD Site Plans within the Woodmont Commons PUD are contained within the PUD Master Plan (*Section 2.4 PUD Site Plan Regulations and Standards*). The specific list of Site Plan waivers is contained in *Section 2.1 Introduction* and are waived. If an existing provision of the *Londonderry Site Plan Regulations* is not waived in the PUD Master Plan, then future proposals will need to comply with the applicable Londonderry provisions in effect on the date of the completed Woodmont Commons PUD Master Plan application (October 3, 2012).

Supplemental Documents

Several supplemental documents have been assembled as appendices; each has a particular relationship to the PUD Master Plan and related approval processes.

- *Section 4.1 Updated Abutters List* – This is a complete list of abutters based on information obtained prior to the final submittal of this PUD Master Plan for Planning Board approval; provision of this list fulfills a formal requirement of the *Londonderry Zoning Ordinance*;
- *Section 4.2 Master Plan Traffic Impact Assessment (MTIA)* – This is a comprehensive technical evaluation of potential transportation impacts based on estimates and approximations of development scenarios. This analysis is part of the

PUD Master Plan Information required by the *Londonderry Zoning Ordinance*. The findings of the MTIA form the basis for various thresholds, mitigation standards or improvement requirements associated with certain types of projects and approvals that are described in relevant portions of *Section 2.0 PUD Regulations and Standards*;

- *Section 4.3 Infrastructure Memorandum* – This is technical information regarding future utility and stormwater management infrastructure based on estimates and approximations of development scenarios. It has been provided as PUD Master Plan Information fulfilling the submission requirements (*Londonderry Zoning Ordinance, Sections 2.8.9.2.14 to 2.8.9.2.17*). Related thresholds, mitigation or improvement requirements that are incorporated into subsequent Planning Board project review and approval processes are provided within *Section 2.0 PUD Regulations and Standards*;
- *Section 4.4 Master Fiscal Impact Analysis (MFIA)* – This section contains information provided at the request of the Planning Board as part of their review of the PUD Master Plan. It evaluates the prospective fiscal impact upon the Town of Londonderry based on estimates and approximations of development scenarios. Related thresholds, mitigation or improvement requirements that will be incorporated into subsequent Planning Board project review and approval processes are provided within *Section 2.0 PUD Regulations and Standards*;
- *Section 4.5 Development Agreement* – Some of the actions, mitigation or improvements associated with implementing the PUD Master Plan are subject to a specific agreement between the Town of Londonderry and the proponents of Woodmont Commons, rather than through the regulatory framework and associated approvals that are addressed within the PUD Master Plan documents. The separate Development Agree-

1.4.1 Documents and Relationships

ment is included for information and reference as a supplemental document;

- Section 4.6 Sample Restrictive Covenant – This Section includes a form of restrictive covenant, to be executed and recorded by each Land Owner;
- Section 4.7 Sample Forms – This Section contains two sample forms that will be used during the PUD Subdivision and PUD Site Plan application processes;
- Section 4.8 Londonderry Zoning Ordinance, as Amended Through December 2011 – This Section contains the text of the *Londonderry Zoning Ordinance, as Amended Through December 2011* and is the version of the *Londonderry Zoning Ordinance* referred to throughout this PUD Master Plan;
- Section 4.9 Subdivision Regulations, June 10, 2009 – This Section contains the text of the *Subdivision Regulations, June 10, 2009* and is the version of the *Londonderry Subdivision Regulations* referred to throughout this PUD Master Plan;
- Section 4.10 Site Plan Regulations, June 10, 2009 – This Section contains the text of the *Site Plan Regulations, June 10, 2009* and is the version of the *Londonderry Site Plan Regulations* referred to throughout this PUD Master Plan;
- Section 4.11 Typical Details for Site and Roadway Infrastructure, May 2009 – This Section contains the text of the *Typical Details for Site and Roadway Infrastructure, May 2009* and is the version of the *Typical Details for Site and Roadway Infrastructure* referred to throughout this PUD Master Plan.



2.0 PUD Regulations and Standards

WOODMONT COMMONS

PLANNED UNIT DEVELOPMENT MASTER PLAN

SEPTEMBER 2013

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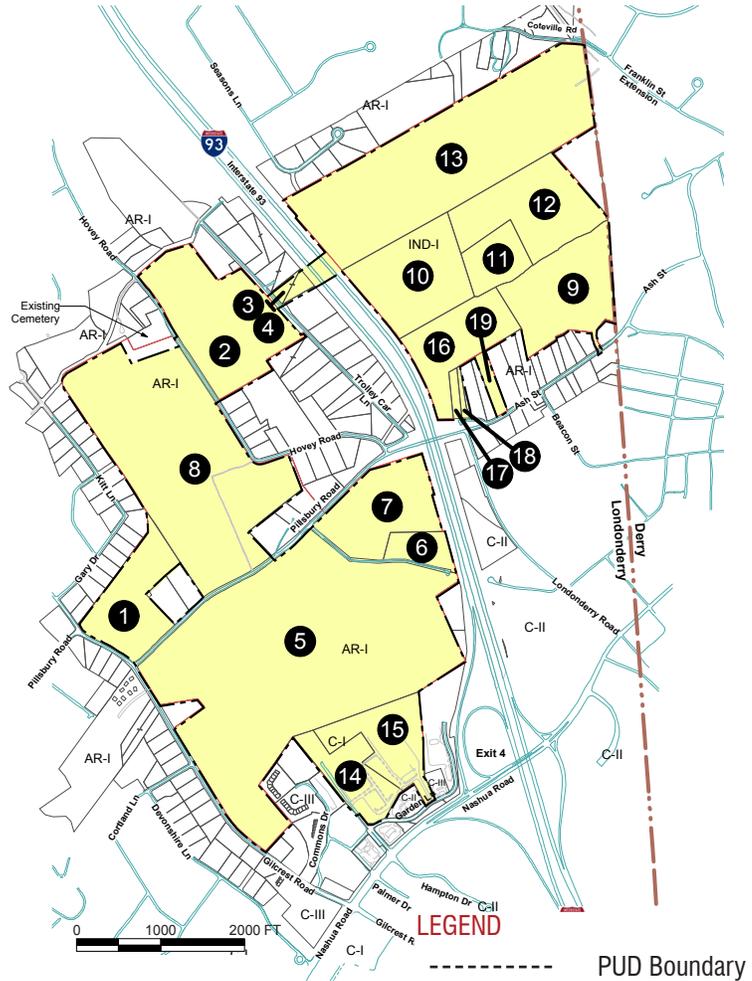
2.1 INTRODUCTION

2.1.1 Purpose of the PUD Regulations and Standards

Section 2.0 PUD Regulations and Standards contains all of the components of PUD Master Plan that shall be used as the criteria to guide all project proposals, reviews and approvals within the Woodmont Commons PUD for which the Planning Board is the regulatory authority for Town of Londonderry. These Regulations and Standards specifically apply to all projects that would otherwise be subject to Planning Board review and approval in implementing the *Londonderry Zoning Ordinance*, the *Londonderry Subdivision Regulations* and the *Londonderry Site Plan Regulations*.

These PUD Regulations and Standards replace or augment all of the Town's regulations and standards that have been waived through the adoption of the PUD Master Plan. If the existing Town regulations and standards subject to Planning Board approval as of October 3, 2012 have not been waived, they will apply as criteria for applicable project reviews and approvals, in concert with the special PUD Regulations and Standards.

2.1.2 Map and Boundary Description



Woodmont Commons Property List

Lot	Parcel ID	Acres	Underlying Zoning	West	East	Lot
1	010 015 0	24.000	Agricultural-Residential (AR-I)	24.000		1
2	010 023 0	38.000	Agricultural-Residential (AR-I)	38.000		2
3	010 029 2A	1.187	Agricultural-Residential (AR-I)	1.187		3
4	010 029 2B	1.187	Agricultural-Residential (AR-I)	1.187		4
5	010 041 0	162.003	Agricultural-Residential (AR-I)	162.003		5
6	010 041 1	5.000	Agricultural-Residential (AR-I)	5.000		6
7	010 041 2	25.190	Agricultural-Residential (AR-I)	25.190		7
8	010 042 0	96.980	Agricultural-Residential (AR-I)	96.980		8
9	010 045 0	28.000	Agricultural-Residential (AR-I)		28.000	9
10	010 046 0	29.000	Industrial I (IND-I)		29.000	10
11	010 047 0	9.000	Industrial I (IND-I)		9.000	11
12	010 048 0	29.000	Industrial I (IND-I)		29.000	12
13	010 050 0	96.000	Industrial I (IND-I)		96.000	13
14	010 052 0	14.650	Commercial I (C-I)	14.650		14
15	010 054 1	18.640	Commercial I (C-I)	18.640		15
16	010 057 0	20.480	Industrial I (IND-I)		20.480	16
17	010 058 0	1.300	Agricultural-Residential (AR-I)		1.300	17
18	010 059 0	1.060	Agricultural-Residential (AR-I)		1.060	18
19	010 062 0	2.720	Agricultural-Residential (AR-I)		2.720	19
		603.397		386.837	216.560	

Source: Londonderry GIS

2.1.3 Planning and Design Principles

The PUD Master Plan has been prepared to fulfill specific community and public interests associated with the *Londonderry Zoning Ordinance* and the relevant plans and policies that contributed to the preparation and adoption of *Section 2.8 Planned Unit Development* and of the PUD Master Plan. The following Planning and Design Principles are intended to provide guidance and be employed as supplemental criteria for two circumstances:

- Interpretation of PUD Regulations and Standards – In some cases, interpretation of specific PUD Regulations and Standards may be required based on the unusual circumstances of a specific project proposal. In such cases, the Planning and Design Principles can be employed by the Planning Board as a guidance document.
- Review and Approval of Compliance Alternatives – In some cases, the underlying public purposes may be accomplished as well or better through alternative design and development approaches that have not been envisioned with the range of PUD Regulations and Standards within this document. As a result, applicable portions of the PUD Regulations and Standards may also be implemented through Compliance Alternatives that are agreed to by both the Planning Board and the project proponents. In such cases, the Planning Board shall use the following Planning and Design Principles as the criteria for determining that the Compliance Alternative achieves the underlying public purposes for any specific Regulation or Standard. See *Section 2.2.1 Description* for more information.

The applicable Planning and Design Criteria address the following:

- Aesthetically attractive features – The features of the PUD Subdivision and/or PUD Site Plan will enhance the aesthetics of the Woodmont Commons PUD as viewed from publicly accessible roads, streets, paths or open space within or adjacent to the PUD.
- Connected, walkable neighborhoods – The characteristics of the improvements or development will contribute to a walkable community that is linked to other portions of the Woodmont Commons PUD or publicly-accessible pedestrian routes adjacent to the PUD.
- Quality site design – The site design will provide public benefits in terms of sustainable use of resources, visual quality from public vantage points, and complementary relationships to adjacent land or development.
- Quality architectural design – Design that will follow Architectural Standards and feature complementary relationships to adjacent land or development as viewed from publicly accessible roads, streets, paths or open space within or adjacent to the PUD.
- Potential impacts and mitigation needs – The proposed PUD Subdivision and/or PUD Site Plan will either have no a material effect on potential municipal impacts or will reduce such impacts. Associated mitigation requirements will employ the PUD Master Plan Regulations and Standards.

2.1.4 Applicability

The PUD Regulations and Standards shall be applied to any proposed change in land use or improvements that would otherwise be subject to compliance or approval by the Town under the *Londonderry Zoning Ordinance*, *Subdivision Regulations* and *Site Plan Regulations* effective on October 3, 2012.

Waivers/Modifications/Compliance with Specified Development Standards

Consistent with the purposes of a Planned Unit Development (PUD) as expressed in *Section 2.8.2* of the *Londonderry Zoning Ordinance*, including allowing “a landowner to propose his/her own development project largely independent from current land use regulations otherwise applicable to that property,” and to otherwise fulfill the objective of creating a “special zoning district ... in terms of uses, dimensions, other development standards,” the PUD Master Plan waives and modifies, pursuant to *Section 2.8.7.4.1 and 2.8.7.4.2*, certain of the existing development standards contained within the *Londonderry Zoning Ordinance*, the *Londonderry Site Plan Regulations* (LSPR), and the *Londonderry Subdivision Regulations* (LSR). To the extent that a development standard is not specifically waived and modified, or the applicable standard is not otherwise determinable, the most appropriate existing and underlying standard shall be applied as determined by the Planning Board. See *Section 2.8.10.2*. Following approval of the PUD, and with respect to the Planning Board’s receipt and review of any PUD Site Plan or PUD Subdivision application concerning the development of lands within the PUD, the Planning Board shall evaluate conformity with the PUD Master Plan, as the same may be amended from time to time.

Application of the following development standards of the *Londonderry Zoning Ordinance* are hereby waived in favor of the alternative design standards contained within the approved PUD Master Plan:

<i>Londonderry Zoning Ordinance</i> Section	Explanation of Waiver or Modification
Section 1.2 – Impact Fees	(MODIFICATION) Impact fees chargeable for site-specific development will be calculated during PUD Site Plan and/or PUD Subdivision reviews in accord with <i>Section 4.5 Development Agreement</i> .
Section 1.3 – Residential Development Phasing	Phasing of residential units shall not be limited by the express calculation of <i>Section 1.3.3</i> .
Section 1.5.2 – Conditional Use Permits	Approval of the PUD Master Plan, and its process for amendments to the PUD, supersedes the need for conditional use permits or zoning variances and special exceptions. See <i>Londonderry Zoning Ordinance Section 2.8.3.9</i> ; see also PUD Master Plan amendment procedure in <i>Section 2.5 Administration</i> . The exception to this waiver is that Conditional Use Permits shall still be required prior to the disruption of wetlands and associated buffers within the Conservation Overlay District.

2.1.4 Applicability

Section 2 – Zoning Districts/Use Table	<p>Within the PUD Master Plan, proposed zoning districts and dimensional standards, along with the uses permitted within each development Subarea, shall, in the first instance, be defined by the terms of the PUD Master Plan, including the Land Use Plan, and associated regulations, standards, tables, illustrations and alternative design standards. Compliance with the PUD Master Plan including <i>Section 2.3 PUD Subdivision Regulations and Standards</i> and <i>Section 2.4 PUD Site Plan Regulations and Standards</i> shall be evaluated by the Planning Board during PUD Subdivision and/or PUD Site Plan review. In the event of a conflict on roadway dimensional standards between the PUD Master Plan and the Town of Londonderry's <i>Typical Details for Site and Roadway Infrastructure</i>, the PUD Master Plan shall control. Lastly, consistent with <i>Londonderry Zoning Ordinance 2.8.6.2</i>, "uses that are permitted in the underlying zoning district ... shall be considered permitted uses in a PUD" and may be proposed for site development notwithstanding the approval of a PUD. The Town is not obligated to accept as public such roadways as are not built to applicable Town standards. The addition of a sidewalk on one or both sides of the roadway, however, does not automatically disqualify a roadway from public acceptance provided the Developer arranges for the maintenance of said sidewalks.</p>
2.6.3 Conservation Overlay Districts	<p>(MODIFICATION) The restrictions imposed by the existing Conservation Overlay District (COD), <i>Londonderry Zoning Ordinance Section 2.6.3</i> shall apply to development within the PUD, provided that the Planning Board may consider and approve proposed encroachments pursuant to <i>Londonderry Zoning Ordinance Section 2.6.3.4.2.2</i> that would, if granted, allow construction of new structures as close as the edge of the jurisdictional wetland.</p>

2.1.4 Applicability

Section 3.1.2 – Local Excavation Standards	(MODIFICATION) Excavation is not a proposed land use within the PUD. Thus, any excavation activities within the PUD, that furthers the purposes of the PUD or prepares the land within the PUD for development along public infrastructure, shall be deemed “exclusively incidental to the construction or alteration of a building or structure or the construction or alteration of a parking lot or way” and exempt from local permitting pursuant to RSA 155-E:2-a(I)(a). The Developer will stabilize or reclaim any excavated areas not immediately developed or redeveloped pursuant to state minimum reclamation standards, and with all required State approvals relating thereto.
Section 3.2 – Performance Standards	Pursuant to <i>Londonderry Zoning Ordinance Section 2.8.7.5</i> , a fifty foot (50’) Perimeter Buffer will be maintained around the exterior PUD tract boundaries as indicated on the Land Use Plan. Within the PUD, performance standards shall be determined by the terms of the PUD Master Plan. Compliance with the PUD Master Plan shall be evaluated by the Planning Board during PUD Subdivision and/or PUD Site Plan review.
Section 3.3.3.1.1 – Conservation Subdivisions	Regardless of lot size, conservation subdivisions shall not be mandatory within the PUD. Instead, conservation subdivisions shall remain “available” as an option for applicants pursuant to <i>Section 3.3.3.1.1</i> and as otherwise consistent with the terms of the PUD Master Plan. If pursued, Compliance with conservation subdivision technical requirements shall be evaluated by the Planning Board during PUD Subdivision review.
Section 3.10 – Vehicle Access and Parking	Vehicle and bicycle access, parking space standards and parking loading shall be determined by the terms of <i>Section 2.3.4 Parking and Loading Standards</i> of the PUD Master Plan. Compliance with the PUD Master Plan shall be evaluated by the Planning Board during PUD Subdivision and/or PUD Site Plan review.

2.1.4 Applicability

Section 3.11 – Signs	Signage standards for commercial and mixed use areas shall be determined by the terms of <i>Section 2.3.7 PUD Subdivision Signage</i> and <i>Section 2.4.4 PUD Site Plan Signage</i> of the PUD Master Plan. Compliance with the PUD Master Plan shall be evaluated by the Planning Board during PUD Subdivision and/or PUD Site Plan review. For residential areas, signage shall remain subject to <i>Londonderry Zoning Ordinance Section 3.11</i> .
Section 3.12.1.1 – Home Occupations	Home occupations shall be permitted in accord with the terms of the PUD Master Plan <i>Section 2.2 Land Use Regulations</i> and without the need for a special exception. Compliance with the PUD Master Plan shall be evaluated by the Planning Board during PUD Subdivision and/or PUD Site Plan review.
Section 3.12.2.1/3.12.3.1 – Child Care/Adult Day Care Facilities as Home Occupations	Child care and adult care facilities as home occupations shall be permitted in accordance with the terms of the PUD Master Plan <i>Section 2.2 Land Use Regulations</i> and without the need for a special exception. Compliance with the PUD Master Plan shall be evaluated by the Planning Board during PUD Subdivision and/or PUD Site Plan review.
Section 4.7 – Definitions	To the extent the PUD Master Plan contains different definitions for terms used in both the <i>Londonderry Zoning Ordinance</i> and the PUD, the definition stated in <i>Section 2.1.5 Definitions</i> of the PUD Master Plan shall prevail.
Appendix A – Roadway Classifications	To the extent conflicts arise in relation to definitions and road classifications, <i>Section 2.3.3 Transportation Network</i> shall control.

2.1.4 Applicability

Application of the following development standards of the LSPR are hereby waived in favor of the alternative design standards contained within the approved PUD Master Plan:	
LSPR Section	Explanation of Waiver or Modification
Section 1.03(d) – Purpose: Sensitivity to Rural and Agricultural Heritage	The Woodmont Commons PUD is specially designed with perimeter uses that are sensitive to and consistent with the character and use of exterior abutting properties. Within the PUD, however, the development themes are neither rural nor agricultural in nature, but express a commonality of theme consistent with the objectives of a planned unit development.
Section 3.01(a) – Approval of Improvements	Improvements will be constructed in accord with the PUD Master Plan, the <i>Londonderry Zoning Ordinance</i> , the LSPR and the LSR, consistent with the waivers and modifications approved by the Planning Board.
Section 3.01(c) – Standards and Specifications	Improvements will be constructed in accord with the PUD Master Plan, the <i>Londonderry Zoning Ordinance</i> , the LSPR and the LSR, consistent with the waivers and modifications approved by the Planning Board.
Section 3.02 – Monuments	(MODIFICATION) All lot boundaries shall be readily determinable within the PUD Master Plan using a computer-aided drafting system (CAD). Conventional monumentation of every boundary shall not be required where not practical. Alternatives to conventional monumentation, such as GIS-based documentation or the use of plaques or other markers, may be used. Given the application of alternative design standards within the PUD, boundaries may be obstructed by the placement of structures, roads and other improvements.
Section 3.07(a)(1) – Storm Drain System	(MODIFICATION) The existing regulations for the Storm Drain System shall apply, except for the provision of underground detention and infiltration systems being allowed without requiring a waiver. Underground detention and infiltration systems shall not be permitted for roadways or other facilities that are planned to be publicly owned, unless otherwise waived by the Planning Board. The analysis of any proposed underground system shall consider site-specific soil data and standard storm events for analysis. An operations and maintenance plan for any underground detention and infiltration system shall be provided for Planning Board review at the time of PUD Subdivision and/or PUD Site Plan application.

2.1.4 Applicability

Section 3.08 – Streets/Driveways/Sidewalks	To the extent conflicts arise, <i>Section 2.3.3 Transportation Network</i> of the PUD Master Plan shall control.
Section 3.09 – Landscaping Design Standards	To the extent conflicts arise, <i>Section 2.4.6 PUD Site Plan Landscape</i> of the PUD Master Plan shall control.
Section 3.11 – Parking Lot Design Standards Except Section 3.11(d)	As required by the <i>Londonderry Zoning Ordinance</i> , the PUD Master Plan contains specific and alternative design standards relating to parking capacities, layout and function.
Section 3.12 – Building and General Appearance Design Standards	(MODIFICATION) The PUD Master Plan includes architectural guidelines under <i>Section 2.4.2 Buildings and Lots</i> for buildings proposed within the Woodmont Commons PUD. Compliance with the PUD Master Plan shall be evaluated by the Planning Board during PUD Site Plan review.
Section 3.13 – Outdoor Lighting Design Standards	To the extent conflicts arise, <i>Section 2.4.5 PUD Site Plan Lighting</i> of the PUD Master Plan shall control.
Section 3.14/4.17 – Traffic Impact Analysis	<i>Section 3.1 Transportation</i> of the PUD Master Plan contains specific and alternative design standards relating to traffic impact analysis.
Section 4.14(d) – Improvement Plans/Streets	To the extent conflicts arise, the PUD Master Plan <i>Section 2.3.3 Transportation Network</i> shall control.
Section 4.15 – Building Renderings	The process for submitting building renderings shall be observed during the PUD Site Plan approval process, but substantive evaluation shall compare the conceptual renderings to the alternative architectural design standards incorporated within the PUD Master Plan under <i>Section 2.4.2 Buildings and Lots</i> and not LSPR <i>Section 3.12</i> .
Section 5.01 – Commercial Wireless Communication Facilities	(MODIFICATION) <i>Section 5.01</i> shall apply to new freestanding wireless communication facilities proposed within the PUD, but shall not apply to wireless facilities contained and concealed within a clock tower, belfry or similar building feature designed to conceal the facility from public view.

2.1.4 Applicability

Application of the following development standards of the LSR are hereby waived in favor of the alternative design standards contained within the approved PUD Master Plan:

LSR Section	Explanation of Waiver or Modification
Section 3.01(c) – Standards and Specifications	Improvements will be constructed in accord with the PUD Master Plan, the <i>Londonderry Zoning Ordinance</i> , the LSPR and the LSR, consistent with the waivers and modifications approved by the Planning Board.
3.02 – Monuments	(MODIFICATION) All lot boundaries shall be readily determinable within the PUD Master Plan using a computer-aided drafting system (CAD). Conventional monumentation of every boundary shall not be required where not practical. Alternatives to conventional monumentation, such as GIS-based documentation or the use of plaques or other markers, may be used. Given the application of alternative design standards within the PUD, boundaries may be obstructed by the placement of structures, roads and other improvements.
Section 3.03 – Lots	As required by the <i>Londonderry Zoning Ordinance</i> , the PUD Master Plan contains specific and alternative design standards relating to lot dimensions and building setback lines. The subdivision of perimeter parcels shall comply with <i>Section 3.03(a) and (b)</i> of the <i>Londonderry Subdivision Regulations</i> .
Section 3.08 – Storm Drain System	(MODIFICATION) The existing regulations for the Storm Drain System shall apply, except for the provision of underground detention and infiltration systems being allowed without requiring a waiver. Underground detention and infiltration systems shall not be permitted for roadways or other facilities that are planned to be publicly owned, unless otherwise waived by the Planning Board. The analysis of any proposed underground system shall consider site-specific soil data and standard storm events for analysis. An operations and maintenance plan for any underground detention and infiltration system shall be provided for Planning Board review at the time of PUD Subdivision and/or PUD Site Plan application.

2.1.4 Applicability

Section 3.09(A) – Streets/General	To the extent conflicts arise, the PUD Master Plan <i>Section 2.3.3 Transportation Network</i> shall control. To the extent historic stonewalls exist and are intact along the PUD perimeter boundary, such features shall be preserved.
Section 3.09(B) – Streets/Access	Within the PUD, newly created lots must have safe, direct frontage and access to an existing roadway or a road proposed in accord with the PUD Master Plan <i>Section 2.3.3 Transportation Network</i> . Proposed streets may be accepted as public ways or held privately with suitable maintenance and joint-use arrangements approved by the Planning Board. To the extent conflicts arise, the PUD Master Plan <i>Section 2.3.3 Transportation Network</i> shall control.
Section 3.09(D) – Streets/Arrangement	Within the PUD, newly created lots must have safe, independent and direct frontage and access to a public space, an existing roadway or a road proposed in accord with the PUD Master Plan <i>Section 2.3.3 Transportation Network</i> . Proposed streets may be arranged in such manner as to connect to private ways, provided suitable maintenance and joint-use arrangements and cross access agreements are approved by the Planning Board. To the extent conflicts arise, the PUD Master Plan <i>Section 2.3.3 Transportation Network</i> shall control.
Section 3.09(E) – Streets/ Classification	To the extent conflicts arise, the PUD Master Plan <i>Section 2.3.3 Transportation Network</i> shall control. The Town is not obligated to accept as public such roadways as are not built to applicable Town standards. The addition of a sidewalk on one or both sides of the roadway, however, does not automatically disqualify a roadway from public acceptance provided the Developer arranges for the maintenance of said sidewalks.
Section 3.09(F, G, I, P, R (incl. Table 1, Pg. 22), S (incl. Tables 2 and 3, Pg., 23)) – Streets/Driveways, Sidewalks, Curbs, Guardrail, Roadway Design Standards, Intersections	To the extent conflicts arise, the PUD Master Plan <i>Section 2.3.3 Transportation Network</i> shall control.
Section 3.10/4.17 – High Intensity Soil Study (HISS)	Since the entire PUD will be serviced by water and sewer, high intensity soil study is not relevant.

2.1.5 PUD Definitions

Definitions are for the purposes of the PUD Master Plan only. The intent is to define those words and phrases that are either not defined in the *Londonderry Zoning Ordinance* or where the PUD Master Plan requires a variation or clarification of meaning.

The general definitions of the *Londonderry Zoning Ordinance* shall pertain to the PUD Master Plan unless otherwise defined herein. Undefined words and phrases within the PUD Master Plan or the *Londonderry Zoning Ordinance* shall have ordinary dictionary meanings.

AGRICULTURE: All operations of a farm such as the cultivation, conserving, and tillage of the soil, dairying, greenhouse operations, the production, cultivation, growing and harvesting of any agricultural, floricultural, sod or horticultural commodities, the raising of livestock, bees, fur-bearing animals, fresh water fish or poultry, or any practices on the farm as an incident to or in conjunction with such farming operations including, but not necessarily restricted to, the following: preparation for market, delivery to storage or to market, or to carriers for transportation to market, or any products or materials from the farm; the transportation to the farm of supplies and materials; the transportation of farm workers; forestry or lumbering operations; the marketing or selling at wholesale or retail or in any other manner any products from the farm and of other supplies that do not exceed in average yearly dollar volume the value of products from such farm. Includes the ability to conduct community supported farming and agriculture.

APPLICANT/PRIMARY APPLICANT: Pillsbury Realty Development, LLC (“Pillsbury”), on behalf of the Land Owners, serves as a the Primary Applicant and principal liaison to the Town of Londonderry with respect to planning and land use approvals.

ARTISAN USE: A use that involves a worker or craftsperson who practices a trade or handicraft, or one that produces something (as cheese, beer or wine) in limited quantities often using traditional methods.

BLOCK: The aggregate of public frontage, private lots and access lanes.

BLOCK FACE: The dimension of a single block face that includes the block and public frontages as measured along the curb from back of curb to back of curb or back of curb to Recreational Trail or PUD property line.

BLOCK PERIMETER: The distance around the perimeter of a block, measured at the curb.

BOUTIQUE HOTEL: Any building containing up to ten guest rooms which are used, rented, or hired for sleeping purposes by transient guests and with access to units primarily from interior lobbies, courts, or halls.

BUILDING, HEIGHT OF: The vertical distance from the average grade adjoining the walls of the structure to the average of the highest points of a roof surface of a flat roof ; and to the deck line of a mansard roof; and the mean height level between eaves and ridge for a gable, hip or gambrel roof, but does not include structures not intended for human occupancy such as chimneys, water towers, steeples, etc.

BUSINESS CENTER DEVELOPMENT: A tract of land, buildings or structures for business activities planned as a whole and intended to include those uses allowed in the Woodmont Commons PUD whether built at one time as a unit or in two or more development stages.

2.1.5 PUD Definitions

CONFERENCE/CONVENTION CENTER: A facility designed and used for conventions, conferences, seminars, product displays, recreation activities and entertainment functions, along with accessory functions including temporary outdoor displays, and food and beverage preparation and service for on-premise consumption. The facility may include hotel functions.

COTTAGE COURT: Residential lots oriented onto a common open space with sidewalk connections to a Public Frontage.

CULTURAL USES AND PERFORMING ARTS: Facilities, both indoor and outdoor, for public assembly and group entertainment including: civic theaters and facilities for “live” theater and concerts, motion picture theaters, public and semi public auditoriums and similar public assembly uses.

DAY CARE CENTER, ADULT: An Adult Day Care facility in which more than 12 clients receive care, maintenance, and supervision by someone other than a relative or legal guardian for less than 24 hours per day, which has been licensed by a state or county licensing agency.

DEDICATED OFFICE: A freestanding single-use office structure containing business offices as its principal use, with over 10,000 gross square feet in building area that may only be located along the Interstate Highway 93 corridor and within 1,000 feet of the PUD boundary line.

DEVELOPER: Pillsbury Realty Development, LLC (“Pillsbury”).

DRIVE-THRU WINDOWS: A customer service facility designed for the convenience of the motoring public accessory to an office or retail establishment which is intended to enable the customer to transact business with a person located within a structure or a machine without exiting the motor vehicle.

DWELLING UNIT, ACCESSORY: A dwelling unit that is incidental to, subordinate to, and customarily found in connection with a single family dwelling and which is situated on the same lot as the single family dwelling. See *Section 2.4.2 Buildings and Lots*.

DWELLING, SINGLE-FAMILY DETACHED: A detached or free-standing residence other than a mobile home, designed for and occupied by one family only.

DWELLING, SINGLE-FAMILY ATTACHED: An attached residence, designed for and occupied by one family only.

EDUCATION AND TRAINING FACILITIES: Public or private (for profit or nonprofit) educational institutions offering instruction in the following: art, ballet and other dance, computers and electronics, drama, driver education, language, music, and other similar activities. This includes business, secretarial, and vocational schools, establishments providing courses by mail, professional schools (law, medicine, etc.), and seminaries/religious ministry training facilities. Also includes facilities, institutions and conference centers that offer specialized programs in personal growth and development, such as fitness, environmental awareness, arts, communications and management.

2.1.5 PUD Definitions

FLEX USE: Artisan Use or Light Industrial Use in combination with retail and office uses related to the business.

GALLERY: An attached flat roof projecting from a building over a sidewalk or passage that is partly open to create a portico or colonnade.

GREEN SPACE, CONSERVED: Land that is reserved or dedicated and retains or accommodates predominately natural features that may have public access to the extent consistent with the conservation goals and applicable regulations, and as specified in the requirements within the PUD Master Plan. This may include regulated open space such as designated wetland areas subject to state and federal regulations, areas that provide shared stormwater drainage and water quality features serving multiple Subareas, and intermittent or perennial streams or ponds. No buildings are allowed on the site except Accessory Structures as defined in the *Londonderry Zoning Ordinance* that store maintenance equipment. See *Section 2.2.3 Land Use Standards* and *Section 2.3.6 Conserved Green Space and Shared Open Space Standards*.

GROUP CHILD CARE CENTERS: Either a full day or half day child care facility (whether or not the facility is known as day nursery, nursery school, kindergarten, etc.) where services are regularly provided for any part of a day, but less than twenty-four (24) hours to thirteen (13) or more children.

HOME OCCUPATION: an occupation, profession, activity or use that is clearly a customary, secondary, and incidental use of a residential dwelling unit.

HOTEL: Any building containing eleven or more guest rooms which are used, rented, or hired for sleeping purposes by transient guests and with access to units primarily from interior lobbies, courts, or halls.

INDUSTRIAL USE, LIGHT: A use that manufactures or assembles small or lightweight articles; or items that use moderate amounts of partially processed materials to produce items of relatively high value per unit weight.

INFORMATION PLAN: One of many conceptual plans that could be built according to the regulations, standards and guidelines in this PUD Master Plan.

LAND USE PLAN: A framework of key elements required by *Section 2.8 Planned Unit Development* of the *Londonderry Zoning Ordinance*.

LAND OWNERS: Pillsbury Realty Development, LLC (“Pillsbury”), Hyrax Derry Partners, LLC, Demoulas Supermarkets, Inc., and Robert D. and Stephen R. Lievens.

LONDONDERRY SITE PLAN REGULATIONS: The *Site Plan Regulations, June 10, 2009*. This document is provided in *Section 4.10 Site Plan Regulations, June 10, 2009*.

LONDONDERRY SUBDIVISION REGULATIONS: The *Subdivision Regulations, June 10, 2009*. This document is provided in *Section 4.9 Subdivision Regulations, June 10, 2009*.

LONDONDERRY ZONING ORDINANCE: The *Londonderry Zoning Ordinance, as Amended Through December 2011*. This document is provided in *Section 4.8 Londonderry Zoning Ordinance, as Amended Through December 2011*.

2.1.5 PUD Definitions

MASTER PLAN TRANSPORTATION IMPACT ASSESSMENT: The traffic analysis conducted for Pillsbury Realty Development, LLC for the Woodmont Commons property by TEC as dated February 6, 2013 and included as a supplemental document in *Section 4.2 Master Plan Transportation Impact Assessment*. (Also referred to as the MTIA)

MIXED USE: Two or more uses contained within the same building or on the same lot.

OPEN SPACE, SHARED: Land that is held or restricted for common benefit and as categorized within the regulations and requirements of the PUD Master Plan, regardless of the ownership of the land. May include pervious or impervious surfaces. See *Section 2.3.6 Conserved Green Space and Shared Open Space Standards*. Shared Open Space includes Civic Open Space, Active Open Space and Passive Open Space. Shared Open Space may include Agricultural Open Space. These categories of open space are described in *Section 2.2.3 Land Use Standards* and *Section 2.3.6 Conserved Green Space and Shared Open Space Standards*.

PARKING STRUCTURE: A building, land, or portion thereof designed or used for the temporary storage of motor-driven vehicles with one or more stories above or below grade, and which shall not be construed as retail space. Also known as a parking deck.

PERIMETER BUFFERS: A buffer that is reserved or dedicated on a common or individual lot along designated perimeters of the Woodmont Commons PUD as indicated on the Land Use Plan. Streets and structures shall be set back a minimum of fifty (50) feet from the exterior PUD tract boundary that abuts residential uses except where transportation or use linkages are desired. Landscaping and other screening devices, such as topography, berms, fences and walls, shall be designed to provide a buffer between the PUD and adjoining property except where compatible uses adjoin one another. Where a buffer is also part of a project, it may also be used to satisfy applicable PUD Subdivision and/or PUD Site Plan Regulations and Standards. May include pervious or impervious surfaces. See *Section 2.3.5 PUD Perimeter Buffers*.

PRIMARY STREET: Primary Streets for Woodmont Commons include both existing and new streets as identified for on-site and off-site improvements in the Master Plan Impact Assessment dated February 6, 2013 and as identified on the Land Use Plan.

PRIVATE FRONTAGE: A Private Frontage is defined as the area between the lot (or property) line and the building façade.

PROFESSIONAL OFFICE: A use within a mixed-use structure or a single-use structure where business or professional services are conducted or rendered.

PUBLIC ACCESS: The ability of the general public to access and use Conserved Green Space and Shared Open Space and their amenities or community facilities designed and made available for this purpose. Public access may be reasonably restricted in terms of times of day or in terms of the activities that are permitted in the interest of security, environmental sensitivity, compatibility with adjacent or nearby uses, and compatibility with access and use of the space by others. Public access may be limited to pedestrian uses and may reasonably restrict the use of motorized vehicles.

2.1.5 PUD Definitions

PUBLIC FACILITIES: Any facility including but not limited to buildings, property, recreation areas, and roads, which are leased or otherwise operated or funded by a governmental body.

PUBLIC FRONTAGE: A Public Frontage is the area from the curb to the lot (or property) line.

PUBLIC REALM: Any publicly owned streets, sidewalks, pathways, rights-of-way, parks, publicly accessible open spaces (whether Conserved Green Space or Shared Open Space) and any public and civic building and facilities.

PUBLIC UTILITIES: All lines and facilities related to the provision, distribution, collection, transmission, or disposal of water, storm and sanitary sewage, oil, gas, power, information, telecommunication and telephone cable, and includes facilities for the generation of electricity.

PUBLIC UTILITIES STRUCTURE: An electricity or gas substation, water or wastewater pumping station, telephone repeater station, water storage tank, reservoir, recycling collection center, cellular and telephone transmission facilities, or similar structure used as an intermediary switching, boosting, distribution, or transfer station for electricity water, wastewater, cable television, or telephone services between the point of generation and the end user, or a wastewater treatment plant, but not including satellite dish antennas, facilities for the handling of solid waste (except for recycling collection centers), or radio, television, or microwave transmission or relay towers.

RELIGIOUS FACILITIES: a building or buildings used for public worship by a congregation. Religious facilities include churches, synagogues, temples, mosques, and other places of religious worship.

RENTAL CAR SATELLITE OFFICE: Vehicle leasing offices and associated automobile parking.

RESTAURANT: a structure in which the principal use is the preparation and sale of food and beverages to the public on demand from a menu during stated business hours, to be consumed on the premises primarily inside the building. Includes cafes, taverns, and similar establishments but does not include a drive-thru establishment.

RESTAURANT, FAST FOOD: an establishment whose primary business is serving food to the public for consumption on or off the premises by order from and service to vehicular passengers outside the structure.

SECONDARY STREET: Secondary Streets for Woodmont Commons include new streets as identified on the Land Use Plan and as allowed by *Section 2.2.1 Description*.

SETBACK: The area of a lot measured from the lot line to a building façade that is maintained clear of permanent structures, with the exception of permitted encroachments listed in *Section 2.4.2 Buildings and Lots* (Var: build-to-line.)

SUPPLEMENTAL TRAFFIC IMPACT ANALYSIS: Also known as Supplemental TIA. A traffic analysis that may be prepared in conjunction with a PUD Subdivision and/or Site Plan submittal. A Supplemental TIA should be compared to the MTIA provided in *Section 4.2 Master Plan Traffic Impact Assessment*.

2.1.5 PUD Definitions

TYPICAL DETAILS FOR SITE AND ROADWAY INFRASTRUCTURE – The *Typical Details for Site and Roadway Infrastructure, May 2009* and is the version of the *Typical Details for Site and Roadway Infrastructure* referred to throughout this PUD Master Plan. This document is provided in *Section 4.11 Typical Details for Site and Roadway Infrastructure, May 2009*.

YIELD STREET: A street where a vehicle traveling in one direction slows or stops to allow a vehicle traveling in the opposite direction to pass.

2.2 LAND USE REGULATIONS

2.2.1 Description

The Land Use Plan depicts a framework incorporating the key elements that are inherent within the Concept Plan, including but not limited to:

- Subareas;
- Primary Street Network and Secondary Streets;
- Conserved Green Space (including regulated environmental areas);
- Open Space;
- Stormwater Detention Areas.

The Land Use Plan is purposefully flexible with the expressed understanding that PUD Subdivision Plans and PUD Site Plans prepared in accordance with the PUD Master Plan will vary and change according to the following subsections.

Subareas

The lines depicting the Subareas are approximate in location, and may move accordingly:

- The interior boundaries of the Subarea lines in WC-5, WC-6, WC-7, WC-9 and WC-11 may move inward by up to 100 feet;
- The Woodmont Commons PUD's interior Subarea boundaries may move inward or outward by +/- 100 feet;
- Uses common to Subareas that are adjacent may have blocks, lots, buildings, streets, open space and other features that cross over Subarea lines so long as the Use follows the regulations and Standards of the most restrictive Subarea;
- Uses exclusive to a Subarea must be located entirely within the Subarea.

Primary Street Network and Secondary Streets

Two types of streets are depicted on the Land Use Plan in *Section 2.2.2 Land Use Plan*, are approximate in their locations and may move accordingly:

- The Primary Street Network is shown in BOLD black lines on the Land Use Plan in *Section 2.2.2 Land Use Plan*. These streets are required streets in terms of vehicular function. While their actual location may vary, the intent as shown in *Section 4.2 Master Plan Traffic Impact Assessment (MTIA)* must be preserved within the PUD Subdivision and PUD Site Plan(s). The actual design of the Primary Streets shall be in accordance with the MTIA or Supplemental TIA, and shall be as selected from the street standards in *Section 2.3.3 Transportation Network* in accordance with the uses for the Block or Subarea;
- Secondary Streets are shown in MEDIUM black lines on the Land Use Plan in *Section 2.2.2 Land Use Plan*. These streets are shown for information purposes only in order to depict a proper application of the rules and regulations of the PUD Master Plan. Additional streets may be included and located at the time of PUD Subdivision and/or PUD Site Plan submittal. The actual design of the Secondary Streets shall be as selected from the street standards in *Section 2.3.3 Transportation Network* in accordance with the uses for the Block or Subarea.

Conserved Green Space

The areas shown on the Land Use Plan in *Section 2.2.2 Land Use Plan* are depicted based upon the known existing conditions as of the Planning Board acceptance of the complete application on October 3, 2012. Actual conditions, locations and protected measures thereto shall be presented at the time of PUD

2.2.1 Description

Subdivision and/or PUD Site Plan submittal. Conserved Green Space includes regulated environmental areas and may include stormwater detention areas. Conserved Green Space shall be provided as described within *Section 2.3.6 Conserved Green Space and Shared Open Space Standards*.

Shared Open Space

In addition to Conserved Green Space, additional areas shall be dedicated to a variety of civic, active and passive activities and provided as described within *Section 2.3.6 Conserved Green Space and Shared Open Space Standards*. Shared Open Space may include stormwater detention areas.

PUD Subdivision and PUD Site Plan(s)

PUD Subdivisions and PUD Site Plans are to be in accordance with the following:

- The general intent of the Land Use Plan In *Section 2.2.2 Land Use Plan* and contained within *Section 2.0 PUD Regulations and Standards*;
- *Section 2.2.4 Subarea Composition Principles and Standards* as applicable to both subdivisions and site plans;
- *Section 2.3 PUD Subdivision Regulations and Standards* as applicable to subdivisions;
- *Section 2.4 PUD Site Plan Regulations and Standards* as applicable to site plans;
- PUD Subdivisions and Site Plans may include all or portions of one or several Subareas;
- PUD Subdivisions may include Site Plans as part of the submittal;
- The submittal process shall be in accordance with *Section 2.5 Administration*.

Compliance Alternatives

The PUD Master Plan provides special PUD Regulations and Standards to implement Town goals for the area through a series of planning and design principles that are defined in *Section 2.1.3 Planning and Design Principles*. Conformance with the PUD Master Plan and in particular with *Section 2.0 PUD Regulations and Standards* establishes entitlements to undertake projects and improvements.

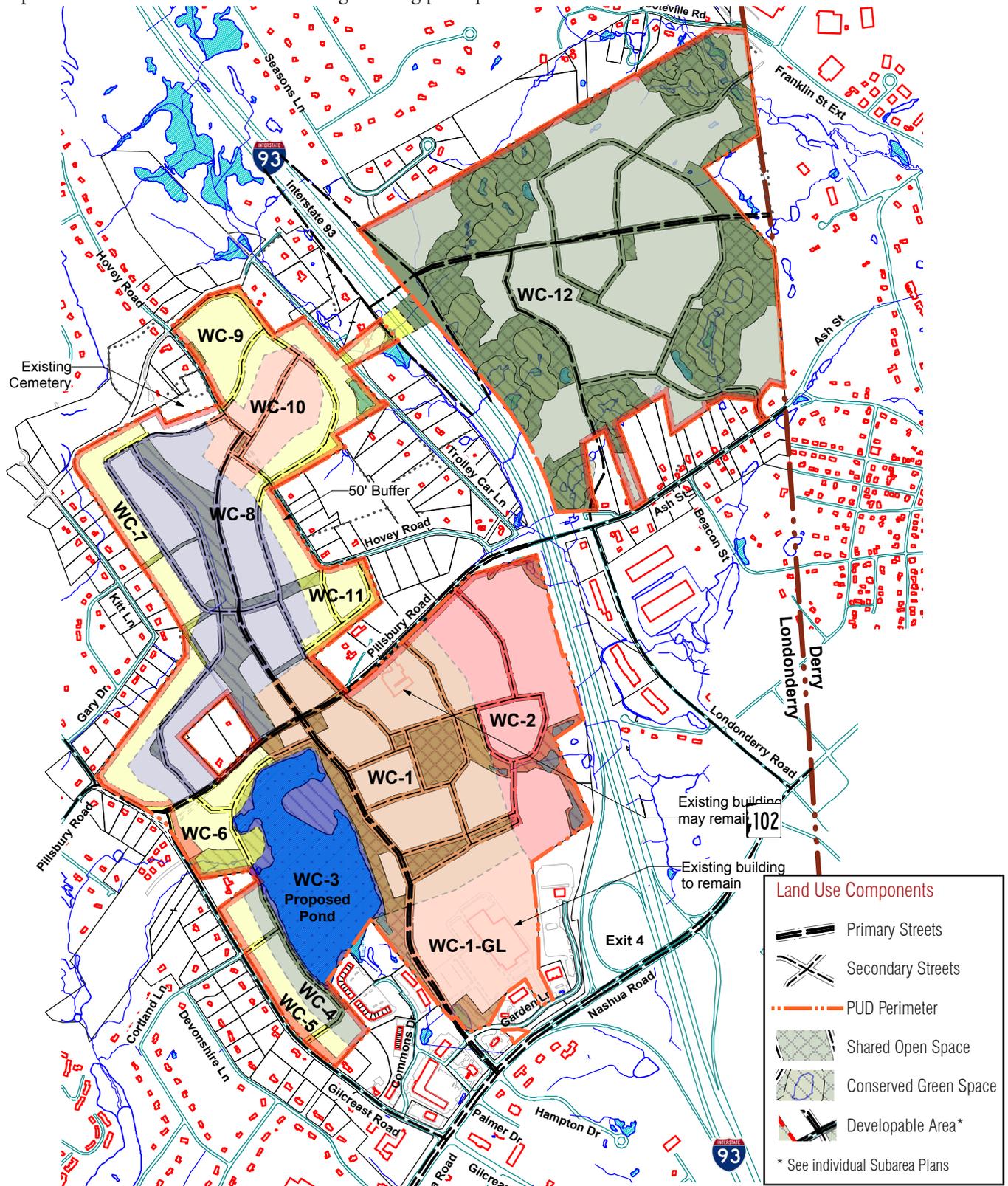
Under PUD Subdivision and/or PUD Site Plan review, the Planning Board and the applicant may mutually agree to an alternative method or standard that meets *Section 2.1.3 Planning and Design Principles* to the same degree or better. This may be accomplished without amending the PUD Master Plan if the applicant documents and the Planning Board approves a Compliance Alternative. A Compliance Alternative must accomplish the relevant Planning and Design Principles: documentation must indicate the specific proposed alternative method or standard that will be used, why the *Section 2.0 PUD Regulations and Standards* are not applicable to the application, and how the project is fully compliant with the intent of the PUD Master Plan. Planning Board approval of a compliance alternative is discretionary, but shall not be unreasonably withheld if the Applicant has provided sufficient documentation to justify such request.

All PUD Regulations and Standards are subject to these Compliance Alternatives, except for the following, which would need an amendment of the PUD Master Plan if the applicant wishes to modify them for a PUD Subdivision and/or PUD Site Plan application:

- Allowable Uses;
- Building Heights;
- Allowable Densities for Uses and Open Space for the PUD;
- Maximum Use Densities.

2.2.2 Land Use Plan

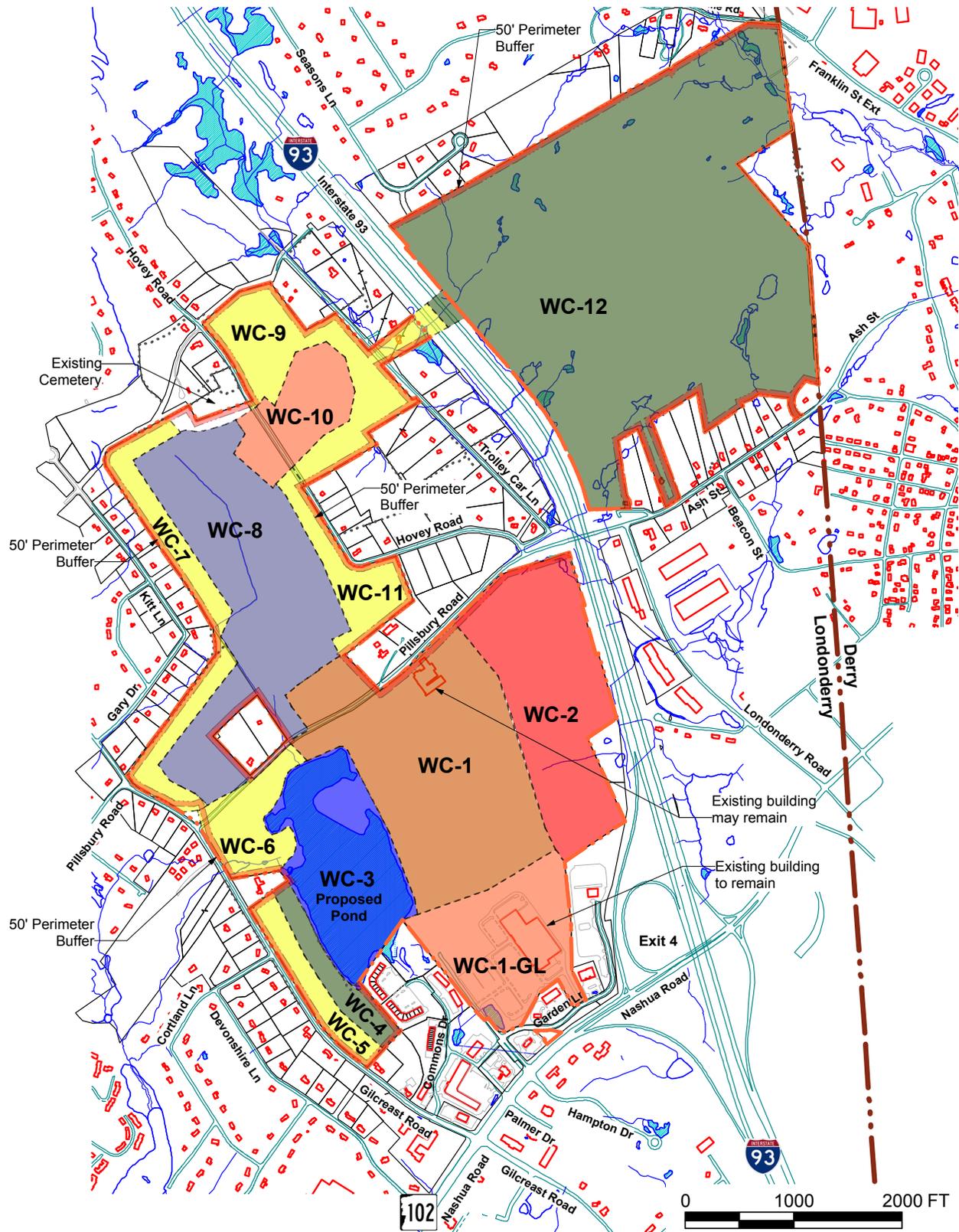
Subareas with the Land Use Components shown. See Section 2.2.4 Subarea Composition Standards and Principles for the development intent of each Subarea and the governing principles and standards.



LAND USE PLAN

2.2.2 Land Use Plan

Outline of each Subarea.



SUBAREA PLAN

2.2.3 Land Use Standards

The Land Use Standards define how land can be developed within the Woodmont Commons PUD. While the Land Use Plan shows the approximate location of streets, Conserved Green Space and Shared Open Space, and developable land, the Land Use Standards provide the rules by which development will be planned and by which PUD Subdivision and PUD Site Plan submittals will be evaluated.

There are several components to the Land Use Standards:

- The **Allowable Uses Table** defines which uses are allowed in each Subarea.
- The **Performance Standards** provide use-specific performance standards.
- The **Allowable Densities Table** defines the maximum density for certain uses by Subarea and for the Woodmont Commons PUD as a whole. This table also provides minimum requirements for Conserved Green Space and Shared Open Space. The footnotes and the flexibility rules are a part of the table and add further clarity about the density requirements.
- **Conserved Green and Shared Open Space Land Use Standards** provide the general rules for provision of these amenities throughout the Woodmont Commons PUD. This Section defines various categories of space and the requirements for the location of publicly accessible space in relation to development within the Woodmont Commons PUD.

These three components are the underlying standards for *Section 2.2.4 Subarea Composition Principles and Standards* which provide more specific details on land use and development requirements by Subarea.

2.2.3 Land Use Standards

Allowable Uses Table

The Allowable Uses Table presents the allowable uses by Subarea. The subsequent sections including the Subarea Standards, and Street, Block, Open Space and Building/Lot Types are subject to and governed by the Allowable Uses Table. For example, an Institutional Building may not be proposed in a Subarea that does not allow institutional uses.

	WC-1-GL	WC-1	WC-2	WC-3	WC-4	WC-5	WC-6	WC-7	WC-8	WC-9	WC-10	WC-11	WC-12
AGRICULTURAL													
1	Agriculture	A	A	A	A	A	A	A	A	A	A	A	A
RESIDENTIAL													
2	Dwelling, Multi-Family		A	A					A	A	A	A	A
3	Dwelling, Single-Family Detached		A	A		A	A	A	A	A	A	A	A
4	Dwelling, Single Family Attached		A	A		A		A	A	A	A	A	A
5	Dwelling, Two-Family		A	A		A		A	A	A	A	A	A
6	Elderly Housing		A	A		A		A	A	A	A	A	A
7	Mixed Use Residential		A	A					A		A		A
8	Accessory Dwelling Units (new subcategory)		A	A		A	A	A	A	A	A	A	A
CIVIC USES													
9	Community Center	A	A	A	A	A		A		A		A	A
10	Public Facilities	A	A	A	A	A		A		A		A	A
11	Public Utilities Structure		A	A									A
12	Public Utilities	A	A	A	A	A	A	A	A	A	A	A	A
13	Recreational Facilities, Public	A	A	A	A	A		A		A		A	A
14	Religious Facilities		A	A					A		A	A	A
15	Cultural Uses and Performing Arts	A	A	A					A		A	A	A
INSTITUTIONAL													
16	Assisted Living		A	A				A		A			A
17	Nursing Home and Accessory Uses		A	A									A
18	Hospital (new subcategory)												A
ACCOMMODATION													
19	Hotels	A	A	A									A
20	Boutique Hotel (new subcategory)		A	A				A		A		A	A
21	Bed and Breakfast Homestay		A	A		A		A		A	A	A	A
COMMERCIAL USES													
22	Business Center Development	A	A	A							A		A
23	Conference/Convention Center		A	A									A
24	Day Care Center, Adult		A	A				A		A			A
25	Drive-Thru Window	A	A	A									A
26	Financial Institution	A	A	A							A		A
27	Flex Uses (new subcategory)	A	A	A									A
28	Education and Training Facilities	A	A	A									A
29	Group Child Care Center	A	A	A					A		A		A
30	Home Occupation		A	A		A	A	A	A	A	A	A	A
31	Membership Club	A	A	A									A
32	Motor Vehicle Station, Limited Service	A	A	A									A
33	Recreation, Commercial	A	A	A				A			A		A
34	Retail Sales Establishment	A	A	A		A			A		A		A
35	Parking Structures (new subcategory)	A	A	A									A
36	Professional Office	A	A	A					A		A		A
37	Dedicated Office Building (new subcategory)		A	A									A
38	Rental Car Satellite	A	A	A									A
39	Repair Services	A	A	A					A		A		A
40	Research or Development Laboratories		A	A									A
41	Restaurant	A	A	A		A					A		A
42	Restaurant, Fast Food	A	A										A
43	Restaurant with Take-out, no Drive-through	A	A	A							A		A
44	Schools, Private and Charter		A	A					A		A		A
45	Service Establishment	A	A	A					A		A		A

A = Allowable Use within Subarea, subject to the Land Use Allocation Summary Table
 = Use not Allowable within Subarea

2.2.3 Land Use Standards

Performance Standards

Accessory Dwelling Unit

To increase housing alternatives while maintaining neighborhood aesthetics and quality, one accessory apartment within a detached single family dwelling shall be permitted provided the following conditions are met:

- A. Maximum of one (1) accessory apartment per lot up to the maximum limit permitted in the Land Use Allocation Summary for the individual Subarea and the Total PUD Maximum;
- B. The property owner must occupy one of the two units;
- C. The exterior appearance and entrances of the dwelling shall be consistent with a single-family residence;
- D. Only one (1) bedroom is permitted in the accessory apartment and to qualify as an accessory apartment under this Section, the apartment may not exceed 750 square feet of floor space.
- E. Garage parking is encouraged;
- F. No accessory apartment shall be permitted on a lot created as part of a back lot development, per the requirements of *Section 3.4.1.3.7 of the Londonderry Zoning Ordinance*;
- G. A dwelling unit that is counted within the Woodmont Commons PUD maximum for Accessory Units.

Cottage Court

- A. Single-family cottage court developments are permitted in all subareas that permit multi-family uses;
- B. Minimum lot sizes shall be waived. This waiver, however, does not provide for increases to the maximum number of residential units shown for Subareas in the Land Use Density Table on pages 68-69;

- C. Cottage units shall be oriented to front, and have a main entry onto, the common open space. Lots abutting a street may front the street. The rear of a cottage unit may face an Access Lane. Cottage units fronting the street shall have a secondary entrance onto the common open space;
- D. Front, rear and side setbacks from the property lines shall be a minimum of three feet;
- E. Cottage units abutting a public street shall meet the setback requirements of *Section 2.4.2 Building and Lots for Single-Family Detached*;
- F. The common open space shall total at least 250 square feet per cottage unit;
- G. Each individual single-family cottage court may contain four to sixteen cottage units in lots around a common open space;
- H. Parking shall be located on the cottage court property and meet the requirements for single-family parking;
- I. Emergency service access shall be maintained to each dwelling unit via the access lane or other method suitable to the authorities having jurisdiction.

Drive-Thru Windows

- A. Drive-Thru Windows may be attached to Financial institutions, Fast Food Restaurants and Service Establishments such as dry-cleaners in WC-1-GL, WC-1, WC-2, and WC-12;
- B. The Drive-Thru must be located behind the building in WC-1 and WC-2.

Flex Uses

- A. Flex Uses are limited to 5,000 square foot per use in Small, Medium and Large Buildings;
- B. Any Flex Use above 5,000 square foot per use must use a Flex Building.

2.2.3 Land Use Standards

Home Occupations, Child Care and Adult Day Care

Home Occupations, Child Care and Adult Day Care as Home Occupations are allowed by special exception under *Sections 3.12.1, 3.12.2 and 3.12.3* of the *Londonderry Zoning Ordinance*. In the Woodmont Commons PUD, these uses are allowable subject to verification of performance standards and without the need for a special exception.

Standards provided in *Sections 3.12.1, 3.12.2 and 3.12.3* of the *Londonderry Zoning Ordinance* shall apply as criteria for the Planning Board's evaluation, provided *Sections 3.12.1.1, 3.12.2.1 and 3.12.3.1* shall not apply because the Planning Board, and not the Board of Adjustment, shall regulate compliance.

Religious Facilities

- A. Religious facilities in residential Subareas must be on lots of two acres minimum and must have traffic access onto a primary road;
- B. All buildings and the parking lot associated with a religious facility will be set back a minimum of 20 feet from all adjacent residential lots, in accordance with *Section 5.02* of the *Londonderry Site Plan Regulations*. The landscaping of this setback buffer zone will be approved by the Planning Board;
- C. Parking for religious facilities shall be determined by the requirements of *Section 3.10, Table 1* of the *Londonderry Zoning Ordinance*.

Rental Car Satellite Office

- A. A rental car satellite office includes the vehicle leasing offices and associated automobile parking but does not include vehicle maintenance, repair, preparation or cleaning (other than minor exterior washing);
- B. The accessory automobile parking areas shall be limited to passenger vehicles, trucks and vans amounting to no more than the storage of 25 rental vehicles and associated office parking.

2.2.3 Land Use Standards

Land Use Density Table

The Allowable Densities Table includes maximum amounts of development and minimum amounts of Open Space by Subarea. The development maximums for a “No Exit 4A” scenario are included to the far right and serve as the limit on development until Exit 4A is constructed.

SUBAREA DEVELOPMENT STANDARDS

LAND USE CATEGORIES		UNIT OF MEASURE	Total PUD MAXIMUM DEVELOPMENT	WC-1-GL	WC-1	WC-2	WC-3	WC-4	WC-5
Total Land Area		Acres		38	77	51	39	9	9
Agricultural		All Types	N/A						
Residential	New Primary Residence	Units	1,300		535	67		18	10
	New Accessory Units	Units	130		15	20		10	1
	Existing Units	Units	9		3	1			
	Maximum Total	Units	1,439		553	88		28	11
Institutional	Hospital ¹	Beds/GSF	300/250,000						
	Nursing Homes and Assisted Living	GSF	250,000		20,000	20,000			
	Maximum Total	GSF	500,000						
Civic	All Types ²								
Accommodations	Hotels ³	No./Rooms	3/550		200	150			
	Boutique Hotels ⁴	Rooms	100						
Commercial Use	New Commercial Uses	GSF	882,500	50,000	362,500	100,000		5,000	
	Office Building Space	GSF	700,000		50,000	300,000			
	Existing Commercial Uses	GSF	272,000	225,000	47,000				
	Maximum Subtotal	GSF	1,854,500	275,000	459,500	400,000		5,000	
Flexibility Factors *				15%	15%/30%*	30%	15%	15%	15%

SUBAREA OPEN SPACE AND BUFFER STANDARDS

OPEN SPACE AND BUFFER CATEGORIES		UNIT OF MEASURE	Total PUD MINIMUM AREA	SUBAREA OPEN SPACE AND BUFFER STANDARDS					
Green and Open Space⁵	Conserved Green Space	Acres	89		1.0	2.0	39.0		
	Shared Open Space	Acres	64	1.5	10.5	2.0		2.5	
	Total	Acres	152	1.5	11.5	4.0	39.0	2.5	
Perimeter Buffers		Acres	38		1.6	1.4		1.0	2.6

Footnotes

1. Hospital: substitution of the maximum area allocated for Hospital use will require a PUD Master Plan amendment.
2. Civic: Civic uses have no area limitation, subject to traffic and utilities mitigation as described in the Development Agreement.
3. Hotels: A maximum of three hotels is permitted in the PUD. The total of all Hotel rooms within the PUD may not exceed 550 rooms.
4. Boutique Hotel: The total of all Boutique Hotel rooms within the PUD may not exceed 100 rooms.
5. At least one half acre of Shared Open Space or publicly accessible Conserved Green Space must be provided within one quarter mile of the front door of any approved building on a PUD Subdivision or PUD Site Plan application. At least one half acre of Active Open Space must be provided within one half mile of the front door of any approved building on a PUD Subdivision or PUD Site Plan application. Publicly accessible Shared Open Space and Conserved Green Space must be connected to a street or sidewalk network.

The footnotes to this table explain how to apply flexibility rules across Subareas and how to interpret them. Note that the Total PUD Minimum Area must be achieved at build-out but that the Subarea minimum areas have some flexibility in their location only in that they may be contiguous with or partially located in adjacent Subareas if the revised location works better with the development plan and is in accordance with the Shared Open Space and Conserved Green Space Standards.

LAND USE CATEGORIES		SUBAREA DEVELOPMENT STANDARDS							MAXIMA WITHOUT Exit 4A	
		WC-6	WC-7	WC-8	WC-9	WC-10	WC-11	WC-12	West (WC-1-GL -WC-11)	East (WC-12)
Total Land Area		13	23	70	32	17	14	216		
Agricultural	All Types									
Residential	New Primary Residence	12	24	190	40	50	24	330	970	330
	New Accessory Units	4		40	10	10		20	110	20
	Existing Units				2			3		
	Maximum Total	16	24	230	52	60	24	353	1,080	350
Institutional	Hospital ¹							250,000	-	-
	Nursing Homes and Assisted Living			20,000		20,000		170,000		
	Maximum Total							420,000	-	-
Civic	All Types ²									
Accommodations	Hotels ³							200	350	-
	Boutique Hotels ⁴									
Commercial Use	New Commercial Uses			5,000		10,000		350,000	532,500	
	Office Building Space							350,000	300,000	400,000
	Existing Commercial Uses									
	Maximum Subtotal			5,000		10,000		700,000	832,500	400,000
Flexibility Factors *		15%	15%	15%	15%	15%	15%	30%		

OPEN SPACE AND BUFFER CATEGORIES		SUBAREA OPEN SPACE AND BUFFER STANDARDS						
Green and Open Space⁵	Conserved Green Space			10.0				36.5
	Shared Open Space	5.0		1.0	0.5	0.5		40.0
	Total	5.0		11.0	0.5	0.5		76.5
Perimeter Buffers		1.1	5.7	1.9	6.6		2.9	13.6

* Flexibility Factors

1. Development maximums within each Subarea may exceed each indicated amount in accord with the specified flexibility factor, i.e., 15% or 30%. Such increases would require a proportionate decrease in allocations for the same use elsewhere within the PUD, and remain subject to site-specific mitigation reviews during the PUD Site Plan and PUD Subdivision phase.
2. In WC-1 only, Residential is limited to a 15% Flexibility Factor. All other uses in WC-1 have a 30% Flexibility Factor.
3. Allowable Area may be exchanged between Nursing Homes and Assisted Living, Accommodations and Commercial Uses on a per square foot basis, subject to review and confirmation that increases are consistent with the MTIA and all applicable improvement and mitigation standards, or other basis that confirms that no net impact will occur off-site due to the change.
4. Shared Open Space and Conserved Green Space may be provided at the PUD Subdivision and/or PUD Site Plan level in an adjoining Subarea as long as the proposed project meets the proximity requirements on the previous page.
5. Total PUD Maximum Development may not be exceeded.
6. When using the Flexibility Factors applicable to other Subareas, the number of allowable residential units in WC-12 may not be decreased by more than 30%.

2.2.3 Land Use Standards

Conserved Green and Shared Open Space Land Use Standards

Open space is an integral part of the overall Land Use Plan for the Woodmont Commons PUD, which will include areas of publicly accessible open space. Walking trails will run throughout many areas of the site. Dwellings will also be in close proximity to a park or green, and many of the streets will be designed to be linear parks for pedestrians and bicyclists. The publicly accessible open space may be dedicated to a municipal or non-profit entity. Shared Open Space and Conserved Green Space are defined in *Section 2.1.5 PUD Definitions*.

Shared Open Space standards at the PUD Subdivision level include publicly accessible open space, natural vegetation and features, and recreational areas. Shared Open Space includes three categories described in this Section: Civic Open Space, Active Open Space and Passive Open Space. These categories are always publicly accessible and may be independent or combined together with other categories to meet the minimum requirements for each Subarea as shown in the Allowable Densities Table on the previous two pages.

Agricultural Open Space, Perimeter Buffers and Conserved Green Space may be publicly accessible or may be on private land. Agricultural Open Space is a category described in this Section and there are neither minimum requirements nor maximum limitations on this category. Perimeter Buffers are defined in *Section 2.1.5 PUD Definitions* and are a separate line in the Allowable Densities Table. Unless the Perimeter Buffer is contiguous with other publicly accessible space, it is not counted toward the Shared Open Space minimums in the Allowable Densities Table.

Conserved Green Space shall be provided within the Woodmont Commons PUD and there are minimum requirements by Subarea as shown in the Allowable Densities Table above. Development of Conserved Green Space is limited to Recreation Trails and Shared Use Trails, as defined in Open Space Types.

Infrastructure also includes open space, such as medians. A description of infrastructure-related open space is included in *Section 2.3.9 PUD Subdivision Landscape*. Standards at the PUD Subdivision level include defining location and access requirements.

Conserved Green Space and Open Space requirements are contained within *Section 2.4 PUD Site Plan Regulations and Standards*.

Each PUD Subdivision submittal shall provide as follows:

- All PUD Subdivision and/or PUD Site Plan submittals must indicate the types and locations of Conserved Green Space and Shared Open Space(s) within the submittal in general accordance with the Land Use Plan, if applicable. The locations may be adjusted according to the PUD Subdivision requirements.
- At least one half acre of Shared Open Space or publicly accessible Conserved Green Space must be provided within one quarter mile of the front door of any approved building. At least one half acre of Active Open Space must be provided within one half mile of the front door of any approved building.
- Publicly accessible space must be connected to a street, sidewalk network or path.
- Shared Open Space may be constructed at the PUD Subdivision or the PUD Site Plan level.

2.2.3 Land Use Standards

Conserved Green and Shared Open Space Examples: Publicly Accessible Categories

Civic Open Space

Open space intended to serve as a gathering or activity space for civic purposes and is available for public access. Civic purposes include walking, sitting, small gatherings or special events. Civic open space may include adjacent sidewalks, pathways, structures and permanent or temporary accessory uses intended primarily to invite access and use.



Active Open Space

Open space that is intended to support organized, formally defined recreational activities such as playgrounds and recreational fields and is available for public access. Active recreation open space may include pathways, parking areas, structures or accessory uses intended primarily to provide access or support the active use of the open space.



Passive Open Space

Open space that is predominately landscaped with plant materials, available for public access, and adaptable to a range of informal recreational activities including walking and bicycling. Passive recreation open space may include pathways, parking areas, structures or accessory uses such as shared or community gardens intended primarily to provide access or support the passive use of the open space.



2.2.3 Land Use Standards

Conserved Green and Shared Open Space Examples: Public or Private Categories

Agricultural Open Space

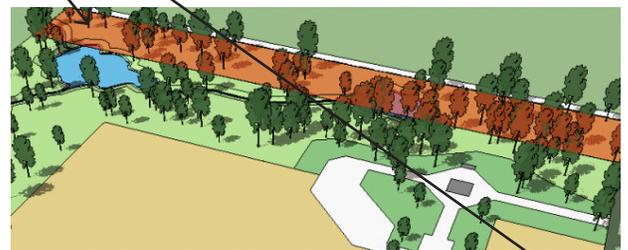
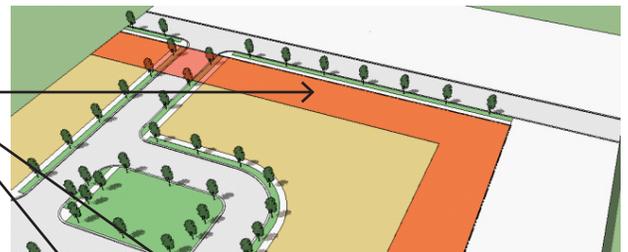
Open space that is used for commercial agricultural production. This type of open space is not intended for public access, but could include such access. Agricultural Open Space may include parking, circulation roads or paths, structures and activities directly associated with the agricultural purposes. A Community Supported Agriculture operation would fall under this category.



Perimeter Buffers

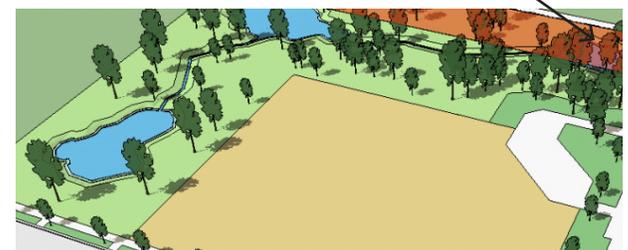
Building setback buffers along designated perimeters of the Woodmont Commons PUD that are predominately planted, but are not required to be publicly accessible. Where a buffer is connected to Shared Open Space (and thus publicly accessible), it may also be used to satisfy the Shared Open Space minimum requirements.

Perimeter Buffers are shown in orange in the illustrations to the right and below.



Conserved Green Space

Open space that retains or provides natural features that may have public access to the extent consistent with the conservation goals and applicable regulations, and as specified in the requirements within the PUD Master Plan. This may include regulated open space such as designated wetland areas subject to state and federal regulations; areas that provide shared stormwater drainage and water quality features serving multiple properties; and intermittent or perennial streams or ponds.



2.2.4 Subarea Composition Principles and Standards

As noted in *Section 2.1.1 Description*, the Subarea Composition Principles and Standards are an integral part of each PUD Subdivision and/or PUD Site Plan submittal and establish guiding principles and standards by which the Planning Board may review an application.

The following pages provide the intent, the allowable regulatory Types, the Subarea Land Use Plan and the composition standards and principles for each Subarea. Each Subarea has three pages.

The first page describes the intent of the Subarea and the allowable Street, Block, Open Space and Building/Lot Types (described in *Section 2.3 PUD Subdivision Regulations and Standards*). The Types allowable in each Subarea are consistent with its intent, so that the Street Types correspond to the Block Types while the Building/Lot Types correspond to the Block Types and reflect the uses allowable for that Subarea as shown in the Allowable Uses Table. Open Space Types are consistent with the development pattern that results from the combination of Street, Block and Building/Lot Types and create the conditions required by *Section 2.2.3 Land Use Standards*.

The second page for each Subarea is a detailed look at the Land Use Plan with the approximate locations of Primary streets, Secondary streets, conserved Green Space and Shared Open Space. The Subarea Development Densities are also listed.

The third page for each Subarea provides the principles and standards for the Subarea. The Subarea Principles for each Subarea are consistent with its intent and provide guidance to developers (for design and planning), Town Staff (for review) and the Planning Board (for review and approval) in the design of PUD Subdivisions and PUD Site Plans for that Subarea.

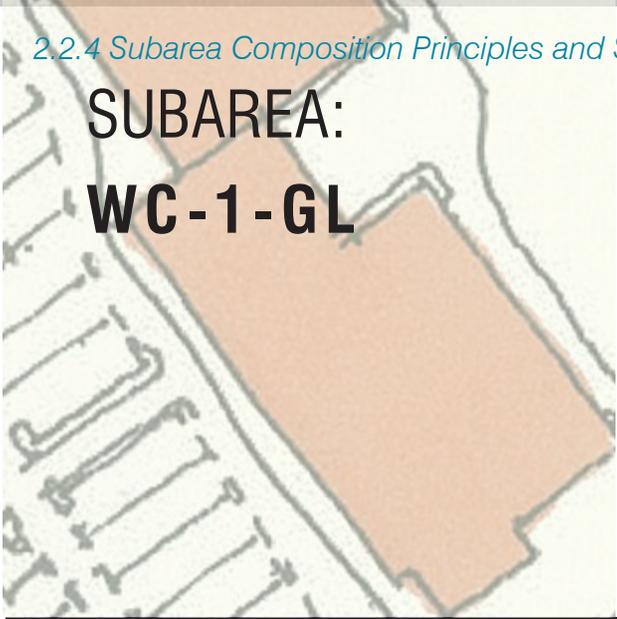
The Subarea Standards are requirements that must be met as part of PUD Subdivision and/or PUD Site Plan approval. These Subarea Standards are broad requirements for the design of streets, pedestrian and bicycle networks, and parking and open spaces. *Section*

2.3 PUD Subdivision Regulations and Standards and *Section 2.4 PUD Site Plan Regulations and Standards* provide additional, specific standards for proposed developments as well as specific parking, lighting, landscape and stormwater standards.

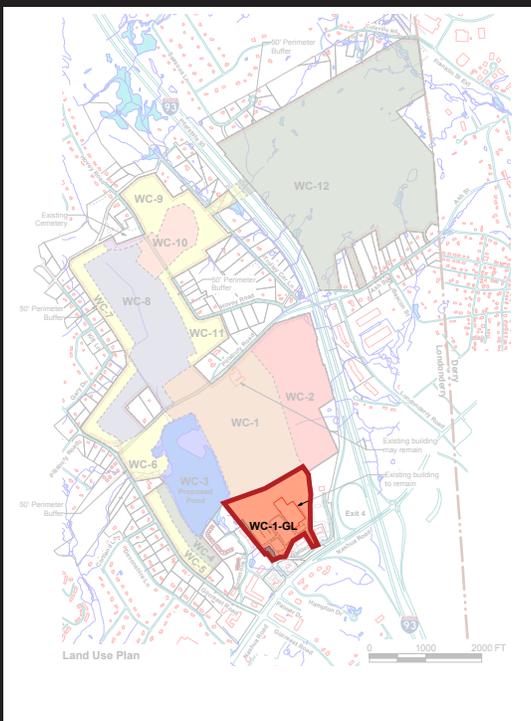
Note that the Compliance Alternatives in *Section 2.2.1 Description* apply to the Subarea Composition Principles and Standards.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-1-GL**



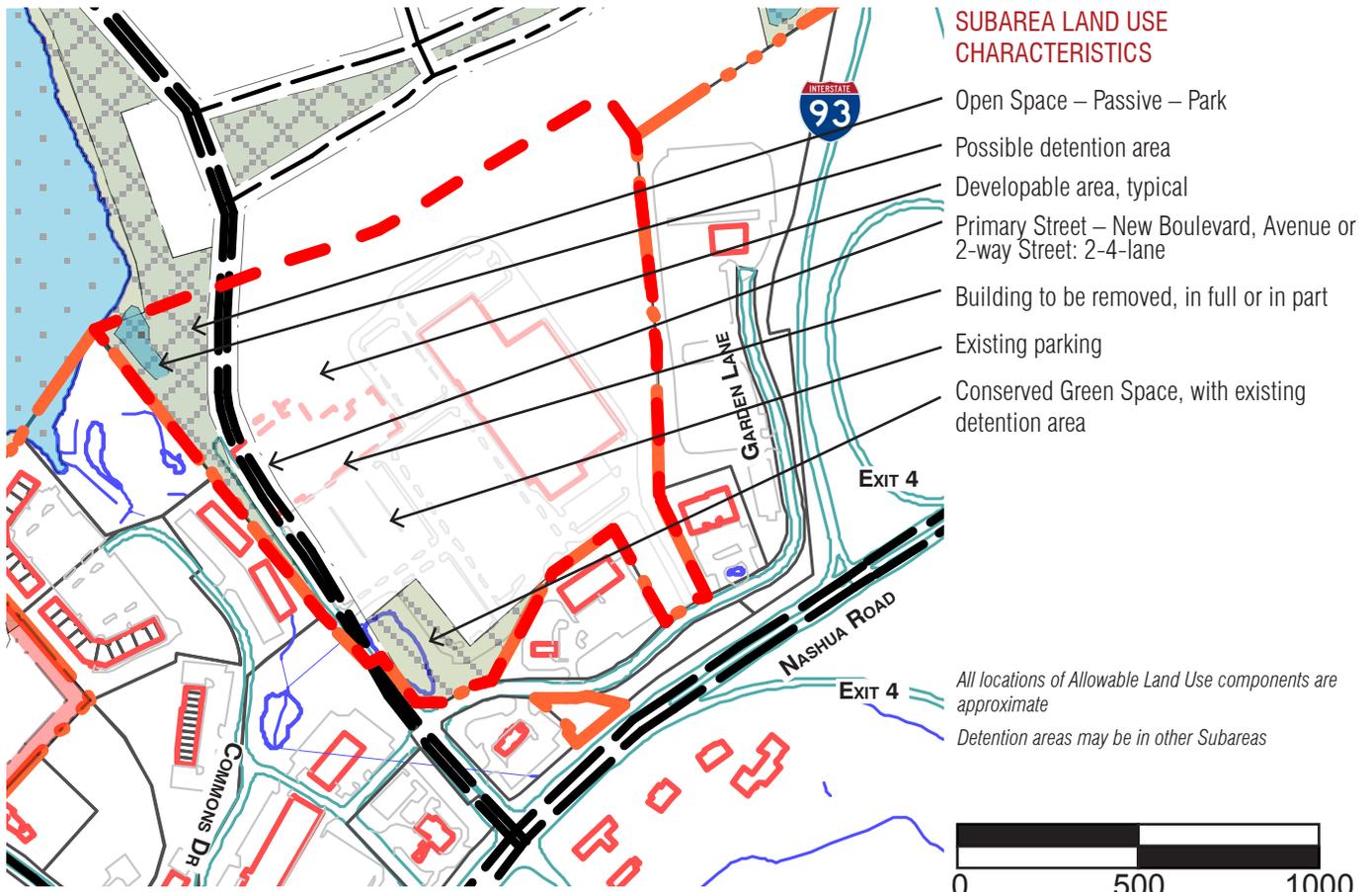
INTENT. The purpose of this Subarea is to create a gateway into the Woodmont Commons PUD and allow access to existing and new retail and commercial uses that serve the Woodmont Commons PUD and the region. This Subarea provides an extension of the commercial uses along Nashua Road.



Regulating Type	Allowed
STREET TYPES (SECTION 2.3.3)	
1 Boulevard	A
2 Avenue	A
3 Two-way Street	A
4 One-way Street	A
5 Access Lane	A
BLOCK TYPES (SECTION 2.3.2)	
1 Village Center	A
2 Neighborhood	
3 Flex	A
4 Perimeter	A
5 Perimeter Neighborhood	
OPEN SPACE TYPES (SECTION 2.3.6)	
1 Conserved Green Space	A
2 Green	A
3 Square	A
4 Plaza	A
5 Playing Field	
6 Playground	A
7 Park	A
8 Shared Use Path	A
9 Recreation Trails	A
BUILDING AND LOT TYPES (SECTION 2.4.2)	
1 Single Family Detached	
2 Single-Family Attached	
3 Two-Family	
4 Multi-family	
5 Small Building	A
6 Medium Building	A
7 Large Building	A
8 Civic Building	A
9 Dedicated Office Building	
10 Large Format Retail Building	A
11 Institutional Building	
12 Parking Structure	A
13 Flex Building	A

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA WC-1-GL

Approximate locations of the allowable Land Use components.

SUBAREA DEVELOPMENT

Subarea Development Maximums Total	
AGRICULTURAL	NO LIMIT
RESIDENTIAL	NOT ALLOWABLE
INSTITUTIONAL	NOT ALLOWABLE
CIVIC	NO LIMIT
ACCOMMODATIONS	NOT ALLOWABLE
COMMERCIAL USE	275,000 GSF

Subarea Open Space Minima Total	
SHARED OPEN SPACE	0
CONSERVED GREEN SPACE	1.5

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards

SUBAREA: WC-1-GL

Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will be organized so that it can support a regional shopping draw within the overall Woodmont Commons PUD. The overall composition of streets, blocks and open spaces will include the following principles:

- Compatible Use – This is an area that is restricted to retail, Flex Space, commercial uses and mixed-use to be compatible with existing adjacencies.
- Expanding the current development patterns for retail, commercial uses and mixed-use – The development patterns should support vehicular access to the large format retail, Flex Space, mixed-use and commercial uses within this Subarea.
- Well-connected pedestrian-oriented access to the Woodmont Commons PUD – Sidewalks along the primary street will facilitate access to publicly accessible open space and adjoining Subareas.
- Frontages along the Secondary Network – The public frontages along the secondary network should be consistent with the primary intended ground level use and its relationship to off-street parking. For segments bordering off-street parking, landscaping should be consistent with landscaped screening for the parking lots. For segments with commercial, retail, mixed-use or civic uses, planting strips should be limited to where new streets are provided. Sidewalk paving may extend to the street edge.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK The Primary Streets are as identified on the Land Use Plan on the preceding page and as may be confirmed or modified by Supplemental TIAs. In this Subarea, the primary street network will be landscaped or have pedestrian oriented commercial uses. The network provides a north/south connection from Pillsbury Road to Garden Lane which may be a boulevard with a central landscaped median and flanking street trees, sidewalks or shared paths on both sides. Parking is not required along this segment of the primary network, but may be provided in either pockets or complete segments.

SECONDARY STREET NETWORK The secondary street network should be designed to allow vehicular access to the commercial uses. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- Frontages along the Primary Network – The public frontages should be landscaped to reinforce the characteristics of the network in this area.

- Frontages along the Secondary Network – The public frontages along the secondary network should be consistent with the primary intended ground level use and its relationship to off-street parking. For segments bordering off-street parking, landscaping should be consistent with landscaped screening for the parking lots. For segments with commercial, retail, mixed-use or civic uses, planting strips should be limited to where new streets are provided. Sidewalk paving may extend to the street edge.

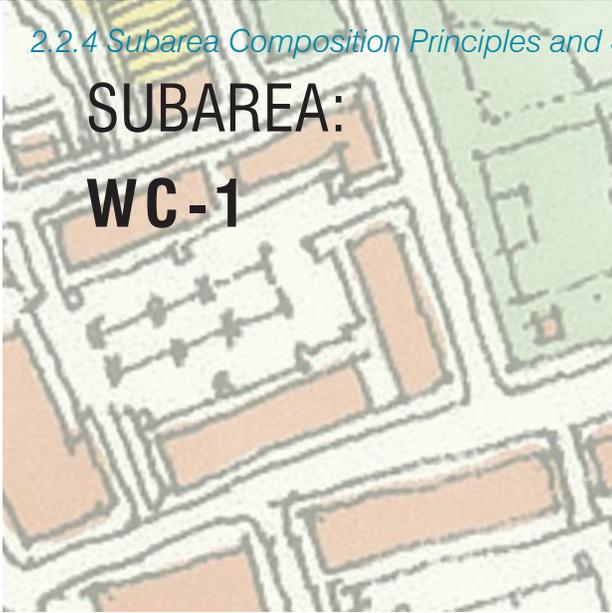
PEDESTRIAN NETWORK Continuous pedestrian networks will be required with sidewalks on one or both sides of all Primary and Secondary Streets within this Subarea. The pedestrian network should connect uses with open space.

BICYCLE NETWORK Accommodations for bicycles will be provided along or parallel to the Primary Street Network. Shared use of streets will be permitted for all other portions of this Subarea.

PARKING This Subarea will require off-street parking for development in this area. Off-street parking should have landscape screening along the public frontage or be placed to the rear of buildings.

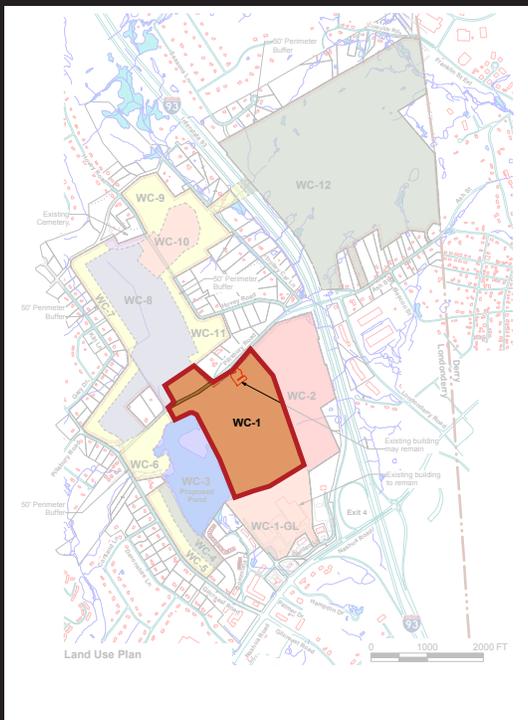
OPEN SPACES Shared open space should be comprised of at least one (1) separate location and one (1) separate publicly-accessible open space type as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards



**SUBAREA:
WC-1**

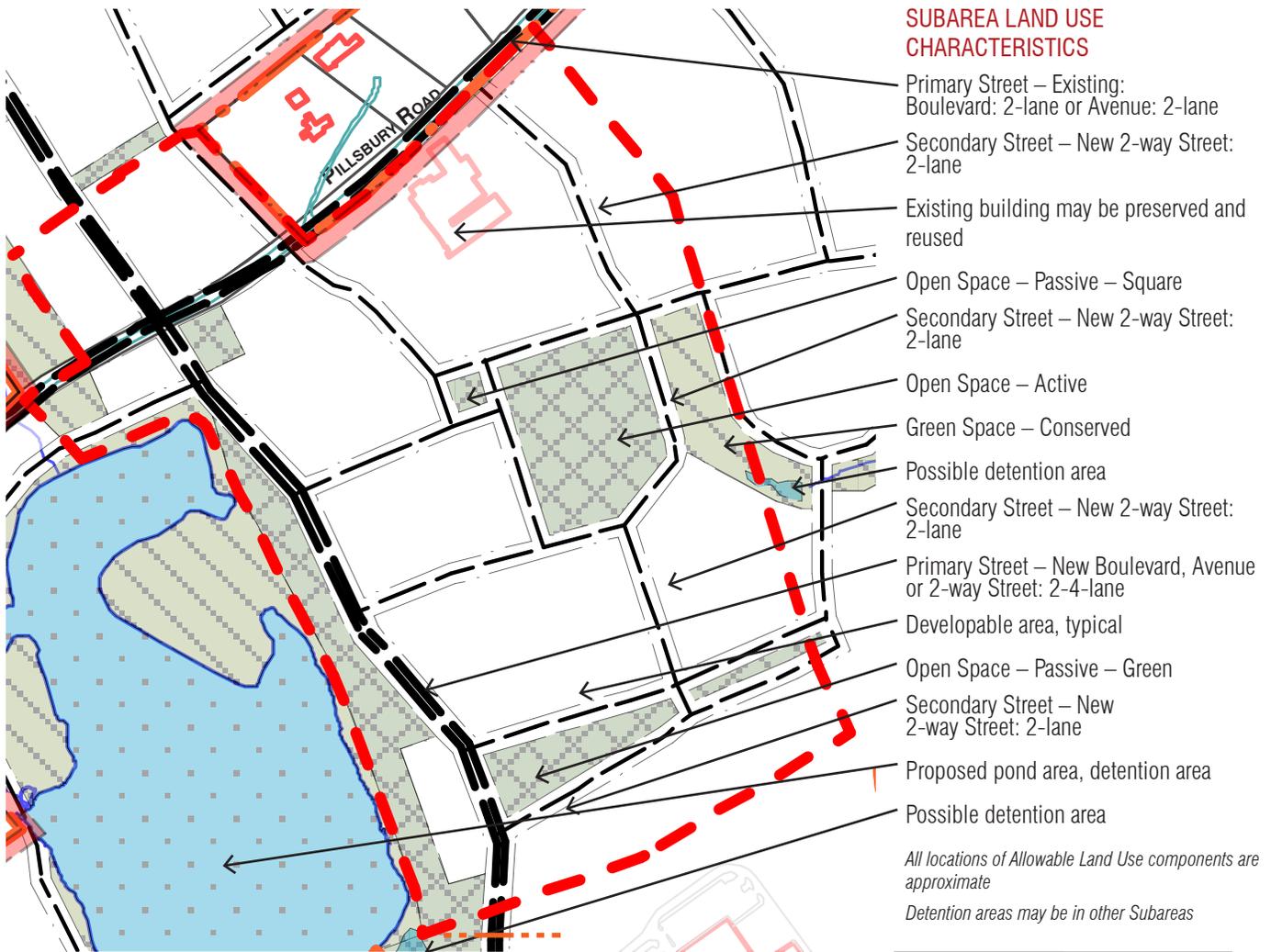
INTENT. This purpose of this Subarea is to create a compact village center with a mix of uses that support diverse building lots and open spaces. The development pattern supports well-connected pedestrian-oriented segments along the streets and sidewalks, lined by residential, retail, mixed-use and commercial uses.



Regulating Type	Allowed
STREET TYPES (SECTION 2.3.3)	
1 Boulevard	A
2 Avenue	A
3 Two-way Street	A
4 One-way Street	A
5 Access Lane	A
BLOCK TYPES (SECTION 2.3.2)	
1 Village Center	A
2 Neighborhood	A
3 Flex	A
4 Perimeter	A
5 Perimeter Neighborhood	A
OPEN SPACE TYPES (SECTION 2.3.6)	
1 Conserved Green Space	A
2 Green	A
3 Square	A
4 Plaza	A
5 Playing Field	A
6 Playground	A
7 Park	A
8 Shared Use Path	A
9 Recreation Trails	A
BUILDING AND LOT TYPES (SECTION 2.4.2)	
1 Single Family Detached	A
2 Single-Family Attached	A
3 Two-Family	A
4 Multi-family	A
5 Small Building	A
6 Medium Building	A
7 Large Building	A
8 Civic Building	A
9 Dedicated Office Building	A
10 Large Format Retail Building	A
11 Institutional Building	A
12 Parking Structure	A
13 Flex Building	A

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA WC-1

SUBAREA DEVELOPMENT

Subarea Development Maximums Total	
AGRICULTURAL	NO LIMIT
RESIDENTIAL	553 UNITS
INSTITUTIONAL	20,000 GSF
CIVIC	NO LIMIT
ACCOMMODATIONS	200 HOTEL ROOMS
COMMERCIAL USE	459,500 GSF

Subarea Open Space Minima Total	
SHARED OPEN SPACE	10.5
CONSERVED GREEN SPACE	1.0

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area



2.2.4 Subarea Composition Principles and Standards

SUBAREA: WC-1

Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will be organized so that it can support a variety of different uses assembled to create a compact village center within the overall Woodmont Commons PUD. The composition of streets, blocks and open spaces will include the following principles:

- **Variety** – This is an area that will benefit from a diversity of building lots, blocks, open spaces and the mix of uses. The division of land and infrastructure should support this variety.
- **Diversity of open spaces** – There are a range of open space types that are located within the area, to provide different types of opportunities and amenities.
- **Compact and connected development patterns for retail, commercial uses and mixed-use** – The development patterns should support compact and well-connected pedestrian-oriented segments along the streets and sidewalks at the village center, connected to the primary street network, and lined by retail, Flex Space, commercial uses or mixed-use. The scale and intensity transition to a more residential scale closer to the PUD perimeter and more residential Subareas.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK The Primary Streets are as identified on the Land Use Plan on the preceding page and as may be confirmed or modified by Supplemental TIAs. In this Subarea, the primary street network provides a north/south connection from Pillsbury Road to Garden Lane which may be a boulevard with a central landscaped median and flanking street trees, sidewalks or shared paths on both sides. Parking is required along this segment of the primary network, but may be provided in either pockets or complete segments.

SECONDARY STREET NETWORK The secondary street network should be designed to create developable blocks or to outline planned open space, and incorporate on-street parking where it can serve as a shared parking resource for business, civic, accommodation, residential or institutional uses. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- **Frontages along the Primary Network** – Except for areas where there is on-street parking in segments or in pockets, the public frontages will have street trees to reinforce the characteristics of the network in this area.
- **Frontages along the Secondary Network** – The public frontages along the secondary network should be consistent with the primary intended ground level use and its relationship to on-street parking. For segments intended to have predominately business, civic, accommodation or institutional uses with short term, on-street parking, planting strips should be limited and sidewalk paving generally extended to the street edge. For predominately residential segments, the frontages should be landscaped, with sidewalks off of the street edge.
- **Frontages along Pillsbury Road** – The public frontages along Pillsbury Road should include a planting strip between the street and the sidewalk next to retail, mixed use or commercial uses.

PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Primary and Secondary Streets within this Subarea. Curb extensions or other traffic calming devices may be provided at Secondary Network intersections serving blocks or frontages intended to serve business, civic accommodation or institutional uses. The pedestrian network should connect uses with open space.

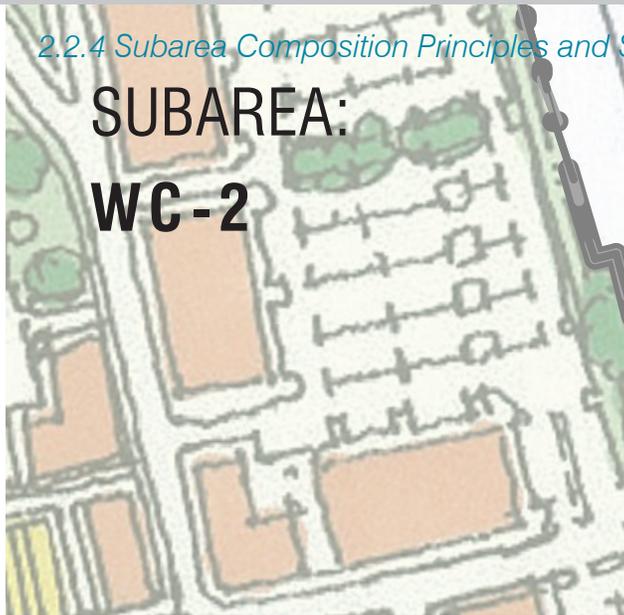
BICYCLE NETWORK Accommodations for bicycles will be provided along or parallel to (within Open Space) the Primary Street Network. Shared use of streets will be permitted for all other portions of this Subarea.

PARKING On-street parking may be considered to contribute to fulfilling the parking requirements of commercial or mixed-use development within 400 feet of the parking spaces for non-residential uses. For residential uses, on-street parking may be allocated for required visitor spaces. Off-street parking should have landscape screening along the public frontage if it fronts on a public street. District parking may be used for off-street parking.

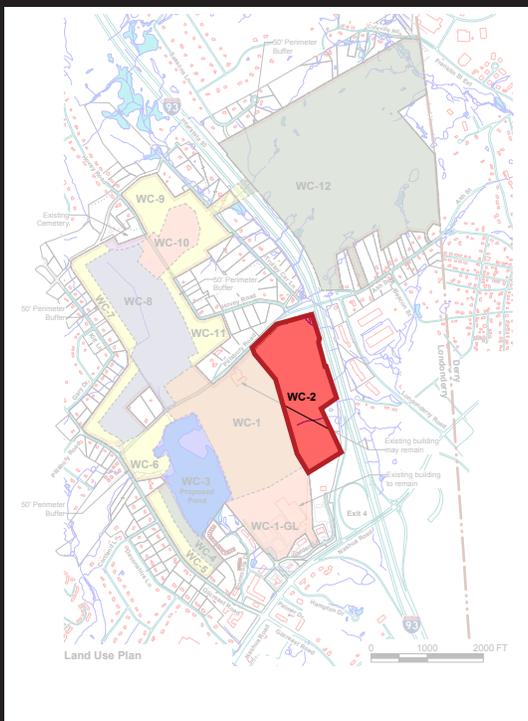
OPEN SPACES Shared open space should be comprised of at least three (3) separate locations and two (2) separate publicly-accessible open space types as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-2**



INTENT. The purpose of this Subarea is to create a mixed use district that provides a zone of Dedicated Office buildings and other commercial/mixed-use buildings along the border of I-93 that separates I-93 from residential neighborhoods within the Subarea. The development pattern supports pedestrian-oriented segments along Pillsbury Road and within the residential neighborhoods of the Subarea while providing vehicular access to the larger buildings along the I-93 perimeter.



Regulating Type

Allowed

STREET TYPES (SECTION 2.3.3)

1	Boulevard	
2	Avenue	A
3	Two-way Street	A
4	One-way Street	A
5	Access Lane	A

BLOCK TYPES (SECTION 2.3.2)

1	Village Center	A
2	Neighborhood	A
3	Flex	A
4	Perimeter	A
5	Perimeter Neighborhood	A

OPEN SPACE TYPES (SECTION 2.3.6)

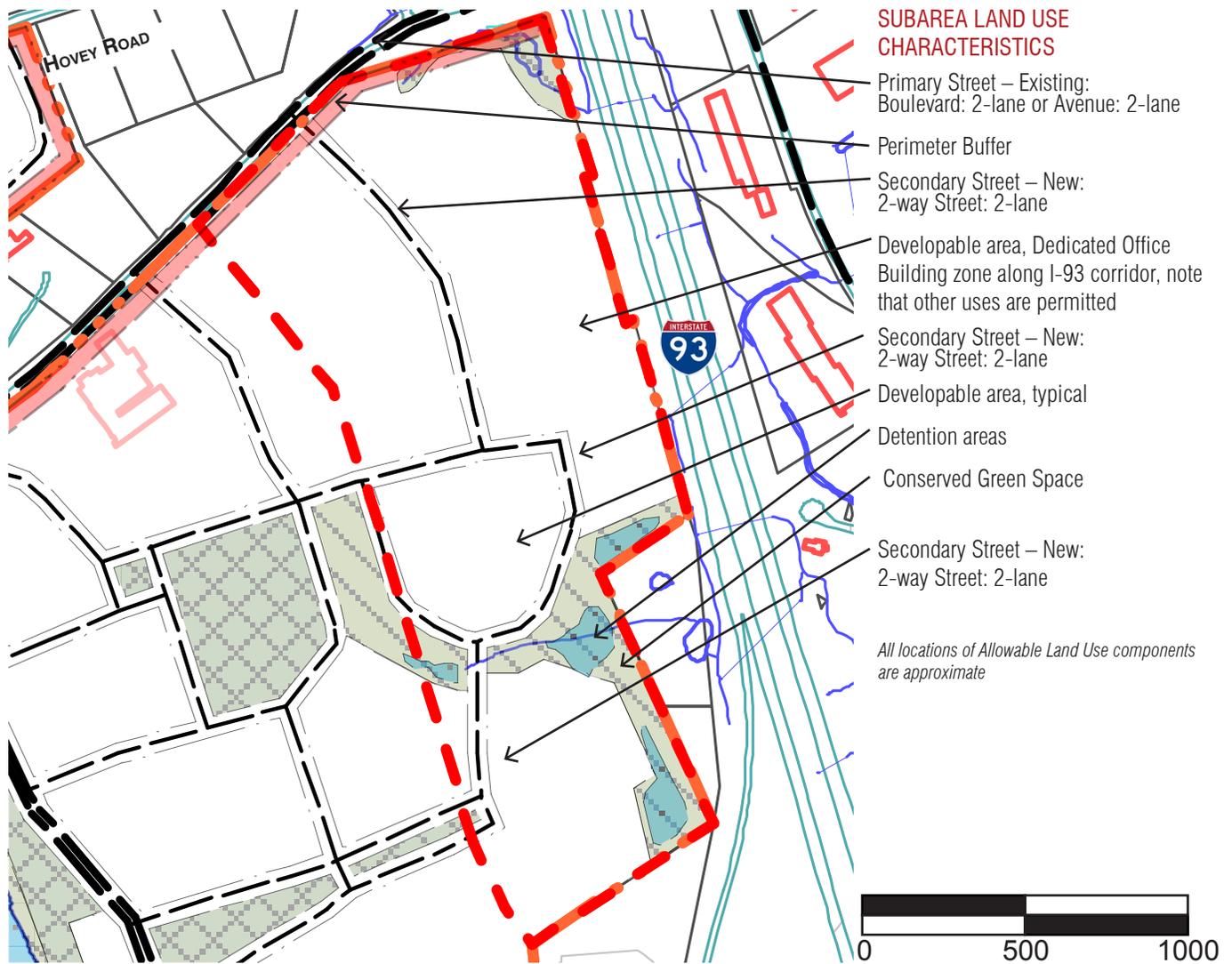
1	Conserved Green Space	A
2	Green	A
3	Square	A
4	Plaza	A
5	Playing Field	A
6	Playground	A
7	Park	A
8	Shared Use Path	A
9	Recreation Trails	A

BUILDING AND LOT TYPES (SECTION 2.4.2)

1	Single Family Detached	A
2	Single-Family Attached	A
3	Two-Family	A
4	Multi-family	A
5	Small Building	A
6	Medium Building	A
7	Large Building	A
8	Civic Building	A
9	Dedicated Office Building	A
10	Large Format Retail Building	A
11	Institutional Building	A
12	Parking Structure	A
13	Flex Building	A

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA WC-2

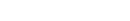
SUBAREA DEVELOPMENT

Subarea Development Maximums Total	
AGRICULTURAL	NO LIMIT
RESIDENTIAL	88 UNITS
INSTITUTIONAL	20,000 GSF
CIVIC	NO LIMIT
ACCOMMODATIONS	150 HOTEL ROOMS
COMMERCIAL USE	400,000 GSF

Subarea Open Space Minima Total	
SHARED OPEN SPACE	2.0
CONSERVED GREEN SPACE	2.0

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

-  PUD Perimeter
-  Subarea Boundary
-  Subdivision Lot Line
-  Primary Street
-  Secondary Street
-  Shared Open Space
-  Conserved Green Space
-  Existing Buildings
-  Developable Area

2.2.4 Subarea Composition Principles and Standards



Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will be organized so that it can support a variety of different uses assembled to provide a transition from a compact area within the overall Woodmont Commons PUD to a zone of office and other buildings adjacent to I-93. The overall composition of streets, blocks and open spaces will include the following principles:

- Variety – This is an area that will benefit from a diversity of building lots, blocks, open spaces and the mix of uses, and the division of land and infrastructure should support this variety.
- Diversity of open spaces – There are a range of open space types that are located within the area to provide different types of opportunities and amenities.
- Development along I-93 – The development patterns may support large buildings for offices, Flex Space or other uses that transition in use and scale from the boundary with I-93 to more residential uses. Parking can be placed between the buildings and this boundary.
- Compact and connected development patterns for retail, commercial uses and mixed-use – The development pattern within this Subarea should integrate with that of WC-1.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK The Primary Streets are as identified on the Land Use Plan on the preceding page and as may be confirmed or modified by Supplemental TIAs. In this Subarea, Pillsbury Road is the primary street network. Parking is not required along this segment of the primary network.

SECONDARY STREET NETWORK The secondary network should be designed to create developable blocks or to outline planned open space, and incorporate on-street parking where it can serve as a shared parking resource for commercial use, retail, mixed-use, civic, accommodation, residential or institutional uses. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- Frontages along the Primary Network – the public frontages will have street trees to reinforce the characteristics of the network in this area.
- Frontages along the Secondary Network – The public frontages along the secondary network should be consistent with the primary intended ground level use and its relationship to on-street parking. For segments intended to have predominately commercial use, retail, mixed-use, civic, accommodation, residential or institutional uses with short term, on-street parking, planting strips should be limited and sidewalk paving generally extended to the street edge. For predominately residential segments or areas where on-street parking is not expected to serve as a short-term supply, the frontages should be landscaped.
- Frontages along Pillsbury Road – The public frontages along Pillsbury Road should include a planting strip between the street and the sidewalk next to retail, mixed use or commercial uses.

PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Primary and Secondary Streets within this Subarea. Curb extensions or other traffic calming devices may be provided at Secondary Network intersections serving blocks or frontages intended to serve business, civic accommodation or institutional uses. The pedestrian network should connect uses with open space.

BICYCLE NETWORK Accommodations for bicycles will be provided along or parallel to the Primary Street Network. Shared use of streets will be permitted for all other portions of this Subarea.

PARKING On-street parking may be considered to contribute to fulfilling the parking requirements of commercial or mixed-use development within 400 feet of the parking spaces for non-residential uses. For residential uses, on-street parking may be allocated for required visitor spaces. Off-street parking should have landscape screening along the public frontage if it fronts on a public street. District parking may be used for off-street parking.

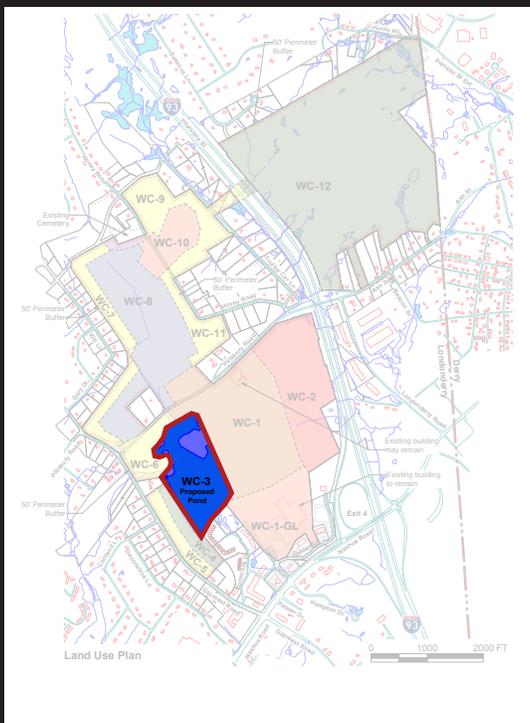
OPEN SPACES Shared open space should be comprised of at least two (2) separate locations and two (2) separate publicly-accessible open space types as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-3**



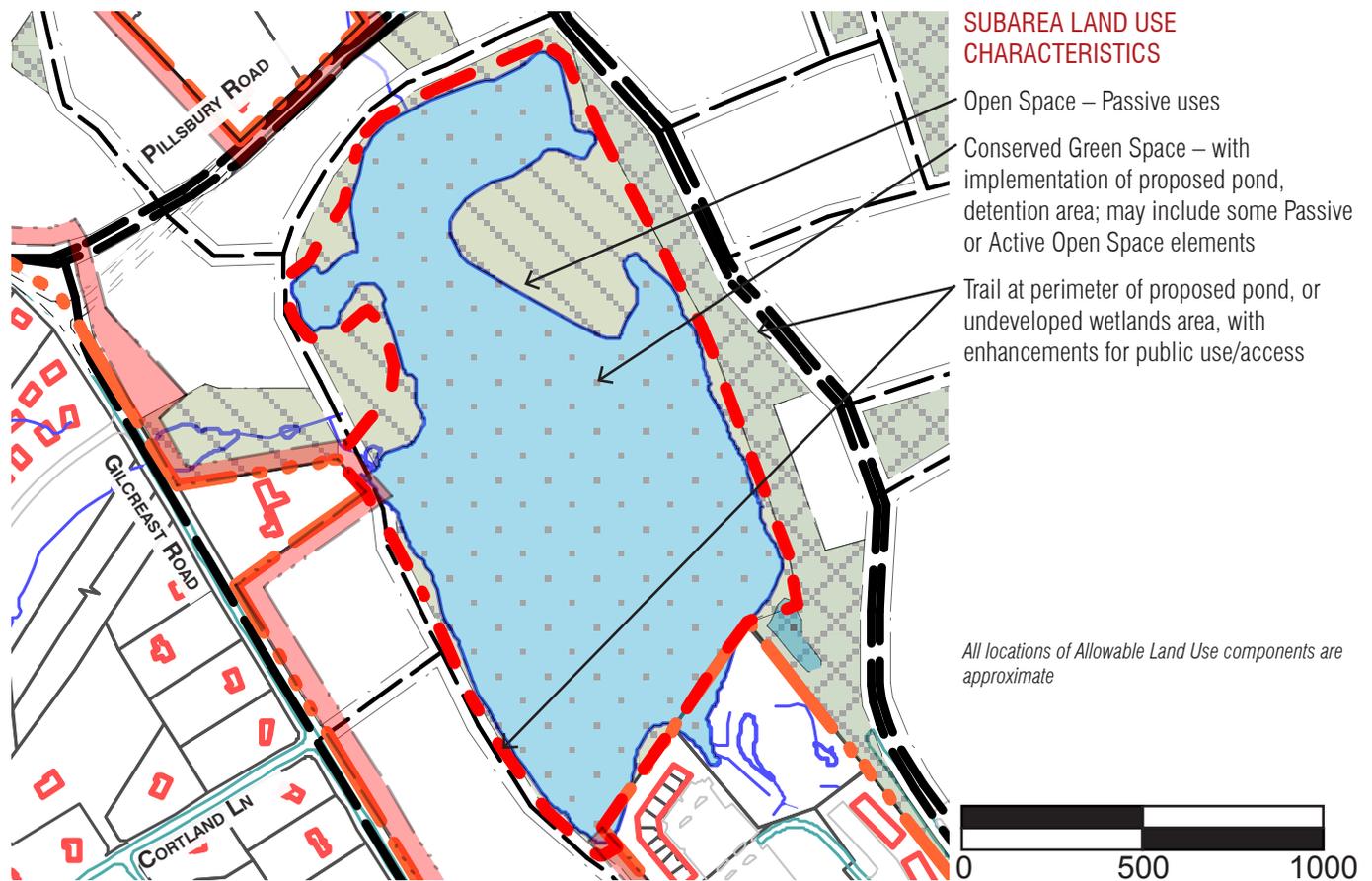
INTENT. The purpose of this Subarea is to preserve significant amounts of open space for public access. Development in this Subarea will be limited to the secondary streets, Recreation Trails and Shared Use Paths that provide public access and accessory buildings that support the use and maintenance of the Subarea.



Regulating Type	Allowed
STREET TYPES (SECTION 2.3.3)	
1 Boulevard	
2 Avenue	
3 Two-way Street	
4 One-way Street	
5 Access Lane	A
BLOCK TYPES (SECTION 2.3.2)	
1 Village Center	
2 Neighborhood	
3 Flex	A
4 Perimeter	
5 Perimeter Neighborhood	
OPEN SPACE TYPES (SECTION 2.3.6)	
1 Conserved Green Space	A
2 Green	A
3 Square	A
4 Plaza	
5 Playing Field	
6 Playground	A
7 Park	A
8 Shared Use Path	A
9 Recreation Trails	A
BUILDING AND LOT TYPES (SECTION 2.4.2)	
1 Single Family Detached	
2 Single-Family Attached	
3 Two-Family	
4 Multi-family	
5 Small Building	
6 Medium Building	
7 Large Building	
8 Civic Building	A
9 Dedicated Office Building	
10 Large Format Retail Building	
11 Institutional Building	
12 Parking Structure	
13 Flex Building	

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA WC-3

SUBAREA DEVELOPMENT

Subarea Development Maximums Total	
AGRICULTURAL	NO LIMIT
RESIDENTIAL	NOT ALLOWABLE
INSTITUTIONAL	NOT ALLOWABLE
CIVIC	NO LIMIT
ACCOMMODATIONS	NOT ALLOWABLE
COMMERCIAL USE	NOT ALLOWABLE

Subarea Open Space Minima Total	
SHARED OPEN SPACE	0
CONSERVED GREEN SPACE	39.0

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards

SUBAREA: WC-3

Principles and Standards

COMPOSITION PRINCIPLES

This Subarea is dedicated to a combination of Conserved Green Space and Passive Open Space including a proposed pond. The overall composition of streets, trails, paths and buildings will include the following principle:

- Compatible Use – This is an area that is restricted to a combination of Conserved Green Space and Passive Open Space. Uses will be compatible with this focus and will include recreation trails and independent use paths that are integrated with the sidewalks at adjacent streets.

OPEN SPACES This Subarea is predominantly publicly accessible Open Space and Conserved Green Space as indicated by the Land Use Plan.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK None

SECONDARY STREET NETWORK The secondary street network should be designed to connect with the street network in adjoining Subareas while creating pedestrian and bicycle access to a system of integrated recreation trails and independent use paths. On-street parking may be provided to allow public access to the open space areas. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- Frontages along the Secondary Network – For segments along Open Space, the frontages should be landscaped.
- Frontages bounding this Subarea – Frontages bounding this Subarea should either contain a public sidewalk or a trail located within the private lot.

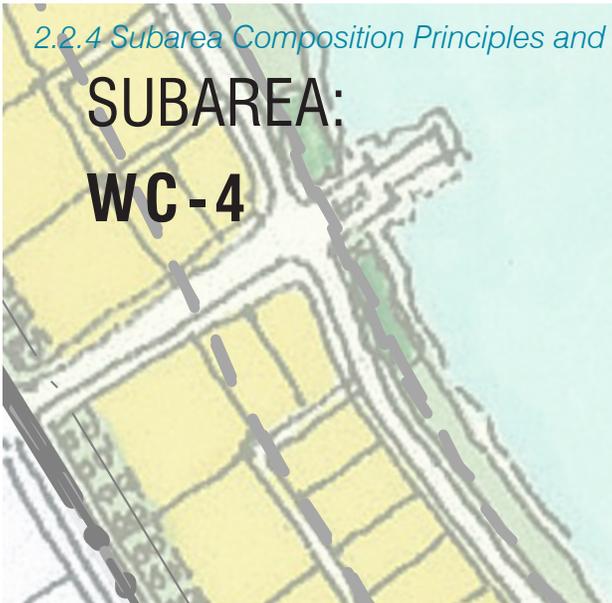
PEDESTRIAN NETWORK Pedestrian networks, including sidewalks or trails, will be provided to connect the trail network within the Open Space to larger circulation networks, including the trail network in WC-8.

BICYCLE NETWORK Accommodations for bicycles will be provided along trails or on the secondary street network.

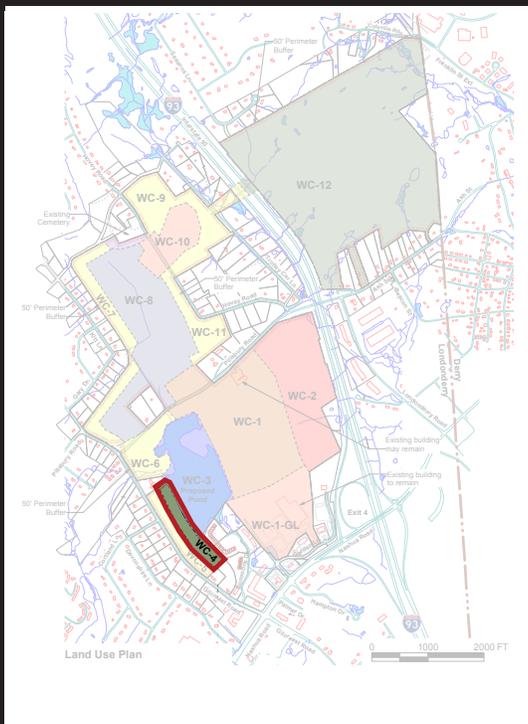
PARKING On-street parking should be provided to allow public access to Shared Open Space. Other parking lots should be limited and integrated into the landscape.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-4**



INTENT. The purpose of this Subarea is to create a residential area that allows for a gradual increase in density from the single-family homes in WC-5 along Gilcrest Road to the mix of uses in WC-1. Development patterns will include residential buildings on smaller lots with access to shared open space.



Regulating Type Allowed

STREET TYPES (SECTION 2.3.3)

- 1 Boulevard
- 2 Avenue
- 3 Two-way Street **A**
- 4 One-way Street **A**
- 5 Access Lane **A**

BLOCK TYPES (SECTION 2.3.2)

- 1 Village Center
- 2 Neighborhood **A**
- 3 Flex
- 4 Perimeter
- 5 Perimeter Neighborhood **A**

OPEN SPACE TYPES (SECTION 2.3.6)

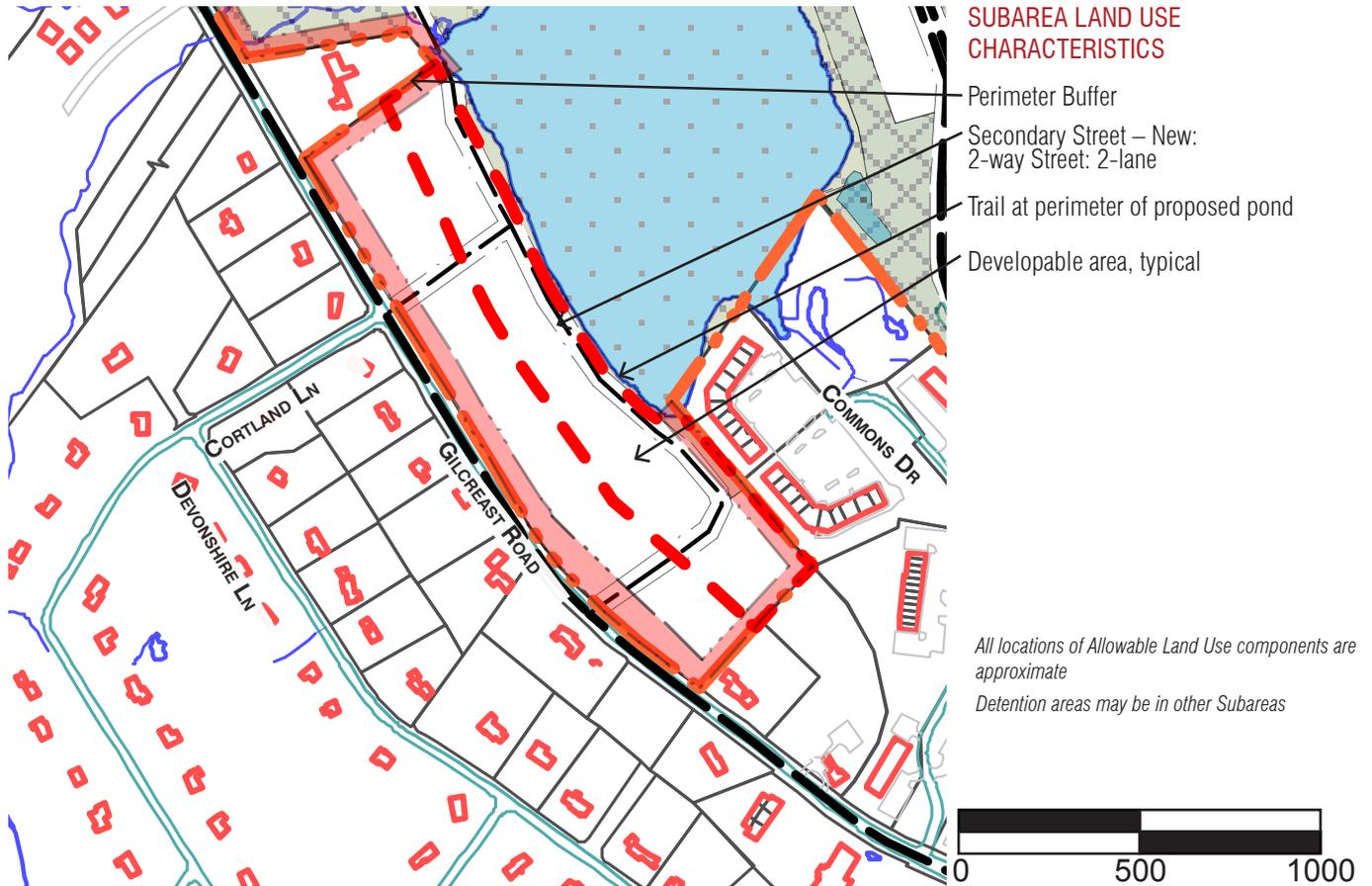
- 1 Conserved Green Space **A**
- 2 Green **A**
- 3 Square **A**
- 4 Plaza
- 5 Playing Field
- 6 Playground **A**
- 7 Park **A**
- 8 Shared Use Path **A**
- 9 Recreation Trails **A**

BUILDING AND LOT TYPES (SECTION 2.4.2)

- 1 Single Family Detached **A**
- 2 Single-Family Attached **A**
- 3 Two-Family
- 4 Multi-family
- 5 Small Building **A**
- 6 Medium Building
- 7 Large Building
- 8 Civic Building
- 9 Dedicated Office Building
- 10 Large Format Retail Building
- 11 Institutional Building
- 12 Parking Structure
- 13 Flex Building

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA LAND USE CHARACTERISTICS

- Perimeter Buffer
- Secondary Street – New: 2-way Street: 2-lane
- Trail at perimeter of proposed pond
- Developable area, typical

All locations of Allowable Land Use components are approximate
 Detention areas may be in other Subareas



SUBAREA WC-4

SUBAREA DEVELOPMENT

Subarea Development Maximums Total	
AGRICULTURAL	NO LIMIT
RESIDENTIAL	28 UNITS
INSTITUTIONAL	NOT ALLOWABLE
CIVIC	NO LIMIT
ACCOMMODATIONS	NOT ALLOWABLE
COMMERCIAL USE	5,000 GSF

Subarea Open Space Minima Total	
SHARED OPEN SPACE	2.5
CONSERVED GREEN SPACE	0

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards

SUBAREA: WC-4

Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will provide a residential neighborhood that provides a transition from existing residential areas outside the Woodmont Commons PUD to the higher density development in WC-1. The overall composition of streets, blocks and open spaces will include the following principles:

- **Compatible Use** – This is an area that is primarily residential uses of a similar scale and character to neighboring properties including single family, duplex and accessory residential units. Some small neighborhood service uses are allowable.
- **Compatible Height** – The maximum height within this Subarea respects the modest scale of the context with a maximum of 35 feet.
- **Orientation to Proposed Pond** – This Subarea borders Subarea WC-3, which is connected to the open space surrounding a proposed pond. The configuration of lots and streets responds to adjacencies and access requirements of WC-3 and WC-5.
- **Compatibility with Adjacent Subareas** – This Subarea borders Subarea WC-3 and Subarea WC-5, with open space and residential uses respectively. Buildings on lots in this Subarea should address both the proposed pond in WC-3 and the street network that is contiguous to WC-5.
- **Perimeter Buffers** – All buildings on lots with a perimeter buffer must be set back 50' from the PUD boundary.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK None

SECONDARY STREET NETWORK The secondary street network should be designed to create access to residential neighborhood blocks and open space and to limit the direct access onto Gilcrest Road. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- **Frontages along the Secondary Network** – The public frontages along the secondary network should be provided with planting strips and sidewalks. On-street parking may be provided.

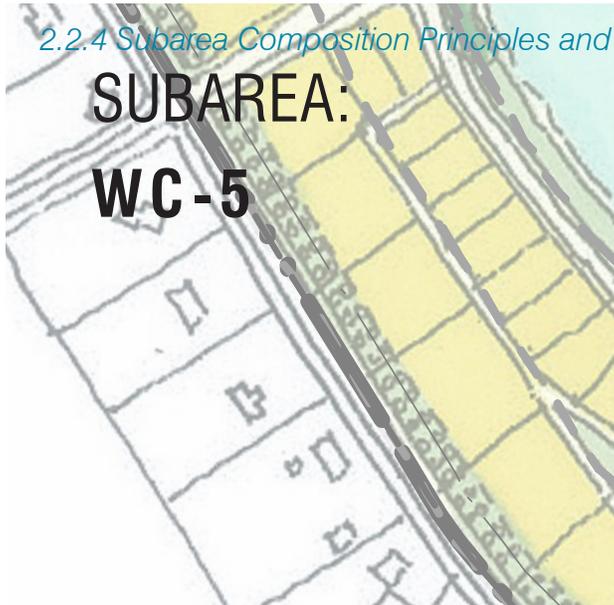
PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Secondary Streets within this Subarea. The pedestrian network should connect uses with open space.

BICYCLE NETWORK Accommodations for bicycles will be provided along trails or on the secondary street network.

PARKING On-street parking should be provided to allow for guest parking and public access to Shared Open Space. Off-street parking may be required for other uses.

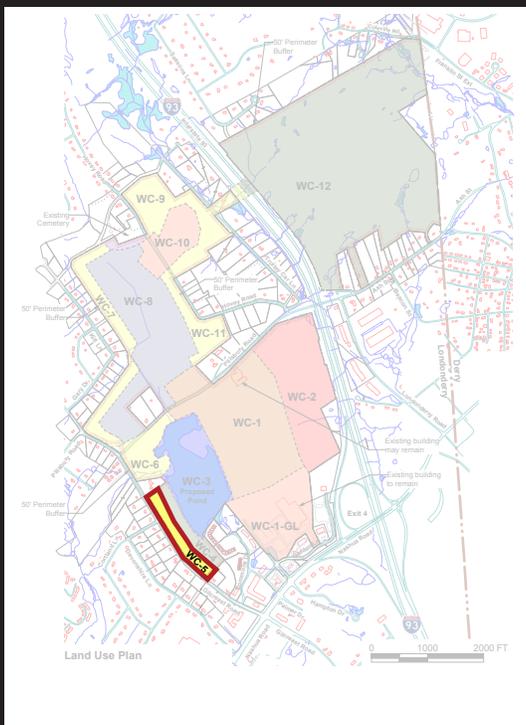
OPEN SPACES Shared open space should be comprised of at least one (1) separate location and ones (1) separate publicly-accessible open space type along the perimeter of the pond as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards



**SUBAREA:
WC-5**

INTENT. This Subarea is intended to create a residential neighborhood that is compatible with the context of existing residential properties.



Regulating Type Allowed

STREET TYPES (SECTION 2.3.3)

- 1 Boulevard
- 2 Avenue
- 3 Two-way Street **A**
- 4 One-way Street **A**
- 5 Access Lane **A**

BLOCK TYPES (SECTION 2.3.2)

- 1 Village Center
- 2 Neighborhood
- 3 Flex
- 4 Perimeter
- 5 Perimeter Neighborhood **A**

OPEN SPACE TYPES (SECTION 2.3.6)

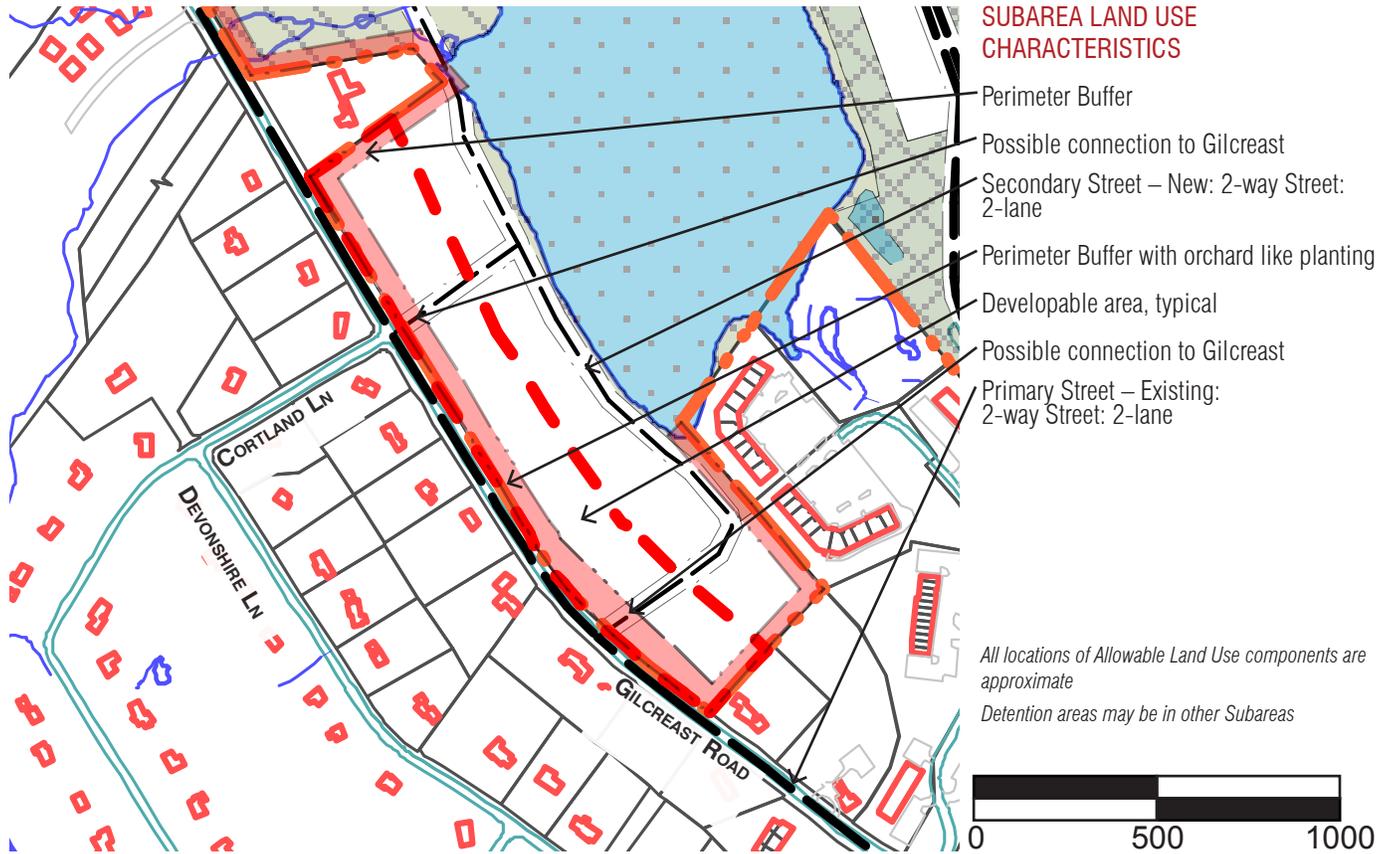
- 1 Conserved Green Space **A**
- 2 Green **A**
- 3 Square **A**
- 4 Plaza
- 5 Playing Field
- 6 Playground **A**
- 7 Park **A**
- 8 Shared Use Path **A**
- 9 Recreation Trails **A**

BUILDING AND LOT TYPES (SECTION 2.4.2)

- 1 Single Family Detached **A**
- 2 Single-Family Attached
- 3 Two-Family
- 4 Multi-family
- 5 Small Building
- 6 Medium Building
- 7 Large Building
- 8 Civic Building
- 9 Dedicated Office Building
- 10 Large Format Retail Building
- 11 Institutional Building
- 12 Parking Structure
- 13 Flex Building

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA WC-5

SUBAREA DEVELOPMENT

Subarea Development Maximums Total

AGRICULTURAL	NO LIMIT
RESIDENTIAL	11 UNITS
INSTITUTIONAL	NOT ALLOWABLE
CIVIC	NO LIMIT
ACCOMMODATIONS	NOT ALLOWABLE
COMMERCIAL USE	NOT ALLOWABLE

Subarea Open Space Minima Total

SHARED OPEN SPACE	0
CONSERVED GREEN SPACE	0

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards



SUBAREA: WC-5

Principles and Standards

COMPOSITION PRINCIPLES

This Subarea is intended to provide a residential neighborhood that provides similarly scaled and disposed uses comparable to the existing residential uses that abut the Woodmont Commons PUD Boundary at this location. The overall composition of streets, blocks and open spaces will include the following principles:

- **Compatible Use** – This is an area that is restricted to residential uses of a similar scale and character to neighboring properties including single family and accessory residential units.
- **Compatible Height** – The maximum height within this Subarea respects the modest scale of the context with a maximum of 35 feet.
- **Compatible Site Disposition** – The minimum frontage (150 feet for single family residences) and minimum setback (50 feet front setback as indicated on the Land Use Plan) provide an overall scale of lots and deployment of buildings that is compatible and respectful of the adjacent context.
- **Perimeter Buffers** – All buildings on lots with a perimeter buffer must be set back 50' from the PUD boundary. Along Gilcreast Road, the perimeter buffer should include orchard-like plantings.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK The Primary Streets are as identified on the Land Use Plan on the preceding page and as may be confirmed or modified by Supplemental TIAs. In this Subarea, the primary street network is an existing street at the boundary of the Woodmont Commons PUD, Gilcreast Road. No more than two egresses onto Gilcreast Road will be provided within this Subarea.

SECONDARY STREET NETWORK The secondary street network should be designed to create access to residential neighborhood blocks and to limit the direct access onto Gilcreast Road. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- **Frontages along the Primary Network** – Buildings must be set back beyond the 50' buffer at Gilcreast Road. The frontages should be landscaped to reinforce the residential neighborhood quality of this Subarea.
- **Frontages along the Secondary Network** – The frontages should be landscaped to reinforce the residential neighborhood quality of this Subarea.

PEDESTRIAN NETWORK Sidewalks do not exist along Gilcreast Road, independent shared use paths or sidewalks at egress locations onto Gilcreast Road are required to provide pedestrian access into the Woodmont Commons PUD and to connect to the larger pedestrian network within the PUD. The pedestrian network should connect uses with open space.

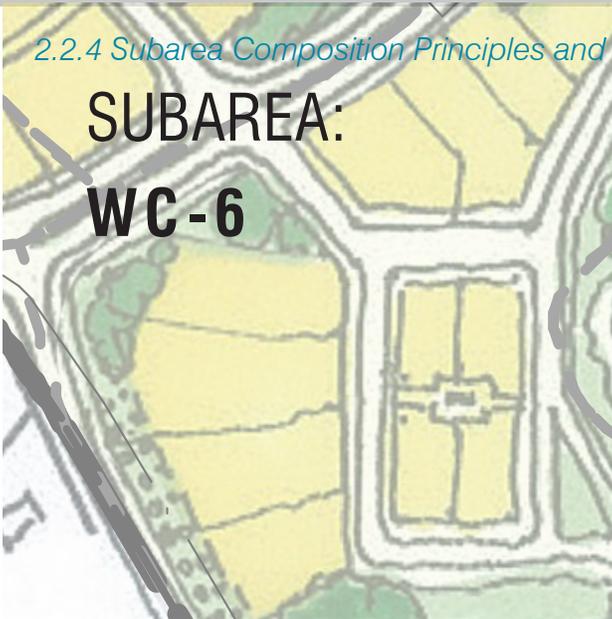
BICYCLE NETWORK Bike paths or routes do not exist along Gilcreast Road. Bicyclists will share the streets within this Subarea as traffic volume and speeds will be low enough to permit both. Access at egress locations onto Gilcreast Road will be connected to the larger bicycle network within the Subarea.

PARKING On-street parking shall be provided on the internal street network but not on Gilcreast Road.

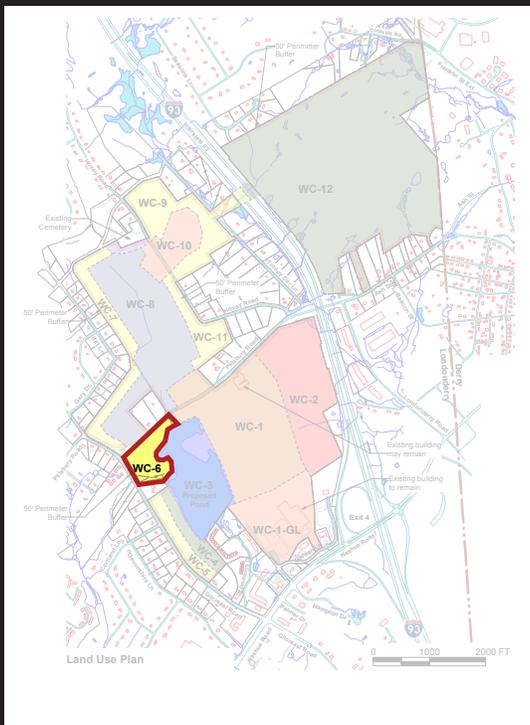
OPEN SPACES No other minimum open space requirements are defined within this Subarea as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-6**



INTENT. The purpose of this Subarea is to create a residential neighborhood that is compatible with the context of existing residential properties. The development pattern could include an internal integrated mix of smaller civic and institutional uses with a pedestrian-oriented network of streets.



Regulating Type Allowed

STREET TYPES (SECTION 2.3.3)

- 1 Boulevard
- 2 Avenue
- 3 Two-way Street **A**
- 4 One-way Street **A**
- 5 Access Lane **A**

BLOCK TYPES (SECTION 2.3.2)

- 1 Village Center
- 2 Neighborhood **A**
- 3 Flex
- 4 Perimeter
- 5 Perimeter Neighborhood **A**

OPEN SPACE TYPES (SECTION 2.3.6)

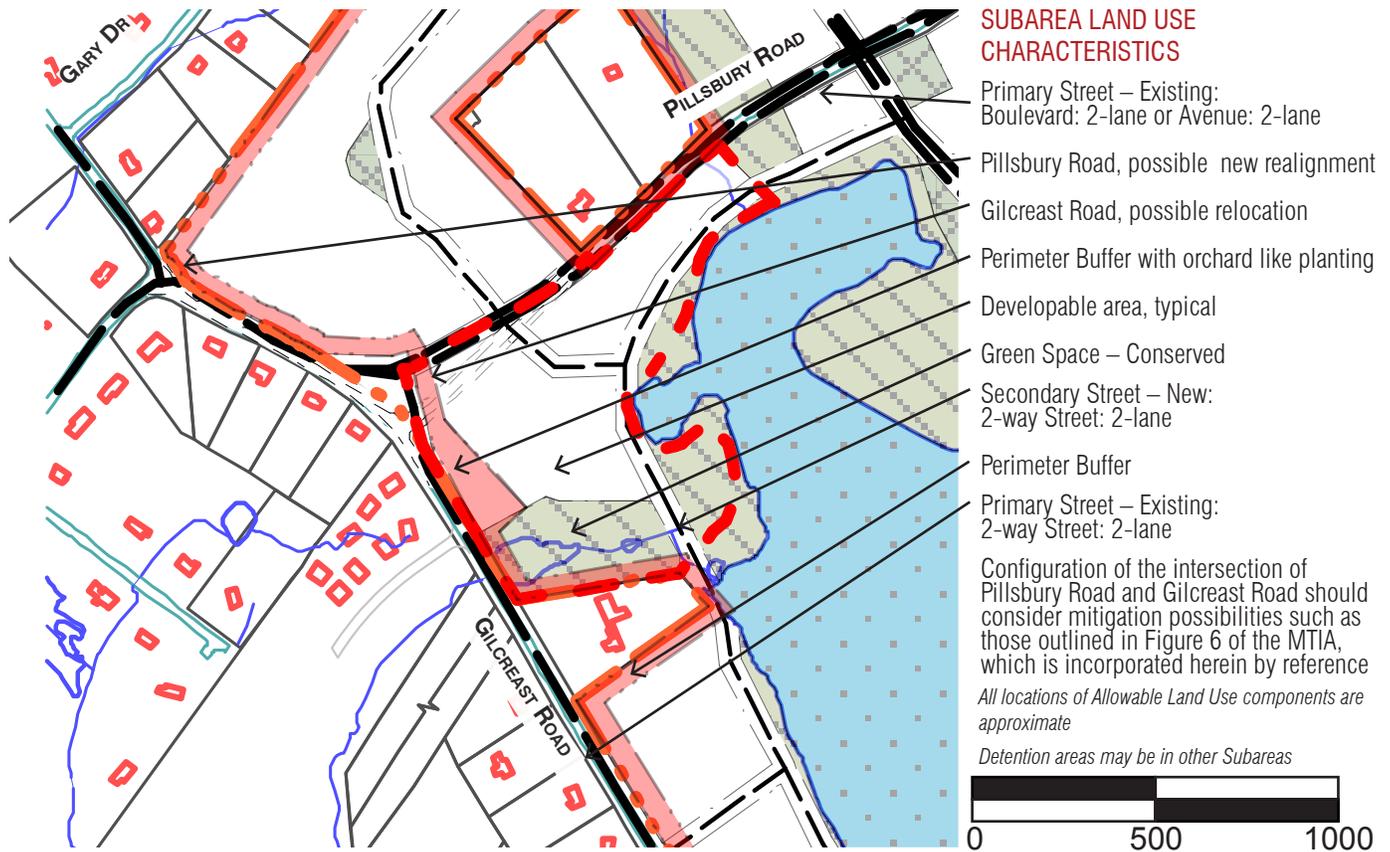
- 1 Conserved Green Space **A**
- 2 Green **A**
- 3 Square **A**
- 4 Plaza
- 5 Playing Field
- 6 Playground **A**
- 7 Park **A**
- 8 Shared Use Path **A**
- 9 Recreation Trails **A**

BUILDING AND LOT TYPES (SECTION 2.4.2)

- 1 Single Family Detached **A**
- 2 Single-Family Attached **A**
- 3 Two-Family
- 4 Multi-family
- 5 Small Building **A**
- 6 Medium Building
- 7 Large Building
- 8 Civic Building **A**
- 9 Dedicated Office Building
- 10 Large Format Retail Building
- 11 Institutional Building **A**
- 12 Parking Structure
- 13 Flex Building

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA WC-6

SUBAREA DEVELOPMENT

Subarea Development Maximums Total	
AGRICULTURAL	NO LIMIT
RESIDENTIAL	16 UNITS
INSTITUTIONAL	NOT ALLOWABLE
CIVIC	NO LIMIT
ACCOMMODATIONS	NOT ALLOWABLE
COMMERCIAL USE	NOT ALLOWABLE

Subarea Open Space Minima Total	
SHARED OPEN SPACE	5.0
CONSERVED GREEN SPACE	0

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards



Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will provide a residential neighborhood that provides similarly scaled and disposed uses comparable to the existing residential uses that abut the Woodmont Commons PUD Boundary at this location. The overall composition of streets, blocks and open spaces will include the following principles:

- **Compatible Use** – This is an area that is primarily residential uses of a similar scale and character to neighboring properties including single family, duplex and accessory residential units. Some small non-residential uses are allowable internal to the Subarea.
- **Compatible Height** – The maximum height within this Subarea respects the modest scale of the context with a maximum of 35 feet.
- **Compatibility with Adjacent Subareas** – This Subarea borders Subarea WC-3. Pedestrian sidewalk access at the street should be integrated with the recreation trails and independent use paths in that Subarea.
- **Perimeter Buffers** – All buildings on lots with a perimeter buffer must be set back 50' from the PUD boundary. Along Gilcreast Road, the perimeter buffer should include orchard-like plantings.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK The Primary Streets are as identified on the Land Use Plan on the preceding page and as may be confirmed or modified by Supplemental TIAs. In this Subarea, Pillsbury Road is the primary street network. A possible improvement outlined in the Master TIA is the redesign of Gilcreast Road as it meets Pillsbury Road. Parking is not required along this segment of the primary network.

SECONDARY STREET NETWORK The secondary street network should be designed to create access to residential neighborhood blocks and open space and to limit the direct access onto Gilcreast Road. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- **Frontages along the Secondary Network** – The frontages should be landscaped to reinforce the residential neighborhood quality of this Subarea.
- **Frontages along Pillsbury Road** – The public frontages along Pillsbury Road should include a planting strip between the street and the sidewalk next to retail, mixed use or commercial uses.

PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Secondary Streets within this Subarea and will link to sidewalks and trails around the pond. The pedestrian network should connect uses with open space.

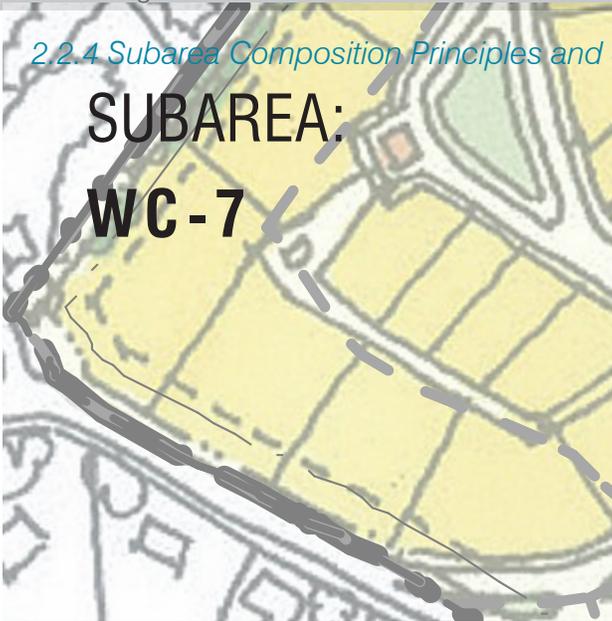
BICYCLE NETWORK Accommodations for bicycles will be provided on the Secondary Street Network.

PARKING On-street parking should be provided to support residential uses. Off-street parking may be required for other uses.

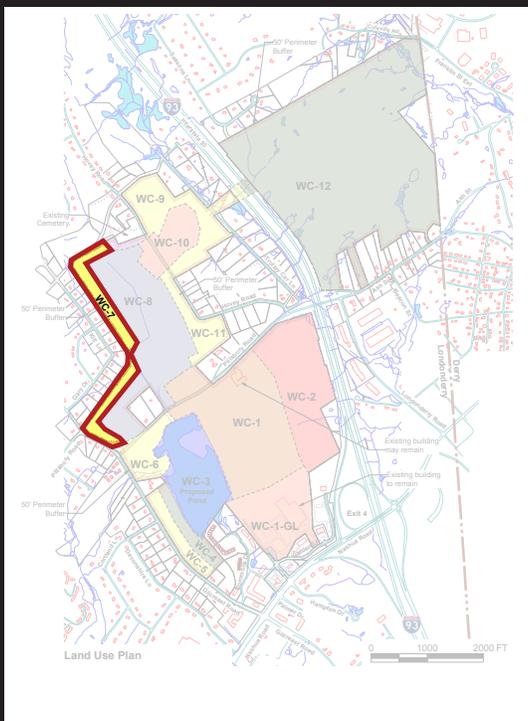
OPEN SPACES Shared open space should be comprised of at least two (2) separate locations and two (2) separate publicly-accessible open space types as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-7**



INTENT. The purpose of this Subarea is to create a residential neighborhood that is compatible with the context of existing residential properties.



Regulating Type Allowed

STREET TYPES (SECTION 2.3.3)

- 1 Boulevard
- 2 Avenue
- 3 Two-way Street **A**
- 4 One-way Street **A**
- 5 Access Lane **A**

BLOCK TYPES (SECTION 2.3.2)

- 1 Village Center
- 2 Neighborhood **A**
- 3 Flex
- 4 Perimeter
- 5 Perimeter Neighborhood **A**

OPEN SPACE TYPES (SECTION 2.3.6)

- 1 Conserved Green Space **A**
- 2 Green **A**
- 3 Square **A**
- 4 Plaza
- 5 Playing Field
- 6 Playground **A**
- 7 Park **A**
- 8 Shared Use Path **A**
- 9 Recreation Trails **A**

BUILDING AND LOT TYPES (SECTION 2.4.2)

- 1 Single Family Detached **A**
- 2 Single-Family Attached
- 3 Two-Family
- 4 Multi-family
- 5 Small Building
- 6 Medium Building
- 7 Large Building
- 8 Civic Building
- 9 Dedicated Office Building
- 10 Large Format Retail Building
- 11 Institutional Building
- 12 Parking Structure
- 13 Flex Building

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA LAND USE CHARACTERISTICS

- Developable area, typical
- Possible trail connection
- Secondary Street – New:
2-way Street: 2-lane
- Perimeter Buffer
- Conserved Green Space
- Secondary Street – New:
2-way Street: 2 to 4 – Lane
- Pillsbury Road, possible relocation
- Pillsbury Road, possible realignment
- Primary Street – Existing:
Boulevard: 2-lane or Avenue: 2-lane

All locations of Allowable Land Use components are approximate
Detention areas may be in other Subareas



SUBAREA WC-7

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street as identified in the Master TIA

- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

SUBAREA DEVELOPMENT

Subarea Development Maximums Total	
AGRICULTURAL	NO LIMIT
RESIDENTIAL	24 UNITS
INSTITUTIONAL	NOT ALLOWABLE
CIVIC	NO LIMIT
ACCOMMODATIONS	NOT ALLOWABLE
COMMERCIAL USE	NOT ALLOWABLE

Subarea Open Space Minima Total	
SHARED OPEN SPACE	0
CONSERVED GREEN SPACE	0

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

2.2.4 Subarea Composition Principles and Standards



Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will provide a residential neighborhood that provides similarly scaled and disposed uses comparable to the existing residential uses that abut the Woodmont Commons PUD Boundary at this location. The overall composition of streets, blocks and open spaces will include the following principles:

- **Compatible Use** – This is an area that is restricted to residential uses of a similar scale and character to neighboring properties including single family and accessory residential units.
- **Compatible Height** – The maximum height within this Subarea respects the modest scale of the context with a maximum of 35 feet.
- **Compatible Site Disposition** – The minimum frontage (150 feet for single family residences) and minimum setback (50 feet rear or side setback, as indicated on the Land Use Plan) provide an overall scale of lots and deployment of buildings that is compatible and respectful of the adjacent context.
- **Perimeter Buffers** – All buildings on lots with a perimeter buffer must be set back 50' from the PUD boundary.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK The Primary Streets are as identified on the Land Use Plan on the preceding page and as may be confirmed or modified by Supplemental TIAs. In this Subarea, Pillsbury Road is the primary street network. Parking is not required along this segment of the primary network.

SECONDARY STREET NETWORK The secondary street network should be designed to create access to residential neighborhood blocks. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- **Frontages along the Secondary Network** – The frontages should be landscaped to reinforce the residential neighborhood quality of this Subarea.

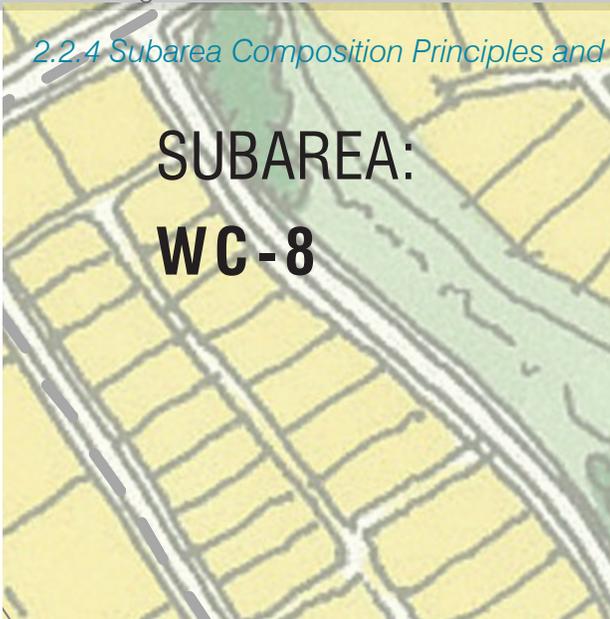
PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Secondary Streets within this Subarea. The pedestrian network should connect uses with open space.

BICYCLE NETWORK Bike paths will share the streets within this Subarea as traffic volume and speeds will be low enough to permit both.

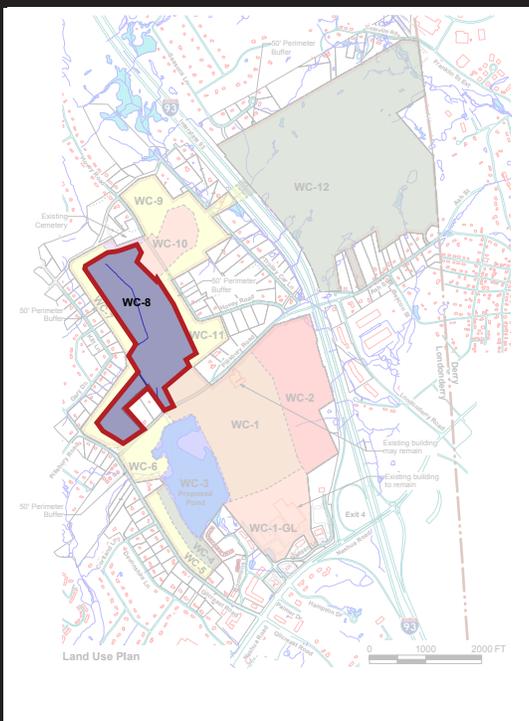
PARKING On-street parking shall be provided on the internal street network.

OPEN SPACES No minimum open space requirements are defined within this Subarea as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards



INTENT. The purpose of this Subarea is to allow for a mix of higher density residential uses with significant conserved green space. Development patterns will include a variety of residential buildings and lot sizes, integrated with a pedestrian-friendly central corridor that includes publicly accessible conserved green space. Some retail and commercial uses will be allowed near the village center in WC-1 and the neighborhood center in WC-10.



Regulating Type Allowed

STREET TYPES (SECTION 2.3.3)

- 1 Boulevard **A**
- 2 Avenue **A**
- 3 Two-way Street **A**
- 4 One-way Street **A**
- 5 Access Lane **A**

BLOCK TYPES (SECTION 2.3.2)

- 1 Village Center **A**
- 2 Neighborhood **A**
- 3 Flex
- 4 Perimeter
- 5 Perimeter Neighborhood

OPEN SPACE TYPES (SECTION 2.3.6)

- 1 Conserved Green Space **A**
- 2 Green **A**
- 3 Square **A**
- 4 Plaza
- 5 Playing Field **A**
- 6 Playground **A**
- 7 Park **A**
- 8 Shared Use Path **A**
- 9 Recreation Trails **A**

BUILDING AND LOT TYPES (SECTION 2.4.2)

- 1 Single Family Detached **A**
- 2 Single-Family Attached **A**
- 3 Two-Family **A**
- 4 Multi-family **A**
- 5 Small Building **A**
- 6 Medium Building
- 7 Large Building
- 8 Civic Building **A**
- 9 Dedicated Office Building
- 10 Large Format Retail Building
- 11 Institutional Building **A**
- 12 Parking Structure
- 13 Flex Building

2.2.4 Subarea Composition Principles and Standards

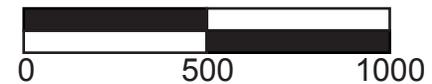
Land Use Plan



SUBAREA LAND USE CHARACTERISTICS

- Developable area, typical
- Possible trail connection
- Secondary Street – New: 2-way Street: 2-lane
- Primary Street – New: 2-way Street: 2-lane
- Primary Street – New Boulevard, Avenue or 2-way Street: 2-4-lane
- Shared Open Space – Passive with a trail system connecting to Green Space
- Conserved Green Space, with a trail system connecting to Subarea WC-3
- Secondary Street – New: 2-way Street: 2-lane
- Perimeter Buffer
- Pillsbury Road, possible relocation
- Pillsbury Road, possible new realignment
- Primary Street – Existing: Boulevard: 2-lane or Avenue: 2-lane

All locations of Allowable Land Use components are approximate
 Detention areas may be in other Subareas



SUBAREA WC-8

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street as identified in the Master TIA
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

SUBAREA DEVELOPMENT

Subarea Development Maximums Total	
AGRICULTURAL	NO LIMIT
RESIDENTIAL	230 UNITS
INSTITUTIONAL	20,000 GSF
CIVIC	NO LIMIT
ACCOMMODATIONS	NOT ALLOWABLE
COMMERCIAL USE	5,000 GSF

Subarea Open Space Minima Total	
SHARED OPEN SPACE	1.0
CONSERVED GREEN SPACE	10.0

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

2.2.4 Subarea Composition Principles and Standards

SUBAREA:

WC-8

Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will be organized so that it can support a variety of different residential uses supported by small neighborhood retail and service uses as the Subarea approaches WC-1 and WC-10. The overall composition of streets, blocks and open spaces will include the following principles:

- **Variety** – This is an area that will benefit from a diversity of building lots, blocks, open spaces and the mix of uses. The division of land and infrastructure should support this variety.
- **Circulation and Open Space Corridor** – This Subarea will include a corridor of conserved and Passive Open Space integrated with a Primary Street Network and a system of integrated recreation trails and independent use paths.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK The Primary Streets are as identified on the Land Use Plan on the preceding page and as may be confirmed or modified by Supplemental TIAs. In this Subarea, the primary street network includes Pillsbury Road and transitions into the neighborhood. Parking is allowed along these segments of the primary network.

SECONDARY STREET NETWORK The secondary network should be designed to create developable blocks or to outline planned open space. Parking can serve as a shared parking resource for commercial, civic, accommodation, residential or institutional uses. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- **Frontages along the Primary Network** – The public frontages should be landscaped to reinforce the characteristics of the network in this area. The public frontages along Pillsbury Road should include a planting strip between the street and the sidewalk except when next to retail, mixed use or commercial uses.
- **Frontages along the Secondary Network** – The public frontages along the secondary network should be consistent with the primary intended ground level use and its

relationship to on-street parking. For segments intended to have predominately commercial, civic, accommodation or institutional uses with short term on-street parking, landscaped borders should be limited and sidewalk paving generally extended to the street edge. For predominately residential segments or areas where on-street parking is not expected to serve as a short-term supply, the frontages should be landscaped.

PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Primary and Secondary Streets within this Subarea. Curb extensions or other traffic calming devices may be provided at Secondary Network intersections serving blocks or frontages intended to serve business, civic accommodation or institutional uses. The pedestrian network should connect uses with open space.

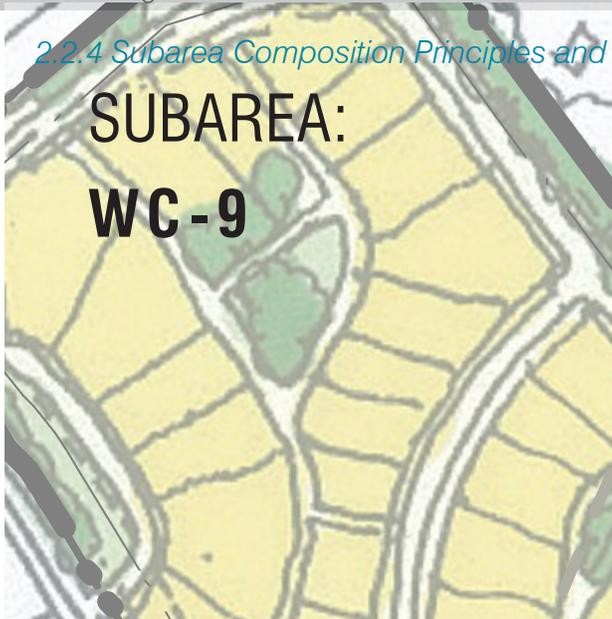
BICYCLE NETWORK Accommodations for bicycles will be provided along or parallel to the Primary Street Network. Shared use of streets will be permitted for all other portions of this Subarea.

PARKING On-street parking should be provided to allow for guest parking and public access to Shared Open Space. Off-street parking may be required for other uses.

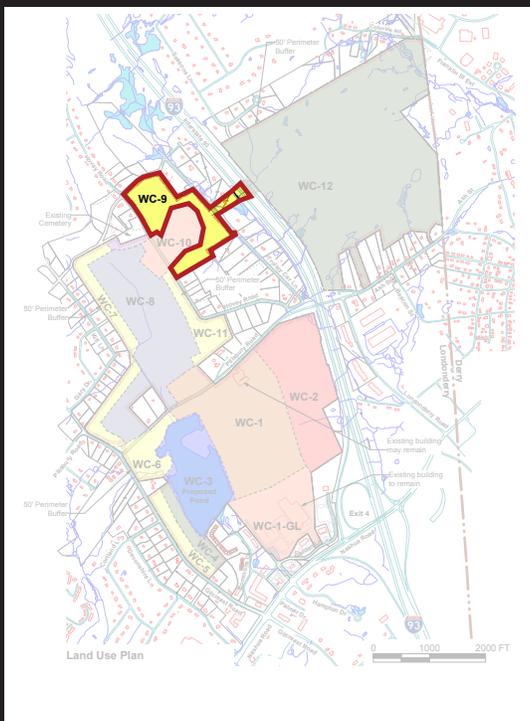
OPEN SPACES Shared open space should be comprised of at least two (2) separate locations and two (2) separate publicly-accessible open space types as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-9**



INTENT. The purpose of this Subarea is to allow for a residential neighborhood that is compatible with the context of existing residential properties. Development patterns would allow increasing density near the internal Subarea boundaries.



Regulating Type Allowed

STREET TYPES (SECTION 2.3.3)

1	Boulevard	
2	Avenue	
3	Two-way Street	A
4	One-way Street	A
5	Access Lane	A

BLOCK TYPES (SECTION 2.3.2)

1	Village Center	
2	Neighborhood	A
3	Flex	
4	Perimeter	
5	Perimeter Neighborhood	A

OPEN SPACE TYPES (SECTION 2.3.6)

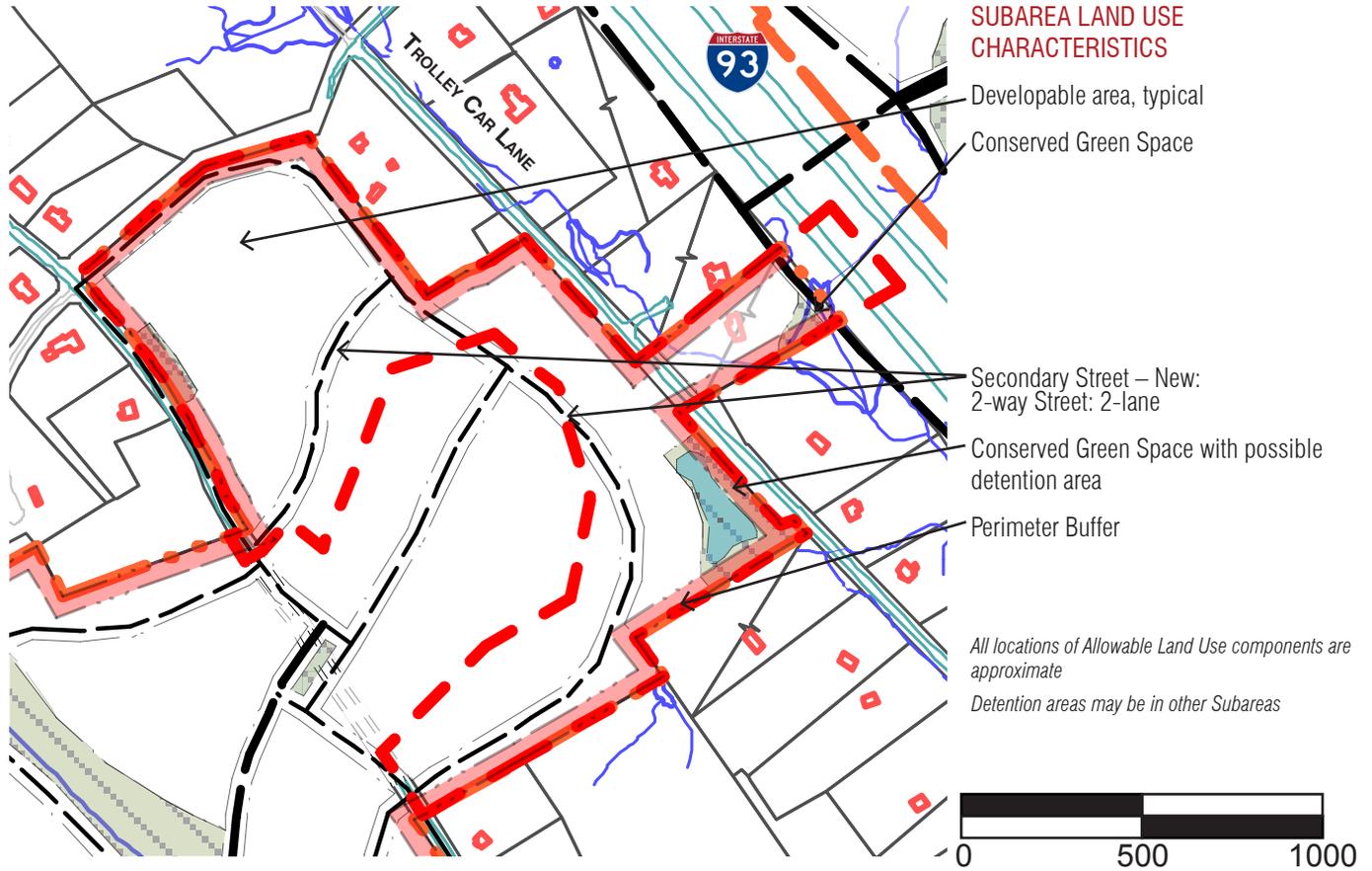
1	Conserved Green Space	A
2	Green	A
3	Square	A
4	Plaza	
5	Playing Field	A
6	Playground	A
7	Park	A
8	Shared Use Path	A
9	Recreation Trails	A

BUILDING AND LOT TYPES (SECTION 2.4.2)

1	Single Family Detached	A
2	Single-Family Attached	A
3	Two-Family	A
4	Multi-family	A
5	Small Building	
6	Medium Building	
7	Large Building	
8	Civic Building	A
9	Dedicated Office Building	
10	Large Format Retail Building	
11	Institutional Building	
12	Parking Structure	
13	Flex Building	

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA WC-9

SUBAREA DEVELOPMENT

Subarea Development Maximums Total	
AGRICULTURAL	NO LIMIT
RESIDENTIAL	52 UNITS
INSTITUTIONAL	NOT ALLOWABLE
CIVIC	NO LIMIT
ACCOMMODATIONS	NOT ALLOWABLE
COMMERCIAL USE	NOT ALLOWABLE

Subarea Open Space Minima Total	
SHARED OPEN SPACE	0.5
CONSERVED GREEN SPACE	0

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards



SUBAREA:

WC-9

Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will provide a residential neighborhood that provides similarly scaled and disposed uses comparable to the existing residential uses that abut the Woodmont Commons PUD Boundary at this location. The overall composition of streets, blocks and open spaces will include the following principles:

- **Compatible Use** – This is an area that is restricted to residential uses of a similar scale and character to neighboring properties including single family and accessory residential units, two-family or multi-family.
- **Compatible Height** – The maximum height within this Subarea respects the modest scale of the context with a maximum of 35 feet.
- **Perimeter Buffers** – All buildings on lots with a perimeter buffer must be set back 50' from the PUD boundary. Uses not compatible with adjoining existing residential uses must be screened with landscaping.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK None

SECONDARY STREET NETWORK The secondary street network should be designed to create access to residential neighborhood blocks. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- **Frontages along the Secondary Network** – The frontages should be landscaped to reinforce the residential neighborhood quality of this Subarea.

PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Secondary Streets within this Subarea. The pedestrian network should connect uses with open space.

BICYCLE NETWORK Bike paths will share the streets within this Subarea as traffic volume and speeds will be low enough to permit both.

PARKING On-street parking shall be provided on the internal street network.

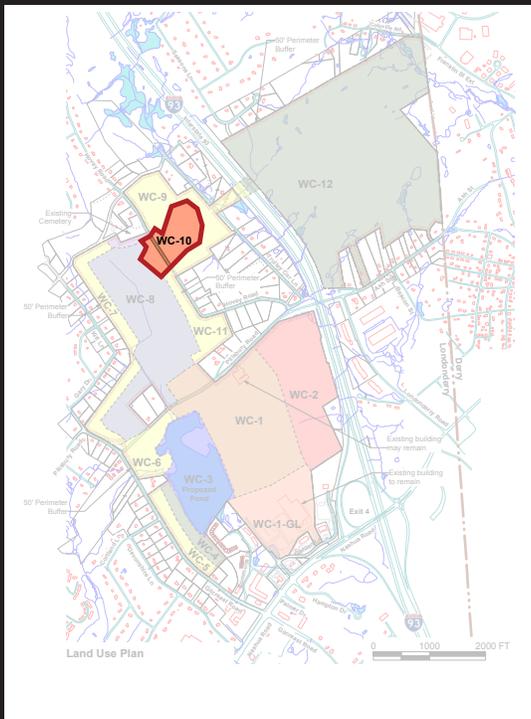
OPEN SPACES Shared open space should be comprised of at least two (2) separate locations and two (2) separate publicly-accessible open space types as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-10**



INTENT. The purpose of this Subarea is to allow for a mix of medium density residential uses with a cluster of neighborhood services, retail and commercial uses. The development pattern supports compact and well-connected pedestrian-oriented segments along the streets and sidewalks, with smaller retail, mixed-use or commercial uses.



Regulating Type Allowed

STREET TYPES (SECTION 2.3.3)

1	Boulevard	A
2	Avenue	A
3	Two-way Street	A
4	One-way Street	A
5	Access Lane	A

BLOCK TYPES (SECTION 2.3.2)

1	Village Center	A
2	Neighborhood	A
3	Flex	
4	Perimeter	
5	Perimeter Neighborhood	

OPEN SPACE TYPES (SECTION 2.3.6)

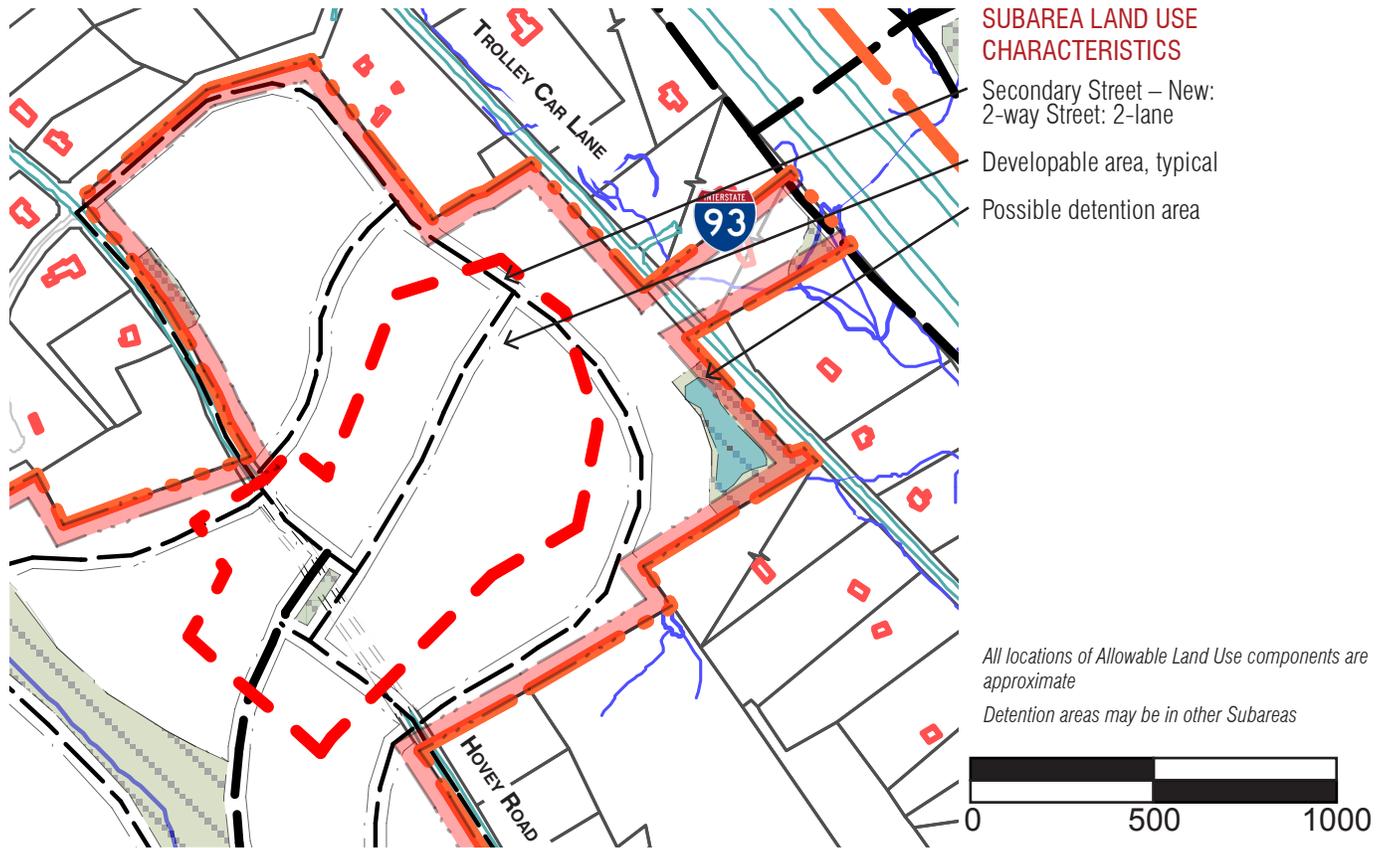
1	Conserved Green Space	A
2	Green	A
3	Square	A
4	Plaza	A
5	Playing Field	
6	Playground	A
7	Park	A
8	Shared Use Path	A
9	Recreation Trails	A

BUILDING AND LOT TYPES (SECTION 2.4.2)

1	Single Family Detached	A
2	Single-Family Attached	A
3	Two-Family	A
4	Multi-family	A
5	Small Building	A
6	Medium Building	A
7	Large Building	
8	Civic Building	A
9	Dedicated Office Building	
10	Large Format Retail Building	
11	Institutional Building	A
12	Parking Structure	
13	Flex Building	

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA WC-10

SUBAREA DEVELOPMENT

Subarea Development Maximums Total	
AGRICULTURAL	NO LIMIT
RESIDENTIAL	60 UNITS
INSTITUTIONAL	20,000 GSF
CIVIC	NO LIMIT
ACCOMMODATIONS	NOT ALLOWABLE
COMMERCIAL USE	10,000 GSF

Subarea Open Space Minima Total	
SHARED OPEN SPACE	0.5
CONSERVED GREEN SPACE	0

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards



Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will be organized so that it can support a variety of different uses assembled to create a compact neighborhood center within the overall Woodmont Commons PUD. The overall composition of streets, blocks and open spaces will include the following principles:

- **Variety** – This is an area that will benefit from a diversity of building lots, blocks, open spaces and the mix of uses, and the division of land and infrastructure should support this variety.
- **Neighborhood Center** – The neighborhood center will allow small retail and commercial uses then transition to residential uses at the Subarea perimeter.
- **Compact and Connected Development Patterns for Retail, Commercial Uses and Mixed-use** – The development patterns should support compact and well-connected pedestrian-oriented segments along the streets and sidewalks, connected to the primary street network, and with smaller retail, commercial uses or mixed-use at the neighborhood center. The scale and intensity transition from a neighborhood center to a residential neighborhood.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK None

SECONDARY STREET NETWORK The secondary network should be designed to create developable blocks or to outline planned open space. Parking can serve as a shared parking resource for commercial, civic, accommodation, residential or institutional uses. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- **Frontages along the Secondary Network** – The public frontages along the secondary network should be consistent with the primary intended ground level use and its relationship to on-street parking. For segments intended to have predominately business, civic, accommodation or institutional uses with short term on-street parking, planting strips should be limited and sidewalk paving generally extended to the street edge. For predominately residential

segments or areas where on-street parking is not expected to serve as a short-term supply, the frontages should be landscaped.

PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Secondary Streets within this Subarea. Curb extensions or other traffic calming devices may be provided at Secondary Network intersections serving blocks or frontages intended to serve business, civic accommodation or institutional uses. The pedestrian network should connect uses with open space.

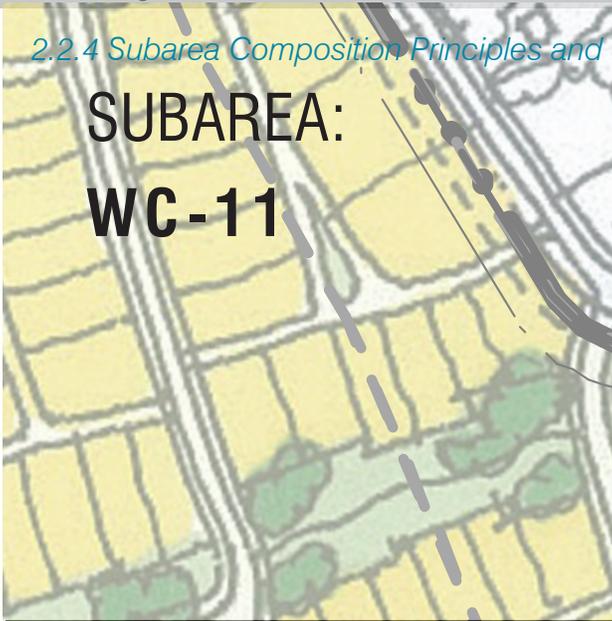
BICYCLE NETWORK Bike paths will share the streets within this Subarea as traffic volume and speeds will be low enough to permit both.

PARKING On-street parking may be considered to contribute to fulfilling the parking requirements of development within 400 feet of the parking spaces for non-residential uses. For residential uses, on-street parking may be allocated for required visitor spaces.

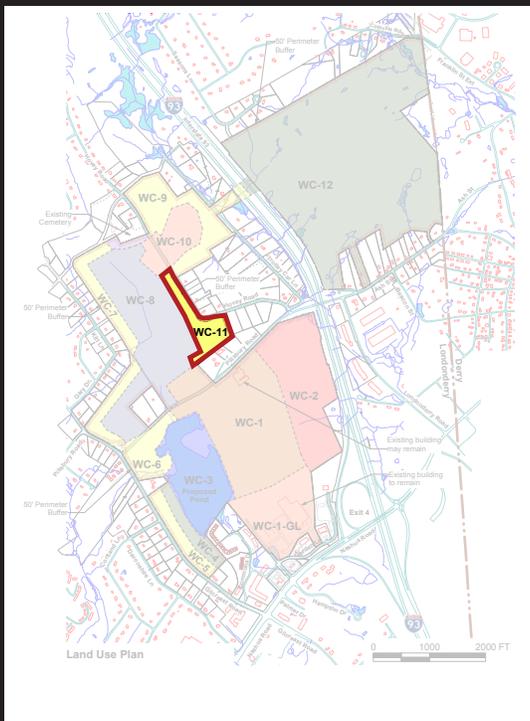
OPEN SPACES Shared open space should be comprised of at least one (1) separate location and one (1) separate publicly-accessible open space type as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-11**



INTENT. The purpose of this Subarea is to create a residential neighborhood that is compatible with the context of existing residential properties. Development patterns would allow increasing density near the internal Subarea boundaries.



Regulating Type Allowed

STREET TYPES (SECTION 2.3.3)

- 1 Boulevard
- 2 Avenue
- 3 Two-way Street **A**
- 4 One-way Street **A**
- 5 Access Lane **A**

BLOCK TYPES (SECTION 2.3.2)

- 1 Village Center
- 2 Neighborhood **A**
- 3 Flex
- 4 Perimeter
- 5 Perimeter Neighborhood **A**

OPEN SPACE TYPES (SECTION 2.3.6)

- 1 Conserved Green Space **A**
- 2 Green **A**
- 3 Square **A**
- 4 Plaza
- 5 Playing Field
- 6 Playground **A**
- 7 Park **A**
- 8 Shared Use Path **A**
- 9 Recreation Trails **A**

BUILDING AND LOT TYPES (SECTION 2.4.2)

- 1 Single Family Detached **A**
- 2 Single-Family Attached **A**
- 3 Two-Family **A**
- 4 Multi-family **A**
- 5 Small Building
- 6 Medium Building
- 7 Large Building
- 8 Civic Building **A**
- 9 Dedicated Office Building
- 10 Large Format Retail Building
- 11 Institutional Building
- 12 Parking Structure
- 13 Flex Building

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA LAND USE CHARACTERISTICS

- Secondary Street – New:
2-way Street: 2-lane
- Developable area, typical
- Open Space – Passive – Green with trail connection to WC-8
- Perimeter Buffer
- Secondary Street – New:
2-way Street: 2-lane

All locations of Allowable Land Use components are approximate
Detention areas may be in other Subareas



SUBAREA WC-11

SUBAREA DEVELOPMENT

Subarea Development Maximums Total	
AGRICULTURAL	NO LIMIT
RESIDENTIAL	24 UNITS
INSTITUTIONAL	NOT ALLOWABLE
CIVIC	NO LIMIT
ACCOMMODATIONS	NOT ALLOWABLE
COMMERCIAL USE	NOT ALLOWABLE

Subarea Open Space Minima Total	
SHARED OPEN SPACE	0
CONSERVED GREEN SPACE	0

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards



Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will provide a residential neighborhood that provides similarly scaled and disposed uses comparable to the existing residential uses that abut the Woodmont Commons PUD Boundary at this location. The overall composition of streets, blocks and open spaces will include the following principles:

- **Compatible Use** – This is an area that is restricted to residential uses of a similar scale and character to neighboring properties including single family and accessory residential units.
- **Compatible Height** – The maximum height within this Subarea respects the modest scale of the context with a maximum of 35 feet.
- **Perimeter Buffers** – All buildings on lots with a perimeter buffer must be set back 50' from the PUD boundary. Uses not compatible with adjoining existing residential uses must be screened with landscaping.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK None

SECONDARY STREET NETWORK The secondary street network should be designed to create access to residential neighborhood blocks. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- **Frontages along the Secondary Network** – The frontages should be landscaped to reinforce the residential neighborhood quality of this Subarea.

PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Secondary Streets within this Subarea. The pedestrian network should connect uses with open space.

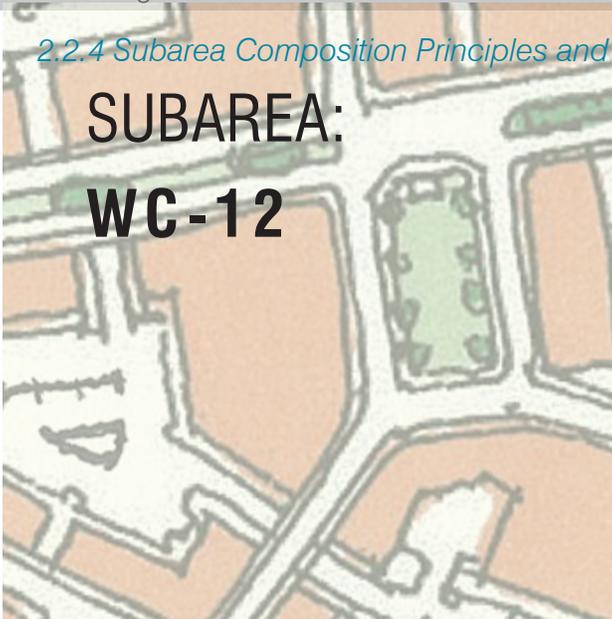
BICYCLE NETWORK Bike paths will share the streets within this Subarea as traffic volume and speeds will be low enough to permit both.

PARKING On-street parking shall be provided on the internal street network.

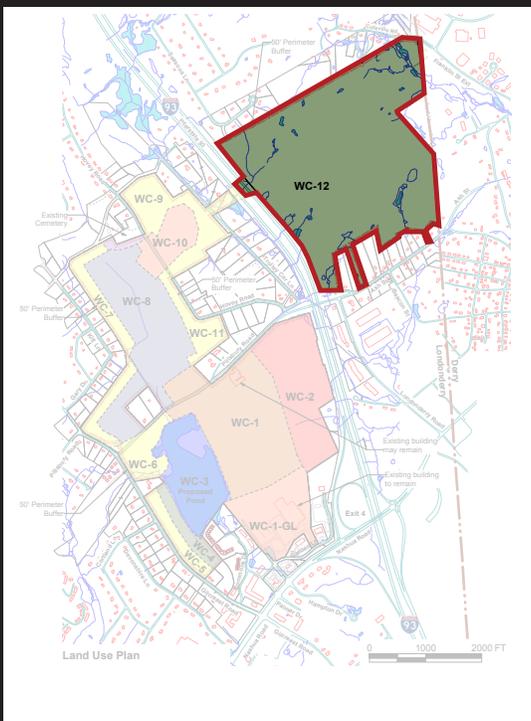
OPEN SPACES No minimum opens space requirements are defined within this Subarea as indicated by the Land Use Plan.

2.2.4 Subarea Composition Principles and Standards

**SUBAREA:
WC-12**



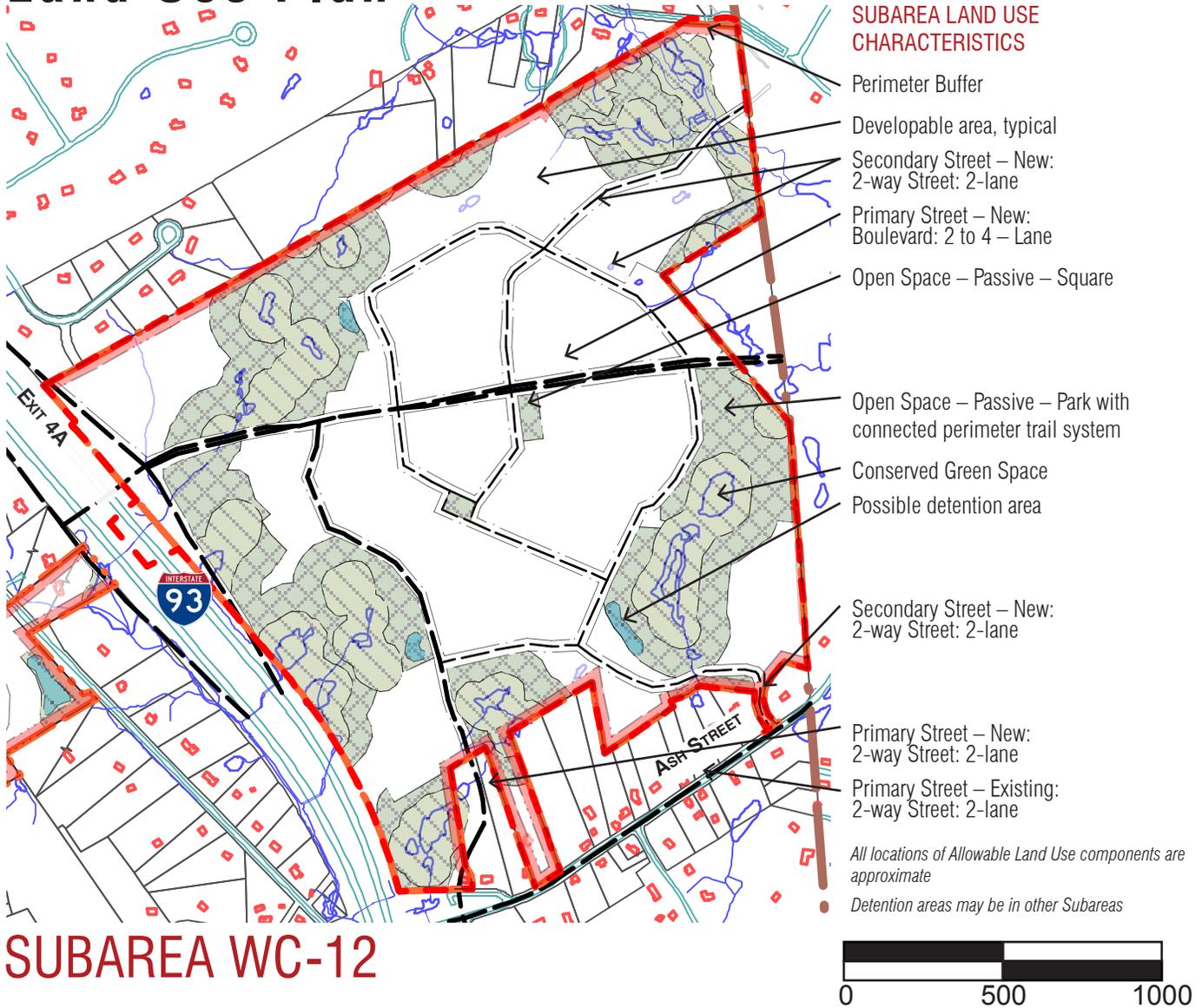
INTENT. The purpose of this Subarea is to create a regional mixed-use district supporting larger retail, commercial, institutional and office uses with some residential uses. Development patterns will be oriented toward connecting vehicular traffic to existing road networks to access offices and large retail, but pedestrian-oriented infrastructure will connect residential areas with publicly accessible open space and neighborhood supporting retail and services.



Regulating Type	Allowed
STREET TYPES (SECTION 2.3.3)	
1 Boulevard	A
2 Avenue	A
3 Two-way Street	A
4 One-way Street	A
5 Access Lane	A
BLOCK TYPES (SECTION 2.3.2)	
1 Village Center	A
2 Neighborhood	A
3 Flex	A
4 Perimeter	A
5 Perimeter Neighborhood	A
OPEN SPACE TYPES (SECTION 2.3.6)	
1 Conserved Green Space	A
2 Green	A
3 Square	A
4 Plaza	A
5 Playing Field	A
6 Playground	A
7 Park	A
8 Shared Use Path	A
9 Recreation Trails	A
BUILDING AND LOT TYPES (SECTION 2.4.2)	
1 Single Family Detached	A
2 Single-Family Attached	A
3 Two-Family	A
4 Multi-family	A
5 Small Building	A
6 Medium Building	A
7 Large Building	A
8 Civic Building	A
9 Dedicated Office Building	A
10 Large Format Retail Building	A
11 Institutional Building	A
12 Parking Structure	A
13 Flex Building	A

2.2.4 Subarea Composition Principles and Standards

Land Use Plan



SUBAREA DEVELOPMENT

Subarea Development Maximums Total

	WITH EXIT 4A	WITHOUT EXIT 4A
AGRICULTURAL	NO LIMIT	NO LIMIT
RESIDENTIAL	353 UNITS	350 UNITS
INSTITUTIONAL	420,000 GSF/300 BED HOSPITAL	NOT ALLOWABLE
CIVIC	NO LIMIT	NO LIMIT
ACCOMMODATIONS	200 HOTEL ROOMS	NOT ALLOWABLE
COMMERCIAL USE	700,000 GSF	400,000 GSF

Subarea Open Space Minima Total

SHARED OPEN SPACE	40.0
CONSERVED GREEN SPACE	36.5

See Section 2.2.3 Land Use Standards: Allowable Uses Table and Allowable Densities Table for more details.

LAND USE COMPONENTS

- PUD Perimeter
- Subarea Boundary
- Subdivision Lot Line
- Primary Street
- Secondary Street
- Shared Open Space
- Conserved Green Space
- Existing Buildings
- Developable Area

2.2.4 Subarea Composition Principles and Standards

SUBAREA: WC-12

Principles and Standards

COMPOSITION PRINCIPLES

This Subarea will be organized so that it can support a variety of different uses assembled to create a compact area within the overall Woodmont Commons PUD. The overall composition of streets, blocks and open spaces will include the following principles:

- **Variety** – This is an area that will benefit from a diversity of building lots, blocks, open spaces and the mix of uses, and the division of land and infrastructure should support this variety.
- **Diversity of open spaces** – There are a range of open space types that are located within the area to provide different types of opportunities and amenities.
- **Development along I-93** – The development patterns may support large buildings for offices or other uses that transition in use and scale from the boundary with I-93 and along the primary street to more residential uses. Parking can be placed along this boundary.
- **Expanding the Current Development Patterns for Retail, Commercial Uses and Mixed-use** – The development patterns should support vehicular access to the large format retail, Flex Space, mixed-use and commercial uses within this Subarea.
- **Perimeter Buffers** – All buildings on lots with a perimeter buffer must be set back 50' from the PUD boundary. Uses not compatible with adjoining existing residential uses must be screened with landscaping.

COMPOSITION STANDARDS

PRIMARY STREET NETWORK The Primary Streets are as identified on the Land Use Plan on the preceding page and as may be confirmed or modified by Supplemental TIAs. In this Subarea, the primary street network provides an east/west connection to the proposed Exit 4A as a boulevard. Parking is not required along this segment of the primary network, but may be provided in either pockets or complete segments.

SECONDARY STREET NETWORK The secondary network should be designed to create developable blocks as indicated on the Land Use Plan or to outline planned open space and incorporate on-street parking where it can serve as a shared parking resource for commercial use, retail, mixed-use, civic, accommodation, residential or institutional uses. The secondary street network will be designed to accommodate traffic generated by uses in accordance with supplemental TIAs.

STREET AND PUBLIC FRONTAGE The relationships between streets and the public frontages should be assembled as follows:

- **Frontages along the Primary Network** – The public frontages will have street trees to reinforce the characteristics of the network in this area.
- **Frontages along the Secondary Network** – The public frontages along the secondary network should be consistent with the primary intended ground level use and its relationship to on-street parking. Areas within this Subarea will have vehicle-oriented uses. For segments intended to have predominately commercial use, retail, mixed-use, civic, accommodation, residential or institutional uses with short term on-street parking, planting strips should be limited and sidewalk paving generally extended to the street edge. For predominately residential segments or areas where on-street parking is not expected to serve as a short-term supply, the frontages should be landscaped.

PEDESTRIAN NETWORK Pedestrian networks will be provided with sidewalks on one or both sides of all Primary and Secondary Streets within this Subarea. Curb extensions or other traffic calming devices may be provided at Secondary Network intersections serving blocks or frontages intended to serve business, civic, accommodation or institutional uses. The pedestrian network should connect uses with open space.

BICYCLE NETWORK Accommodations for bicycles will be provided along or parallel to the Primary Street Network. Shared use of streets will be permitted for all other portions of this Subarea.

PARKING On-street parking may be considered to contribute to fulfilling the parking requirements of commercial or mixed-use development within 400 feet of the parking spaces for non-residential uses. For residential uses, on-street parking may be allocated for required visitor spaces. Off-street parking should have landscape screening along the public frontage.

OPEN SPACES Shared open space should be comprised of at least four (4) separate locations and up to four (4) separate publicly-accessible open space types as indicated by the Land Use Plan. A trail system around this Subarea may be connected in part by public sidewalks.

2.3 PUD SUBDIVISION REGULATIONS AND STANDARDS

2.3.1 General

The PUD Subdivision Regulations and Standards contain the rules used to review and approve proposals that will be submitted for PUD Subdivision approvals by the Town of Londonderry. Because PUD Subdivision Plans within the Woodmont Commons PUD are regulated by the PUD Master Plan and under the control of the Developer, submittals will require review and written approval for consistency with the PUD Master Plan by the Developer prior to being submitted to the Town for review. Submittals may also be made by the Developer, and as such, are subject to the same confirmation standards.

PUD Subdivision Plans include infrastructure projects that establish new property lines and appropriate access and service, prior to individual building or site-specific improvement projects, which are addressed in *Section 2.4 PUD Site Plan Regulations and Standards*. PUD Subdivision Plans are subject to the following:

- PUD Subdivision Plans may be submitted for portions of Subareas, for entire Subareas or for land contained within multiple Subareas.
- PUD Subdivision Plans may include buildings or other site-specific improvements as part of their submittal. In such cases, the submittals shall be governed by this Section and the buildings or other site-specific improvements shall be governed by *Section 2.4 PUD Site Plan Regulations and Standards*.
- PUD Subdivision Plans must comply with *Section 2.3 PUD Subdivision Regulations and Standards* and with the Allowable Uses and Use and Distribution Standards contained within *Section 2.2 Land Use Regulations*.
- All PUD Plans must address the requirements of this Section including how the submittal will address Open Space requirements.

- Submittals to or by the Developer shall be in accordance with this Section and follow the submittal process outlined in *Section 2.5 Administration*.

Once approval is received from the Developer, submittals to the Town shall be in accordance with the *Londonderry Site Plan Regulations* and/or the *Londonderry Subdivision Regulations*, current as of the date of the Submittal and as modified herein. No submittal to the Town will be accepted without Developer approval.

Note that the Compliance Alternatives in *Section 2.2.1 Description* apply to the PUD Subdivision Regulations and Standards.

2.3.2 Block Types

The Woodmont Commons PUD has five allowable Block Types. Each block has a separate purpose. The Block Types are as follows:

- Village Center;
- Neighborhood;
- Flex;
- Perimeter;
- Perimeter Neighborhood.

The following pages describe each Block Type and provide the relevant standards, including dimensional standards, the Subareas in which the Block Type is allowed, and how vehicles access the block from the street.

Block Assembly Rules

The Block Types are to be combined with the Street Types and Open Space Types to form a PUD Subdivision. The Block Types indicate which Subarea the Block Type is permitted to be used within. Perimeter Blocks and Perimeter Neighborhood Blocks are intended to be used along the PUD Boundary where connections to the adjacent property is restrained either by adjacent lots, a physical constraint (i.e. I-93) or the need to reduce the amount of connectivity to adjacent residential areas.

Open Space

Open space, as defined in *Section 2.3.6 Conserved Green Space and Shared Open Space Standards*, including Conserved Green Space and Shared Open Space may be incorporated into any Block Type, or may be contained within an independent block without restrictions on the size and dimension of the block.

Surface Parking

Surface parking may be located within the Private Frontage on Flex blocks, or portions thereof, in Subareas WC-1-GL and WC-2, and in any block in

Subarea WC-12 subject to the following:

- Surface parking is not permitted within the Private Frontage, except as permitted in *Section 2.4.2 Buildings and Lots*, on a Primary street as delineated on the Land Use Plan in *Section 2.2.2 Land Use Plan* and *Section 2.2.4 Subarea Composition Principles and Standards*;
- The use associated with the surface lot must be non-residential or multi-family;
- The Lot Type setbacks for building placement on lot and parking placement on lot as noted in *Section 2.4.2 Buildings and Lots* shall be increased to allow for parking;
- New surface parking shall be set back from the right-of-way a minimum of six (6) feet;
- New Public Frontage shall meet the requirements of *Section 2.3.3 Transportation Network* for the use(s) being served;
- New surface parking may be located to the front of buildings in Flex Blocks and shall be screened and landscaped in accordance with *Section 2.4.6 PUD Site Plan Landscape*;
- Parking may be designed so that buildings may be accommodated over time.

Other Assembly Rules

Other assembly rules include the following:

- Village Blocks with block faces exceeding 500 feet shall have a Mid-Block crossing;
- Any block face over 1,000 feet shall include a Mid-Block pedestrian crossing or other speed management devices;
- Any block dimension can be increased by 50% if a Mid-Block crossing is connected to an internal Recreation Trail;
- Blocks may be separated by Recreation Trails rather than streets;
- Speed management devices on a Perimeter

Block or Perimeter Neighborhood Block may include a cross street that intersects with the street along the Block but that does not intersect with the Block itself;

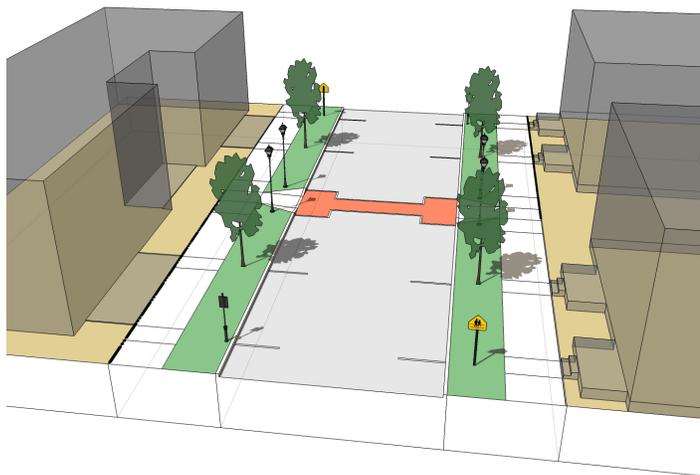
- Blocks abutting or containing Conserved Green Space may be increased in length to preserve the Conserved Green Space.

Speed Management Devices

Speed management devices include methods from *Designing Walkable Urban Thoroughfares: A Context Sensitive Approach, An ITE Recommended Practice, 2010*.

Active methods include:

- Lateral shifts or narrowing (curb extensions with a center island or other techniques that require vehicles to move out of a straight path or create neckdowns);
- Smaller curb-return radii to slow turning vehicles and the elimination of free-flow channelized right-turn lanes;
- Provision of on-street parking where adjacent land uses and activities will generate demand;
- Speed cushions or speed platforms (less impact



Example of a Mid-Block Crossing

A mid-block crossing provides a safe method for pedestrians to cross when a longer block is required. A mid-block crossing may connect to another Block Type, a Recreation Trail or a Shared Use Path.

on emergency vehicles than hump and tables);

- Narrowed travel lanes;
- Raised crosswalks combined with curb extensions to narrow street.

Passive methods include:

- Visually narrowing road using pavement markings;
- Visually enclosing street with buildings, landscaping and street trees.

Mid-Block Crossings

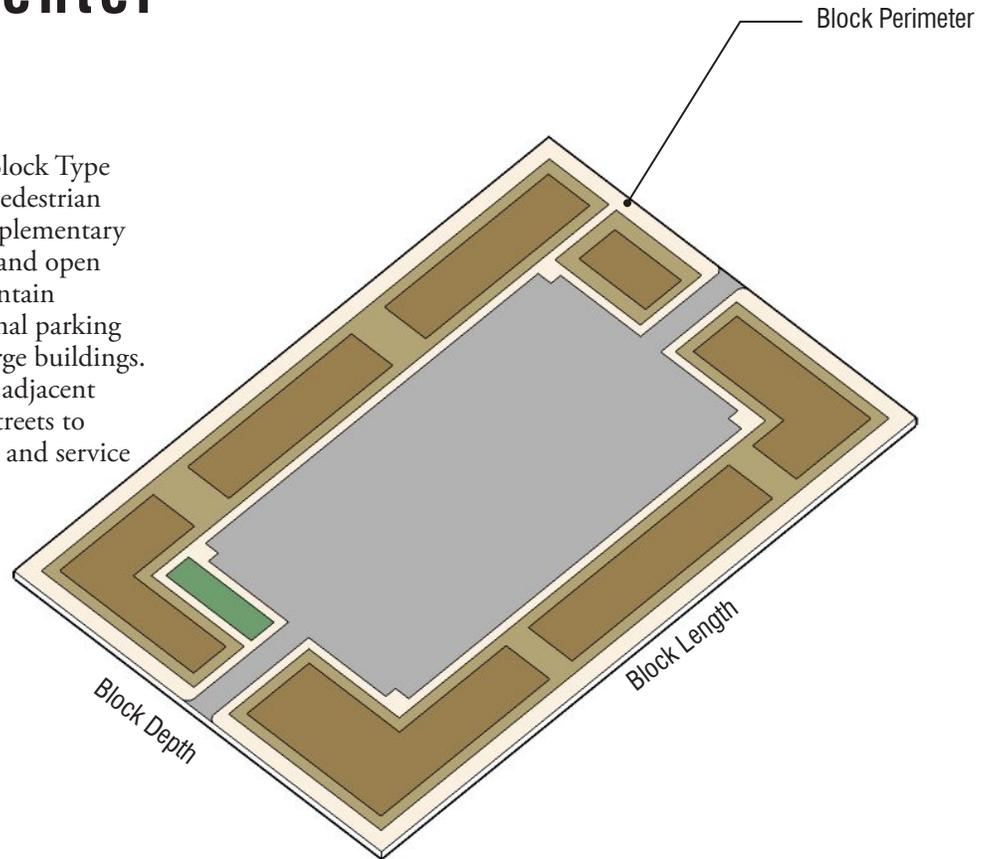
Mid-Block crossing criteria include:

- Mid-Block crosswalks should be located at least 100 feet from an intersection;
- May be located on streets with an average daily traffic volume (ATD) of 12,000 vehicles per day or less;
- May be located on multi-lane streets with less than 15,000 ATD if a raised pedestrian refuge median is provided;
- Serves a minimum pedestrian crossing volume of 25 pedestrians per hour for at least four hours of a typical day;
- Note that the requirement for speed management devices may be omitted if found by the Town to be in conflict with public safety access interests or public maintenance requirements.

2.3.2 Block Types

BLOCK TYPE: Village Center

DESCRIPTION This Block Type will create an attractive pedestrian environment with a complementary mix of buildings, streets and open space. The Block may contain perimeter lots with internal parking for a range of small to large buildings. Site circulation connects adjacent primary and secondary streets to internal parking, loading and service areas.

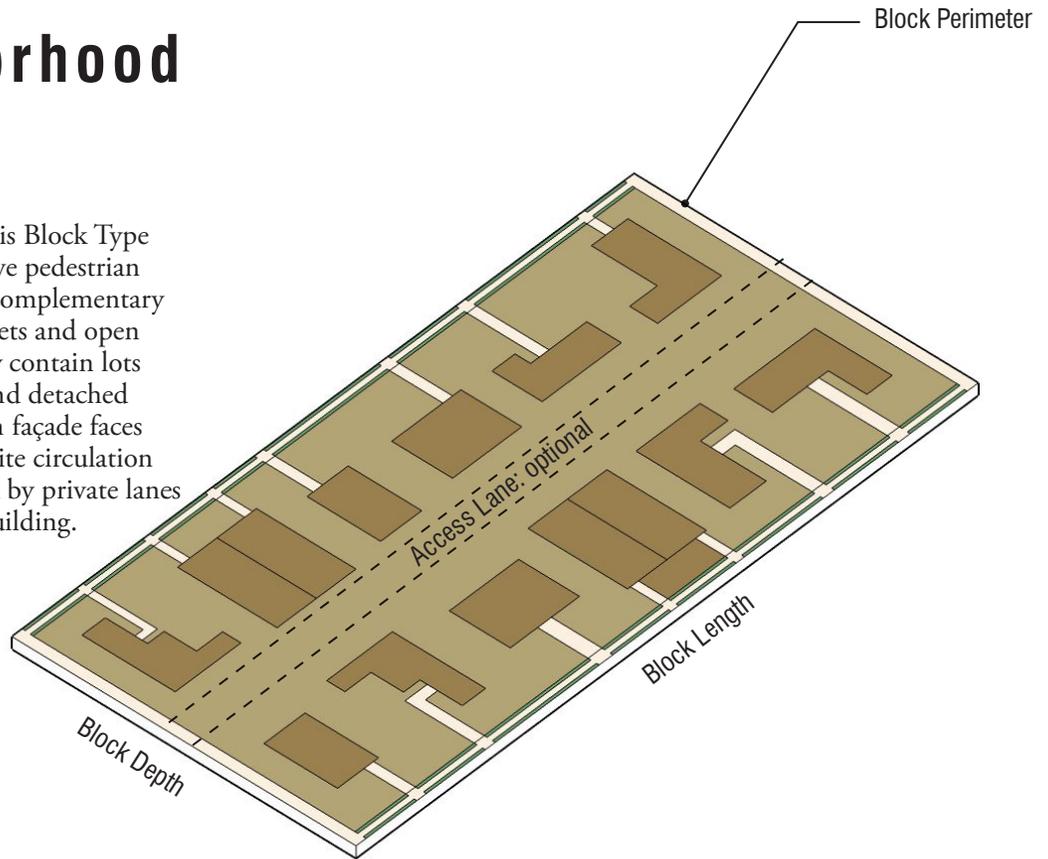


SIZE AND DIMENSION	
Block Perimeter – Maximum	2,400 linear feet 3,500 linear feet with structured parking in WC-12 only
Block Face – Maximum	600 feet
ACCESS AND SERVICE	
Primary Entry Orientation	To street
Service Area/Route	Block can be permeated by Access Lanes and pedestrian passage On-street loading permitted
Pedestrian Circulation	Block length over 400' shall provide mid-block crossing/passage
OPEN SPACE	
Open Space Types Allowed	All - no limit
Open Space Required	Refer to PUD Subarea rules and requirements
SUBAREAS	
Allowed in Subarea(s)	WC-1-GL, WC-1, WC-2, WC-8, WC-10, WC-12

2.3.2 Block Types

BLOCK TYPE: Neighborhood

DESCRIPTION This Block Type will create an attractive pedestrian environment with a complementary mix of buildings, streets and open space. The Block may contain lots with both attached and detached buildings whose main façade faces the principal street. Site circulation may be accomplished by private lanes located behind the building.

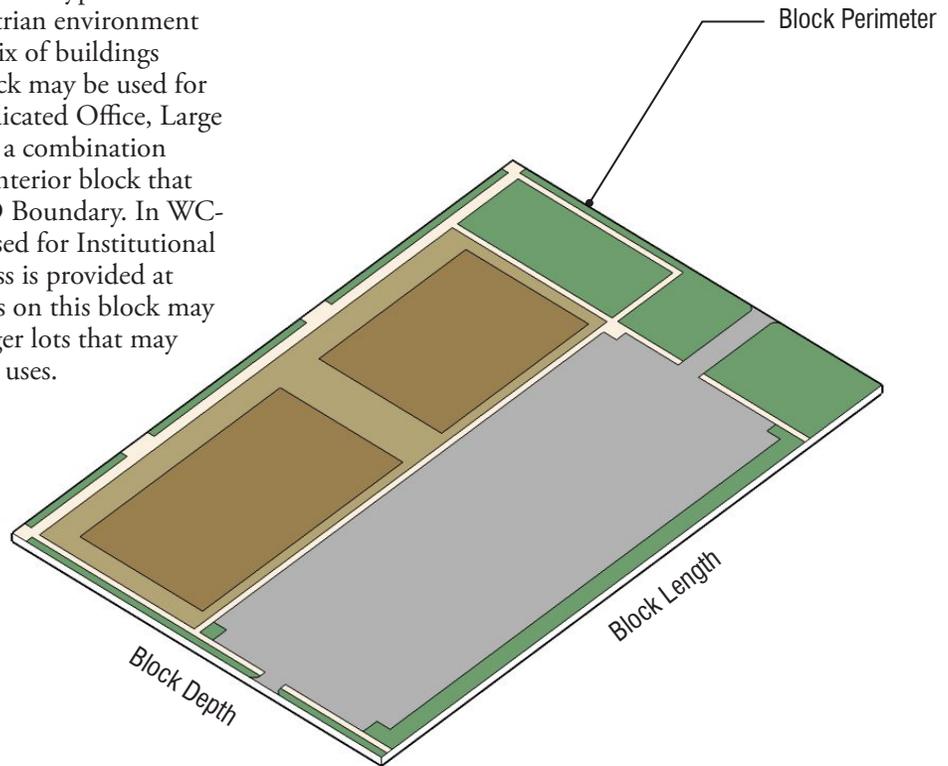


SIZE AND DIMENSION	
Block Perimeter – Maximum	3,000 linear feet
Block Face – Maximum	750 feet
ACCESS AND SERVICE	
Primary Entry Orientation	To street
Service Area/Route	Block can be permeated by Access Lanes and pedestrian passage
Pedestrian Circulation	At block perimeter and intersections
OPEN SPACE	
Open Space Types Allowed	All - no limit
Open Space Required	Refer to PUD Subarea rules and requirements
SUBAREAS	
Allowed in Subarea(s)	WC-1, WC-2, WC-4, WC-6, WC-7, WC-8, WC-9, WC-10, WC-11, WC-12

2.3.2 Block Types

BLOCK TYPE: Flex

DESCRIPTION This Block Type will create an attractive pedestrian environment with a complementary mix of buildings and streetscapes. The Block may be used for Large Format Retail, Dedicated Office, Large Buildings, or blocks with a combination of uses. This block is an interior block that should not abut the PUD Boundary. In WC-12, Flex blocks may be used for Institutional Buildings. Vehicular access is provided at adjacent street edges. Lots on this block may be combined to form larger lots that may share building or parking uses.



SIZE AND DIMENSION

Block Perimeter – Maximum	4,000 linear feet
Block Face – Maximum	800 linear feet

ACCESS AND SERVICE

Primary Entry Orientation	To street
Service Area/Route	Off-street or Access Lane or Internal Parking
Pedestrian Circulation	At block perimeter and intersections

OPEN SPACE

Open Space Types Allowed	All - no limit
Open Space Required	Refer to PUD Subarea rules and requirements

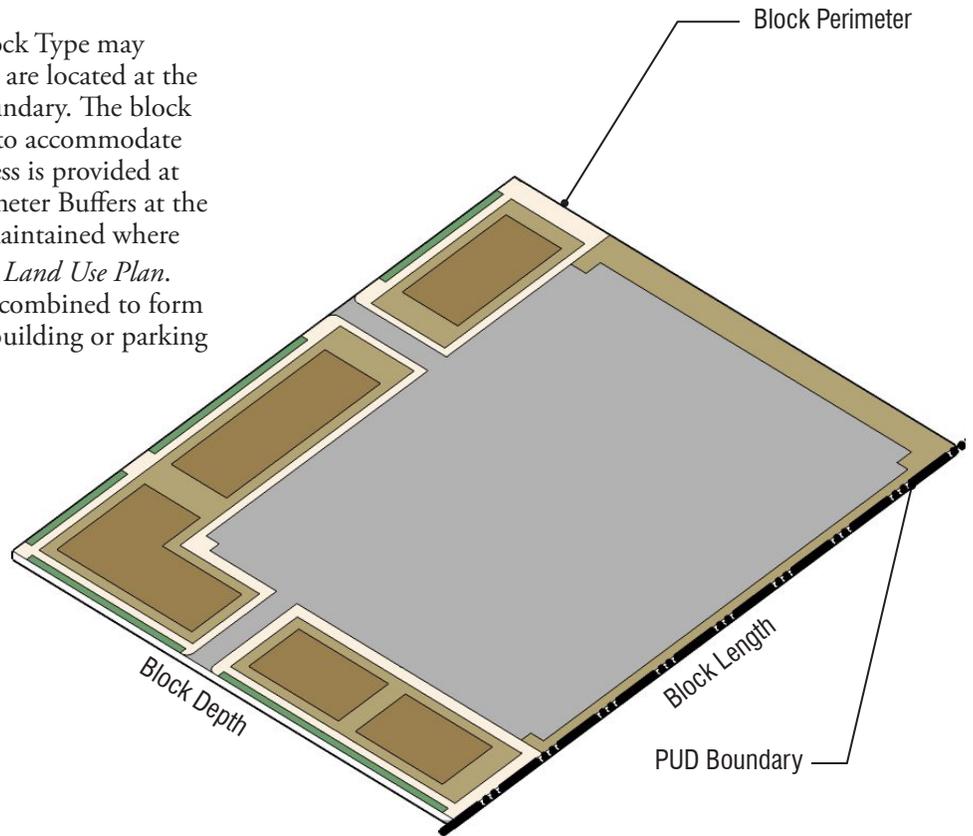
SUBAREAS

Allowed in Subarea(s)	WC-1-GL, WC-1, WC-2, WC-3, WC-12
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2.3.2 Block Types

BLOCK TYPE: Perimeter

DESCRIPTION This Block Type may contain perimeter lots that are located at the perimeter of the PUD Boundary. The block may be irregularly shaped to accommodate perimeter conditions. Access is provided at adjacent street edges. Perimeter Buffers at the PUD Boundary shall be maintained where indicated in *Section 2.2.2 Land Use Plan*. Lots on this block may be combined to form larger lots that may share building or parking uses.

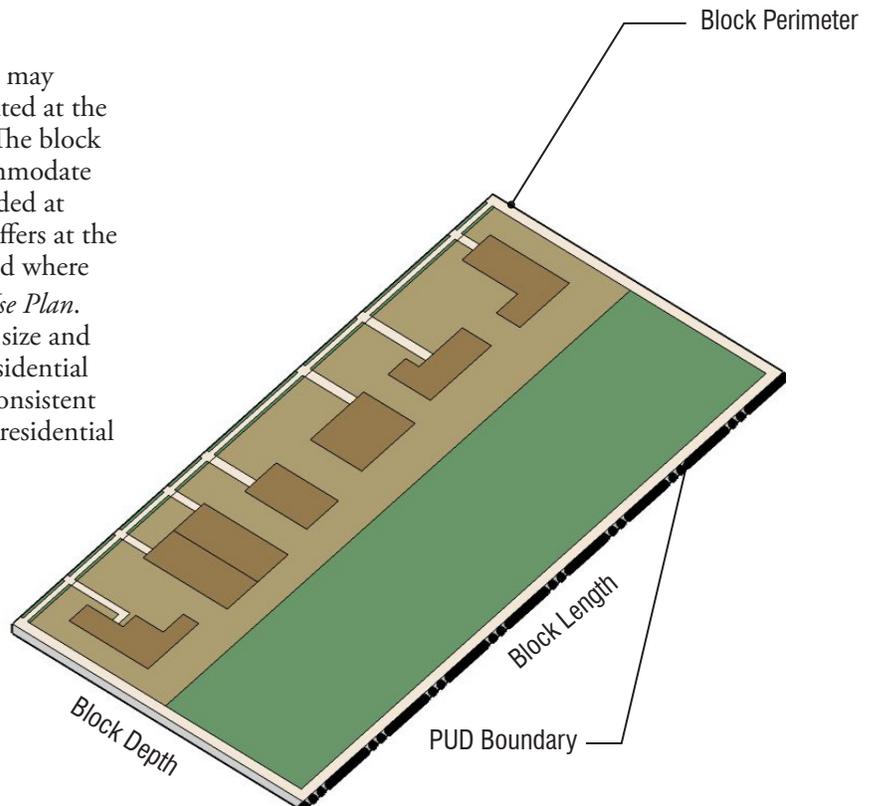


SIZE AND DIMENSION	
Block Perimeter – Maximum	None
Block Face – Maximum	None
ACCESS AND SERVICE	
Primary Entry Orientation	To street
Service Area/Route	Off-street or Access Lane or Internal Parking
Pedestrian Circulation	At block perimeter and intersections
OPEN SPACE	
Open Space Types Allowed	All - no limit
Open Space Required	Refer to PUD Subarea rules and requirements
SUBAREAS	
Allowed in Subarea(s)	WC-1-GL, WC-1, WC-2, WC-12

2.3.2 Block Types

BLOCK TYPE: Perimeter Neighborhood

DESCRIPTION This Block Type may contain perimeter lots that are located at the perimeter of the PUD Boundary. The block may be irregularly shaped to accommodate perimeter conditions. Access provided at adjacent street edges. Perimeter Buffers at the PUD Boundary shall be maintained where indicated in *Section 2.2.2 Land Use Plan*. Lots should be consistent with the size and distribution of adjacent existing residential lots. Building setbacks should be consistent with setbacks for adjacent existing residential buildings.



SIZE AND DIMENSION	
Block Perimeter – Maximum	None
Block Face – Maximum	None
ACCESS AND SERVICE	
Primary Entry Orientation	To street
Service Area/Route	Block can be permeated by Access Lanes; Residential may be front-loaded
Pedestrian Circulation	At block perimeter and intersections
OPEN SPACE	
Open Space Types Allowed	All - no limit
Open Space Required	Refer to PUD Subarea rules and requirements
SUBAREAS	
Allowed in Subarea(s)	WC-1, WC-2, WC-4, WC-5, WC-6, WC-7, WC-9, WC-11, WC-12

2.3.3 Transportation Network

Performance and Operational Standards

The MTIA provided in *Section 4.2 Master Plan Traffic Impact Assessment* provides the baseline and future-year assessments of traffic accommodations for the PUD. The key assumptions are listed as they relate to the study area, trip generation, local and regional distribution of trips, the PUD's relationship to background improvement projects such as Exit 4A, and conceptualized mitigation measures. The planned roadway network within the PUD encourages connectivity between existing and proposed streets as a means of distributing trips and providing route options.

The Woodmont Commons PUD will be constructed over the next 20 years. As PUD Subdivisions and/or PUD Site Plans are proposed, applicants shall submit updated traffic data, if needed, and analysis if the project exceeds thresholds as described in *Section 3.1 Transportation*. The Planning Board shall use the MTIA in *Section 4.2 Master Plan Traffic Assessment* as a document of record by which future studies may be compared for conformance with the PUD Master Plan.

The projected transportation impacts of the PUD are thoroughly examined within the attached traffic studies in *Section 4.2 Master Plan Traffic Assessment* provided by the applicant and subsequently reviewed by the Town's peer review agent. The land use densities in each Subarea, assuming the conditions with and without the availability of Exit 4A as shown in *Section 2.2.3 Land Use Standards*, have been derived from a careful assessment of the associated trip-making characteristics. These record traffic studies shall be used as a guide for the Planning Board to assess the reasonableness of mitigation as various PUD Subdivision and/or Site Plan proposals are presented.

The Developer has identified traffic improvement measures for each PUD Subdivision that would reasonably mitigate the capacity impacts for those in-

tersections that are overcapacity as defined by the level of service and volume-to-capacity ratio. Applicants for PUD Subdivision and/or PUD Site Plan approval and the Town shall work cooperatively to implement the improvement measures that reasonably satisfy the Town's historic corridor plans and the Developer's capacity needs as defined by the Development Agreement.

Streets

The PUD Master Plan defines a variety of allowable Street Types in combination with Public Frontage Types that are suitable for multimodal transportation including pedestrian traffic. In order to maintain safety and accessibility for pedestrians and bicyclists, traffic must be slowed down, and most of the streets in the Woodmont Commons PUD will be designed to encourage lower vehicle speeds.

The transportation network will include regular geometric patterns of Street Types and Block Types arranged to provide comprehensible and diverse routes of travel within a hierarchy of streets – some varied and convenient for a balanced mix of both pedestrians and automobiles while others are designed to carry greater traffic and serve as the major boulevards, avenues, or commercial main streets.

The streets in the PUD will not be classified according to conventional functional classification, nor be subject to any requirements pertaining to such classifications. The existing system of functional classification defines three types of streets (local, collector, arterial) each for two types of environments: urban and rural. Within the Woodmont Commons PUD, the design of the streets will be informed by the context of the Subareas in which they reside. The actual street composition will be determined at the time of PUD Subdivision submittal in accordance with the design standards within this Section and any Supplemental TIA. Street types are to be combined with frontage types to make up the public right-of-way.

2.3.3 Transportation Network

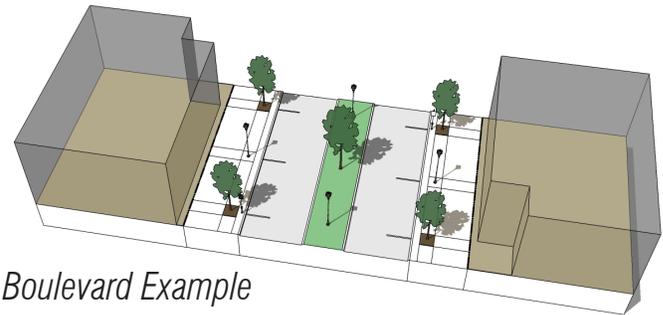
The Woodmont Commons PUD will include streets that are integral components of community design. Similar to the ITE's *Context Sensitive Solutions in Designing Major Urban Thoroughfares for Walkable Communities*, context sensitive solutions are used in the planning and design of streets for walkable communities. This results in streets that serve all users and are compatible with the surroundings through which they pass. The street design should complement buildings, public spaces and landscape, as well as support the human and economic activities associated with adjacent and surrounding land uses. The PUD will be designed to accommodate future transit service, if and when available, as well as efficient pedestrian and bicycle connections between the existing Park-n-Ride facility and adjacent Subareas.

The intent is to provide enough capacity for traffic related to the use and density of each Subarea and for the PUD as a whole. Each Street Type has its own set of standards, for example, total width, curb radius, travel lane width, presence of a median, on-street parking. Street Types and Public Frontage Types, in assembled combinations, also include pedestrian infrastructure, bicycle infrastructure, and landscaping to meet the needs of different areas.

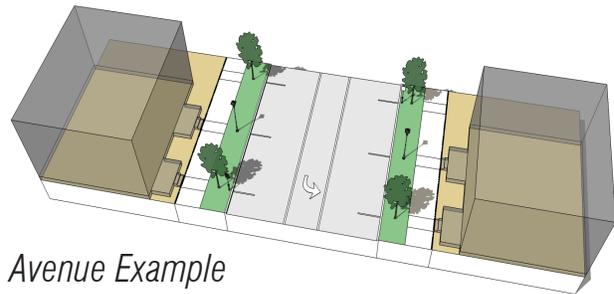
Street Types

The components of the Street Types in the Woodmont Commons PUD include travel lanes, medians, bike lanes, on-street parking and curbs. The minimum lane widths are shown within the Street Types diagrams. However, the actual lane widths may be wider and the number of lanes may be increased as determined by Supplemental TIAs to accommodate the transportation and emergency access needs. Street Types include the following:

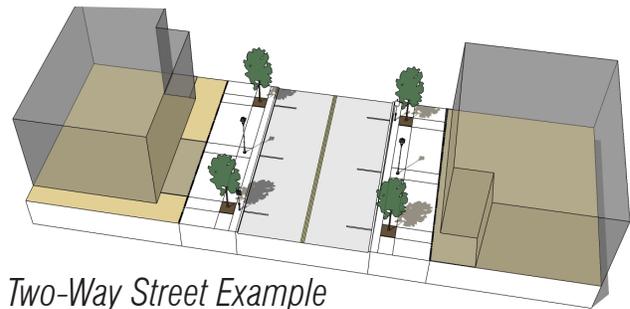
- A Boulevard is generally a larger multi-lane and generally urban corridor with a central, planted median, its designator is "B."
- An Avenue is a connector which may be have



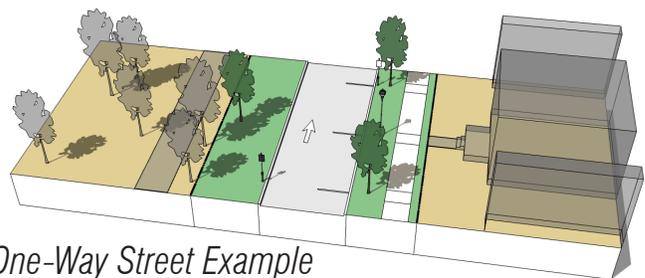
Boulevard Example



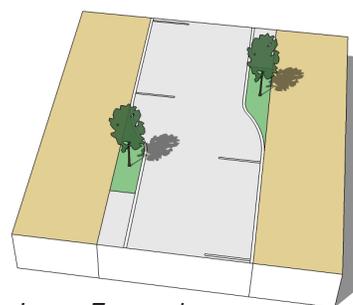
Avenue Example



Two-Way Street Example



One-Way Street Example



Access Lane Example

2.3.3 Transportation Network

multiple lanes that runs through a community; its designator is “A.”

- A Street may allow one-way or two-way vehicular travel, is of closed-section (curb and gutter) design, will predominately have sidewalks on both sides, does not have a central median or “refuge” area, and is the most common corridor in the Woodmont Commons PUD; its designator is “2W” for two-way streets and “1W” for one-way streets.
- An Access Lane is found to the rear of lots, has no sidewalks or setbacks, is designed as the service routes for the lots they abut, and is typically constructed with inverted crowns and center drainage, but may be constructed otherwise where lower density residential lots only are serviced; its designator is “L.”

Public Frontage Types

The public frontage is the layer between the lot line and the edge of the vehicular lanes. The public frontage may include sidewalks, street planters, trees and other vegetated landscaping, utilities, benches, street signage, lampposts, and other street furniture. There are six Public Frontage Types as defined on pages 128-129.

Sidewalk space shall be managed in a way that promotes pedestrian safety, efficiency and comfort and provides adequate space for street trees. Sidewalks should enhance the visual character of streets with landscaping used to reduce the impacts of vehicle traffic. In commercial areas, the sidewalk zone closest to the building may be used for merchandising and outdoor dining and seating. The sidewalk zone along the curb includes the area for street trees and furniture, however, in all cases, a clear pedestrian zone of five (5) feet minimum must be maintained along the

path of travel.

Buildings on corner lots shall have two public frontages.

Private Frontage Types

The private frontage is the privately owned layer between the building façade and the lot line. Private frontages may include encroachments and yards. Private frontages are an important part of the context when designing for and creating visually cohesive streets. This frontage is based upon the use located within the lot. There are seven Private Frontage Types as defined on pages 130-131.

Buildings on corner lots shall have two private frontages.

Intersections

Offset intersections: Street intersections from opposite sides are permitted to be offset.

- Angles at intersections – Unless alternative (lesser) intersection alignments and angles are approved by the Planning Board during PUD Subdivision and/or PUD Site Plan review, intersections shall be designed with roadway centerlines intersecting at 90 degrees. Site-specific traffic controls shall include, without limitation, traffic signal control, all-way stop control, or other means of limiting conflicts associated with a one-way flow pattern of certain PUD streets.
- Right-of-way radii – Not required.
- Pavement corner radii – Minimum edge of pavement radii for intersection rounding corners shall be in accordance with *Section 2.3.3 Street Assembly Examples*.
- Intersection sight triangles – The minimum sight triangle for stop conditions at street inter-

2.3.3 Transportation Network

sections shall be 70 feet along the major road right of way and 10 feet along the minor road right of way. The intersection sight triangle shall be permanent right of way. This is permitted to be reduced for lower design (below 25 mph) speeds on streets.

- E. On-street parking – On-street parking shall be held back from intersections in order to increase the effective turning radius. Parking shall be restricted by posting, marking or striping.
- F. Intersection Sight Distance Guidelines – Intersection Sight Distance (ISD) represents the length of the roadway visible to a driver waiting to exit a driveway or minor street. ISD at intersections within the Woodmont Commons PUD should meet American Association of State Highway and Transportation Officials (AASHTO) guidelines for desirable ISD for the design speed wherever practicable. In denser locations where on-street parking or buildings located close to the roadway may restrict sight lines, the ISD shall meet the guidelines for minimum Stopping Sight Distance (SSD) for the design speed. ISD shall be measured from an eye height of 3.5 feet to an object height of 3.5 feet, and measured from a distance 15 feet off the edge of the travel-way of the major roadway to represent a driver waiting to exit a driveway or minor roadway.
1. On-street parked vehicles will not be considered an obstruction to ISD so long as on-street parking is prohibited within 20 feet of the minor street and at least 12 feet of travel-way is provided adjacent to the on-street parking stalls to allow a vehicle exiting a driveway or minor street to pull slightly into the roadway to see beyond a parked vehicle. On-street parking closer than 20 feet to the minor street will be considered an obstruction to ISD.
 2. For signalized intersections or unsignalized intersections with all-way STOP control, the

ISD measured in either direction on each approach shall extend to a point at least 10 feet behind the STOP line on each of the other approaches to ensure that a driver stopped on any approach can see a vehicle stopped on any of the other approaches.

3. For unsignalized intersections with STOP control on one or more approaches and free-flowing traffic on one or more approaches, ISD exiting the STOP-controlled approaches should meet the following guidelines for the given design speed.

Operating Speed (Designed)	AASHTO Guidelines (in feet)
10 mph	88
15 mph	80
20 mph	115
25 mph	155
30 mph	200
35 mph	250

- G. Sight distances do not apply at signalized intersections. Vertical curves and superelevation are not needed (or desired) for design speeds under 35 mph.

Street Assemblies

The Street Types, and Public Frontage Types and Private Frontage Types can be assembled along with the appropriate Block Types and Building/Lot Types for the related Subarea and particular proposed uses. Context is the primary method of assembling the streets. The design should complement the buildings, public spaces and landscape, as well as support the human and economic activities associated with adjacent and surrounding land uses. The intent is to build streets that are integral components of community design within the Woodmont Commons PUD. The streets serve as primary pedestrian access to buildings and spaces and should be scaled for pedestrians. Capacity

2.3.3 Transportation Network

needs identified in the MTIA and any Supplemental TIAs should also be considered in the assembly.

Each street will have its own dimensional requirements. For example, an expanded sidewalk with landscape strip might be paired with 2-way street – 2-lane Section in a multi-family residential area. An expanded sidewalk would not be paired with a 2-way street in a single family residential area.

Materials, Treatments and Construction

The dimensional standards for the streets and sidewalks within the Woodmont Commons PUD shall be in accordance with the PUD Master Plan. The materials and construction methods for the streets and sidewalks shall be in accordance with the *Londonderry Subdivision Regulations* and the *Typical Details for Site and Roadway Infrastructure* except that streets and sidewalks may have treatments that are not asphalt or concrete and may include brick or cobblestone. Vertical granite curbs will be used within the PUD in accordance with the *Londonderry Subdivision Regulations* and the *Typical Details for Site and Roadway Infrastructure*. Other treatment materials and/or treatments may also be used as appropriate for the neighborhood and shall be subject to review at the time of PUD Site Plan or PUD Subdivision submittal.

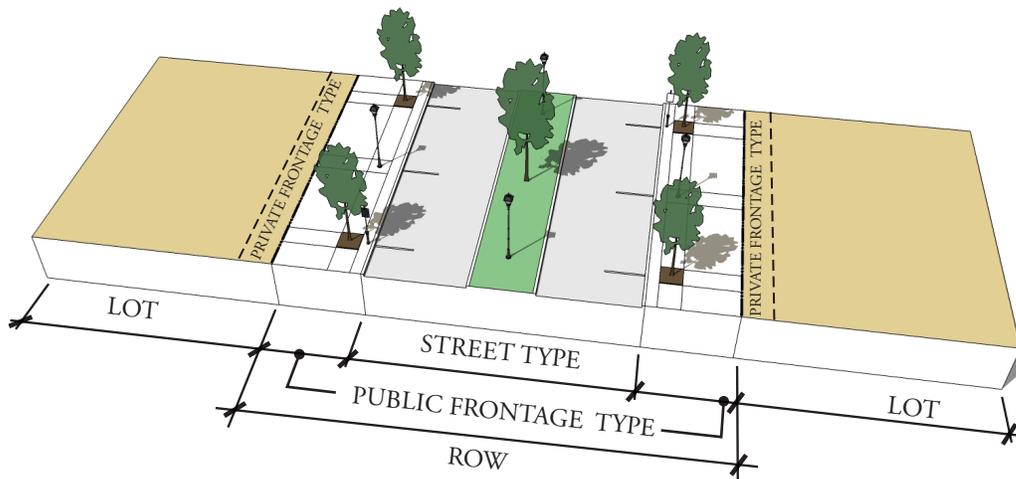
Streets that are intended to be accepted as public streets must have a travel way with a minimum width of twenty-four (24) feet and conform to the standards within the Town of Londonderry *Typical Details for Site and Roadway Infrastructure* dated May 2009. The structural capacity of streets shall meet or exceed Town of Londonderry standards.

Any construction details pertinent to the Woodmont Commons PUD which are not specifically included in the Londonderry or NHDOT standards shall be subject to review at the time of PUD Site Plan or PUD Subdivision submittal.

2.3.3 Transportation Network

STREETS | COMPONENTS

Standards



STREET DIAGRAM

DESCRIPTION Street rights-of-way are composed of the Street Types and Public Frontages. They are assembled from the following components based on their context, the traffic capacity they need to serve and the uses that are located on the lots along the streets. The Street Type dimension is to the back of the curb. The Street Types and Public Frontages may change along the street length according to the following:

- A. The number of travel lanes may be reduced or increased after an intersection, as long as the traffic capacity and emergency access needs are served;
- B. Turn lanes may be incorporated into medians;
- C. A one-way street or median-divided lane of more than 300 feet in length should have a minimum width of 16-18 feet;
- D. The public frontage may change after a street intersection when the uses warrant a different type;
- E. The selection of the public frontage type is based upon the building uses and their private frontages;
- F. The public frontages may differ from one side of the street to the other depending upon the uses served;
- G. Bike lanes may be added to Street Types. Bike way types may vary along the street length depending upon vehicular speed and the availability of alternate trails or means of connection;
- H. Parking may be unmarked in single family residential areas. On-street parking may vary depending on street location and frontage adjacencies;
- I. The location of on-street parking may vary along the street length.

MINIMUM STREET DESIGN STANDARDS

Travel Lanes (feet)	10 minimum; where striped, lanes can be greater than or equal to 10
Parking Lanes (feet)	8 minimum
Median Width (feet)	6 minimum
Bike Way Width (feet)	5 where applicable

2.3.3 Transportation Network

STREETS | COMPONENTS

Types

FRONTAGE TYPES		STREET TYPES					
							BOULEVARD
							BOULEVARD
							AVENUE
							TWO WAY
							TWO WAY
							ONE WAY
							ACCESS LANE

PRIVATE

PUBLIC

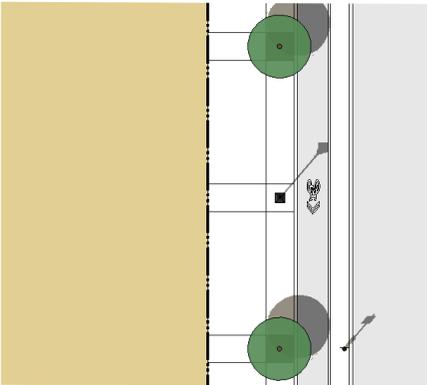
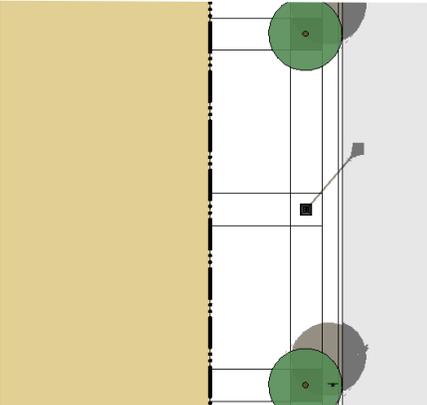
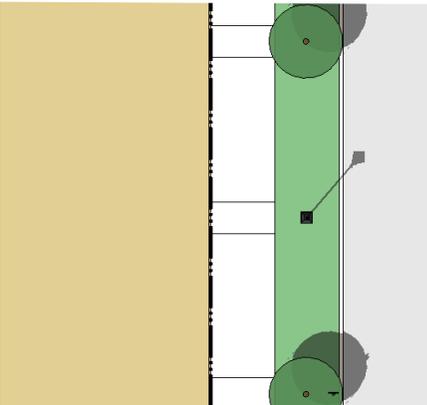
DESCRIPTION The illustrations above show examples of each type of assembled street. Street Assemblies are to be made up of the components above, the Public Frontage Types shown on pages 128-129, and the Private Frontage Types on pages 130-131.

Three-dimensional examples of assembled streets and the related dimensional standards are found on pages 132-143. These street examples are not intended to limit the number and types of streets in Woodmont Commons.

2.3.3 Transportation Network

STREETS | COMPONENTS

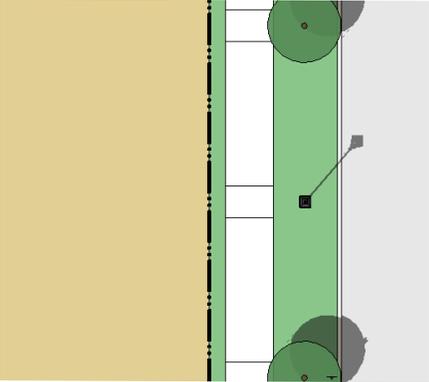
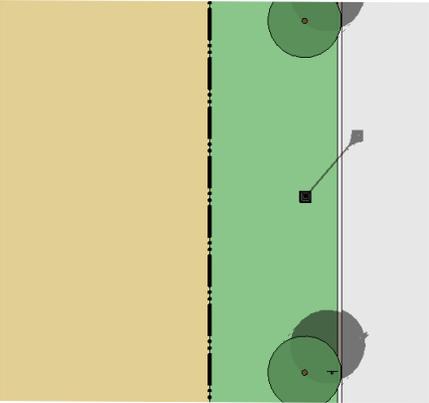
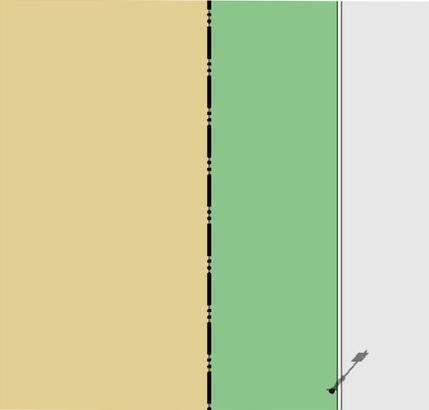
Public Frontage Types

Plan		Frontage Description
LOT PRIVATE FRONTAGE	ROW PUBLIC FRONTAGE	
		<p>Public Frontages Type 1.</p> <p>Location: This Frontage is to be used with Primary Streets where a raised cycletrack may be used. The Frontage consists of the walk and cycletrack with street trees in tree wells.</p> <p>Uses: Commercial Uses and Residential, Civic and Accommodation Uses when located within a mixed-use area or mixed-use building.</p> <p>Dimensions: Typically eight (8) to ten (10) feet of walk and seven (7) to eight (8) feet of cycletrack and signage zone at the curb.</p>
		<p>Public Frontages Type 2.</p> <p>Location: This Frontage is to be used with Primary or Secondary Streets. The Frontage consists of the walk with street trees.</p> <p>Uses: Commercial Uses and Residential, Civic and Accommodation Uses when located within a mixed-use area or mixed-use building.</p> <p>Dimensions: Typically twelve (12) to sixteen (16) feet of walk.</p>
		<p>Public Frontages Type 3.</p> <p>Location: This Frontage is to be used with Primary or Secondary Streets. The Frontage consists of the walk with street trees in a continuous planting strip.</p> <p>Uses: Residential Uses and Commercial, Civic and Accommodation Uses not located within a mixed-use area.</p> <p>Dimensions: Typically seven (7) to eight (8) feet of walk and seven (7) to eight (8) feet of continuous planting strip at the curb.</p>

2.3.3 Transportation Network

STREETS | COMPONENTS

Public Frontage Types

Plan		Frontage Description
LOT PRIVATE FRONTAGE	ROW PUBLIC FRONTAGE	
		<p>Public Frontages Type 4.</p> <p>Location: This Frontage is to be used with Primary or Secondary Streets. The Frontage consists of the walk with street trees in a continuous planting strip. The planting strip may include some stormwater features such as rain gardens.</p> <p>Uses: Residential Uses and Commercial, Civic and Accommodation Uses not located within a residential Subarea.</p> <p>Dimensions: Typically two (2) to three (3) feet of planting strip at the right-of-way line, five (5) to six (6) feet of walk and six (6) to eight (8) feet of continuous planting strip at the curb.</p>
		<p>Public Frontages Type 5.</p> <p>Location: This Frontage is to be used with Primary or Secondary Streets. The Frontage consists of street trees in a continuous planting strip. The planting strip may include some stormwater features such as swales or rain gardens.</p> <p>Uses: Residential Uses facing on to existing streets where no sidewalk is desired, and Shared Open Space areas.</p> <p>Dimensions: Typically ten (10) to sixteen (16) feet of continuous planting strip.</p>
		<p>Public Frontages Type 6.</p> <p>Location: This Frontage is to be used with Primary or Secondary Streets. The Frontage consists of a continuous planting strip. The planting strip may include some stormwater features such as swales or rain gardens.</p> <p>Uses: Residential Uses facing on to existing streets where no sidewalk is desired and trees are located within a perimeter buffer; Shared Open Space areas with trees located within the lot.</p> <p>Dimensions: Typically ten (10) to sixteen (16) feet of continuous planting strip at the curb.</p>

General Notes

1. The Public Frontages contribute to the character and walkability of the Subareas. Increased dimensions are permitted for any Frontage Type. Decreased dimensions may be permitted on Secondary Streets of limited length where not required for connectivity.
2. Access Lanes do not require Public Frontages.
3. Street trees, where provided, may vary in species. See *Section 2.3.9 PUD Subdivision Landscape* for additional requirements pertaining to street trees.

2.3.3 Transportation Network

STREETS | COMPONENTS

Private Frontage Types

Private Frontages – General. The Private Frontage is the area between the building façade and the lot line.

Plan		Frontage Description
LOT PRIVATE FRONT-AGE	ROW PUBLIC FRONTAGE	
		<p>Private Frontages Type 1. Description: A frontage wherein the façade is at or aligned close to the lot line with the building entrance at sidewalk grade. The private frontage should be predominantly paved but may include intermittent sections of landscaping. This frontage may have encroachments into the right-of-way. Use with: Public Frontage Types 1, 2 Uses: Commercial Uses, particularly retail, and Residential, Civic and Accommodation Uses when located within a mixed-use area or mixed-use building, medium and large building.</p>
		<p>Private Frontages Type 2. Description: A planted frontage wherein the façade may be set back from the lot line. A wall at the lot line may be used to maintain the street spatial definition. This frontage may have encroachments into the setback. Use with: Public Frontage Types 1, 2 or 3. Uses: Dedicated Office Buildings, Commercial Uses and Residential, Civic and Accommodation Uses when located within a medium to small single use building or a medium to small mixed-use area or mixed-use building.</p>
		<p>Private Frontages Type 3. Location: A frontage wherein the façade is aligned close to the lot line with the first story elevated from the sidewalk sufficiently to secure privacy for the windows. The entrance is usually an exterior stair and landing, stoop or porch. This type is recommended for ground-floor residential use. Use with: Public Frontage Type 3. Uses: Residential Uses for densities above fifteen (15) dwelling units per acre.</p>
		<p>Private Frontages Type 4. Location: A planted frontage wherein the façade is set back from the lot line. The front yard created is visually continuous with adjacent yards, supporting a common landscape. The setback provides a buffer where the higher speed streets occur. Use with: Public Frontage Type 4 or 5. Uses: Residential Uses, and Small Buildings with Commercial, Civic and Accommodation Uses.</p>

2.3.3 Transportation Network

STREETS | COMPONENTS

Private Frontage Types

Private Frontages – General. The Private Frontage is the area between the building façade and the lot line.

Plan		Frontage Description
LOT PRIVATE FRONTAGE	ROW PUBLIC FRONTAGE	
		<p>Private Frontages Type 5.</p> <p>Location: A planted frontage wherein the façade is set back from the lot line and access to the lot is from the front street. The front yard created is visually continuous with adjacent yards, supporting a common landscape. The setback provides a buffer where the higher speed streets occur.</p> <p>Use with: Public Frontage Type 4, 5 or 6.</p> <p>Uses: Residential Uses facing on to existing streets where no sidewalk is desired, and Shared Open Space areas.</p>
		<p>Private Frontages Type 6.</p> <p>Location: A natural area or public area. The frontage may include a trail or shared use path.</p> <p>Use with: Public Frontage Type 4 or 5.</p> <p>Uses: Shared Open Space or Conserved Green Space areas.</p>
LOT PRIVATE FRONTAGE	R.O.W. PUBLIC FRONTAGE	Frontage Description

		<p>Private Frontages Type 7.</p> <p>Location: This Frontage is to be used with Side Streets. The frontage consists of a continuous eight (8) foot planting strip for screening where surface parking occurs in the frontage. The planting strip may include some stormwater features such as swales or rain gardens.</p> <p>Use with: Public Frontage Type 3 or 4.</p> <p>Uses: Large Format Retail, Flex Buildings, and other uses located on a Side Street.</p>
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General Notes

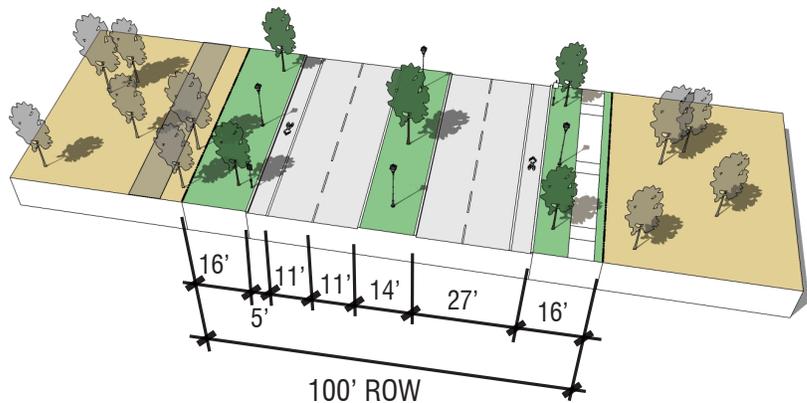
1. See Lot Types for specific building placement requirements.
2. The uses listed are a guideline for the private frontage type. Other uses may be appropriate depending upon the Subarea principles and standards.
3. See Section 2.4.6 PUD Site Plan Landscape for screening and landscaping requirements for surface parking lots.

2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE: Entrance Boulevard

STREET TYPE FRONTAGE TYPE
PUBLIC PRIVATE

- B-2 PF-5/PF-4 PV-6



DESCRIPTION This street is intended for connection to a major route such as Route 102. It is a wider street that accommodates both non-motorized and vehicular traffic with a landscaped median and landscaped pedestrian realm.

STREET DESIGN STANDARDS

Travel Lanes	11' (Turning Lanes 12')
Parking Lanes	None
ROW Width (feet)	100'
Pavement Width	27' on each side of the median
Greenscape Buffer without sidewalk	Up to 16'
Traffic Flow	Two Way
Curb Type	Vertical
Curb Radius (feet)	20'
Vehicular Design Speed	35 mph
Pedestrian Crossing Time	7 seconds curb to median
Road Edge Treatment	Curb
Bike Way Type	Dedicated Lane
Bike Way Width (feet)	5'

PUBLIC FRONTAGE

Planter Type	Continuous
Planting Pattern	Trees at 44' O.C. average
Planter Strip / Box Width	See frontage
Tree Type	Vary species, drought / salt tolerant
Utilities	Underground
Street Light Type	Pedestrian scale ornamental
Street Light Spaces	No limit
Sidewalk Placement	One side
Sidewalk Width (feet)	6'
Sidewalk Encroachment	None

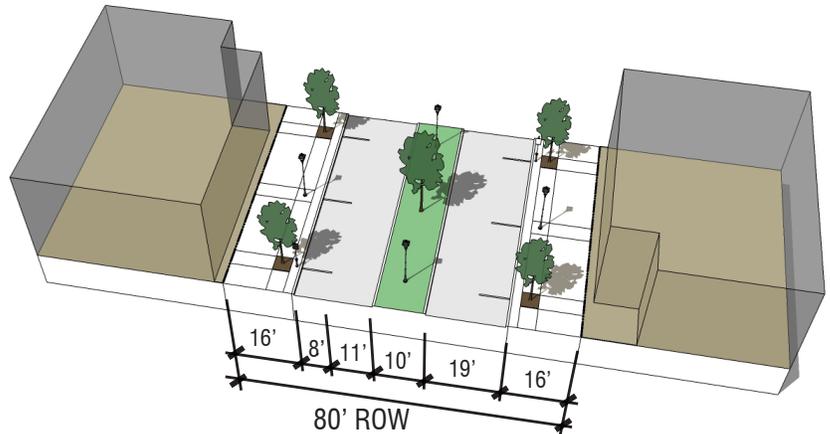
SUBAREAS

Allowed in Subarea(s)	WC-1-GL, WC-12
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2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE: Commercial Boulevard

STREET TYPE	FRONTAGE TYPE	
	PUBLIC	PRIVATE
• B-7	PF-3	PV-1



DESCRIPTION This street is intended for major connecting routes in commercial and mixed-use areas such as the Village Center. It accommodates both non-motorized and vehicular traffic with a landscaped median and expanded pedestrian realm.

STREET DESIGN STANDARDS

Travel Lanes	11'
Parking Lanes	Parallel, both sides, lined
ROW Width (feet)	80'
Pavement Width	19' on each side of the median
Traffic Flow	Two Way
Curb Type	Vertical
Curb Radius (feet)	16'
Vehicular Design Speed	25 mph
Pedestrian Crossing Time	5 seconds curb to median
Road Edge Treatment	Curb
Bike Way Type	Shared lane
Bike Way Width (feet)	None

PUBLIC FRONTAGE

Planter Type	Individual in frontage, continuous at median
Planting Pattern	Trees at 44' O.C. average
Planter Strip / Box Width	4' x 4' with expandable grates
Tree Type	Vary species, drought / salt tolerant
Utilities	Underground
Street Light Type	Pedestrian scale ornamental
Street Light Spaces	44' interval (as per light level)
Sidewalk Placement	Both sides
Sidewalk Width (feet)	16' (extension onto lot permitted)
Sidewalk Encroachment	Seating and signage as allowed

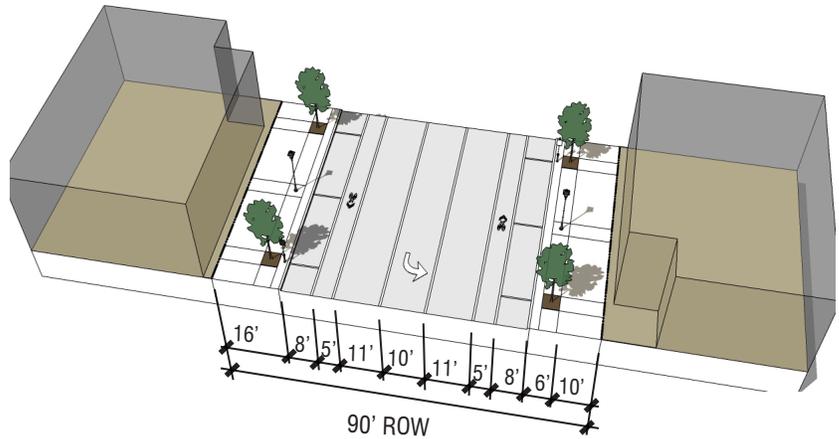
SUBAREAS

Allowed in Subarea(s)	WC-1-GL, WC-1, WC-2, WC-8, WC-10, WC-12
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2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE: Commercial Avenue

STREET TYPE	FRONTAGE TYPE	
	PUBLIC	PRIVATE
• A-1	PF-1	PV-1



DESCRIPTION This is a primary street intended for less intense connections, a simple avenue with variable dimensions.

STREET DESIGN STANDARDS

Travel Lanes	11'
Parking Lanes	Parallel, both sides, lined
ROW Width	90'
Pavement Width	58'
Traffic Flow	Two way
Curb Type	Vertical
Curb Radius (feet)	20'
Vehicular Design Speed	30 mph
Pedestrian Crossing Time	15 seconds
Road Edge Treatment	Curb
Bike Way Type	Bicycle lanes
Bike Way Width	5'

PUBLIC FRONTAGE

Planter Type	Individual in Sidewalk, intermittent median
Planting Pattern	Trees at 44' O.C. average
Planter Strip / Box Width	4' x 4' with expandable grates
Tree Type	Vary species, drought / salt tolerant
Utilities	Underground
Street Light Type	Pedestrian scale ornamental
Street Light Spaces	44' interval (as per light level)
Sidewalk Placement	Both sides
Sidewalk Width	8' (extension onto lot permitted)
Sidewalk Encroachment	Seating and signage as allowed

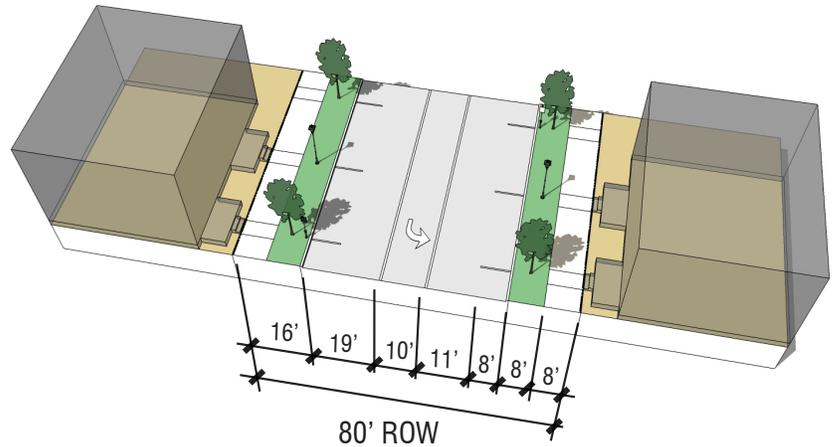
SUBAREAS

Allowed in Subarea(s)	WC-1-GL, WC-1, WC-2, WC-8, WC-12
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2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE: Mixed-Use Avenue

STREET TYPE	FRONTAGE TYPE	FRONTAGE TYPE
	PUBLIC	PRIVATE
• A-2	PF-1	PV-2



DESCRIPTION This is a primary or secondary street intended for well-traveled connections such as Pillsbury Road with frequent site access requirements. It is intended for medium density residential areas and neighborhood service areas.

STREET DESIGN STANDARDS

Travel Lanes	11'
Parking Lanes	Parallel, both sides, lined
ROW Width	80'
Pavement Width	48'
Traffic Flow	Two way
Curb Type	Vertical
Curb Radius (feet)	15'
Vehicular Design Speed	25 mph
Pedestrian Crossing Time	12 seconds
Road Edge Treatment	Curb
Bike Way Type	Shared lane
Bike Way Width	-

PUBLIC FRONTAGE

Planter Type	Continuous planting strip, intermittent median
Planting Pattern	Trees at 44' O.C. average
Planter Strip / Box Width	8'
Tree Type	Vary species, drought / salt tolerant
Utilities	Underground
Street Light Type	Pedestrian scale ornamental
Street Light Spaces	44' interval (as per light level)
Sidewalk Placement	Both sides
Sidewalk Width	8' (extension onto lot permitted)
Sidewalk Encroachment	Seating and signage as allowed

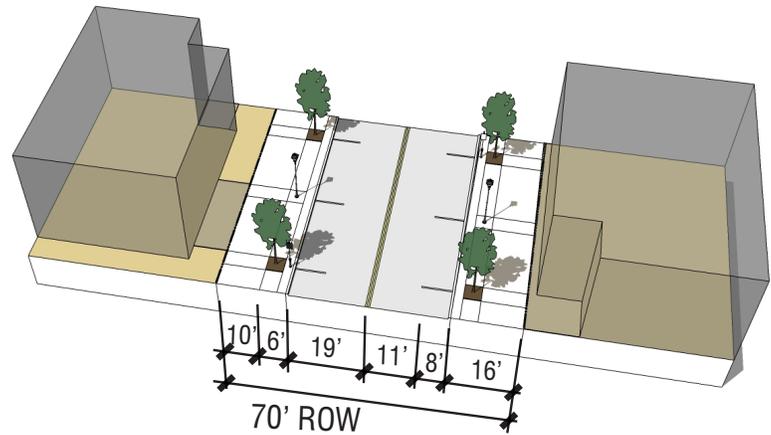
SUBAREAS

Allowed in Subarea(s)	WC-1-GL, WC-1, WC-2, WC-8, WC-10, WC-12
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2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE: Commercial Street

STREET TYPE	FRONTAGE TYPE
	PUBLIC PRIVATE
• 2W-1	PF-2 PV-1/PV-2



DESCRIPTION This is a secondary street for commercial and mixed-use areas with two-way travel in two dedicated lanes intended for a medium capacity street.

STREET DESIGN STANDARDS

Travel Lanes	11'
Parking Lanes	Parallel, both sides, lined
ROW Width	70'
Pavement Width	38'
Traffic Flow	Two Way
Curb Type	Vertical
Curb Radius (feet)	15'
Vehicular Design Speed	20-25 mph
Pedestrian Crossing Time	10 seconds
Road Edge Treatment	Curb
Bike Way Type	Shared lane
Bike Way Width	-

PUBLIC FRONTAGE

Planter Type	Individual in Sidewalk
Planting Pattern	Trees at 44' O.C. average
Planter Strip / Box Width	4' x 4' with expandable grates
Tree Type	Vary species, drought / salt tolerant
Utilities	Underground
Street Light Type	Pedestrian scale ornamental
Street Light Spaces	44' interval (as per light level)
Sidewalk Placement	Both sides
Sidewalk Width	16' (extension onto lot permitted)
Sidewalk Encroachment	Seating and signage as allowed

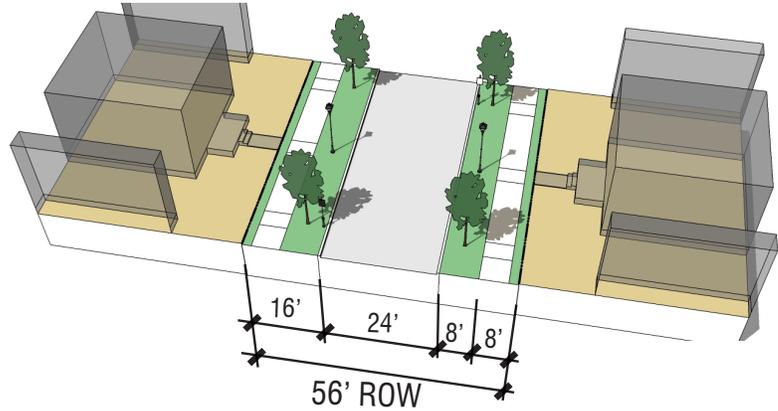
SUBAREAS

Allowed in Subarea(s)	All Subareas except WC-3
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2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE: Residential Street

STREET TYPE	FRONTAGE TYPE	
	PUBLIC	PRIVATE
• 2W-2	PF-3	PV-4



DESCRIPTION This is a secondary street for lower density residential uses with two-way travel in a shared yield lane intended for a lower capacity street.

STREET DESIGN STANDARDS

Travel Lanes	Yield Street
Parking Lanes	Intermittent parallel, both sides, unlined
ROW Width	56'
Pavement Width	24'
Traffic Flow	Two Way
Curb Type	Vertical
Curb Radius (feet)	5-20'
Vehicular Design Speed	20-25 mph
Pedestrian Crossing Time	6 seconds
Road Edge Treatment	Curb
Bike Way Type	Shared lane
Bike Way Width	-

PUBLIC FRONTAGE

Planter Type	Individual in Sidewalk
Planting Pattern	Trees at 44' O.C. average
Planter Strip / Box Width	Continuous
Tree Type	Vary species, drought / salt tolerant
Utilities	Underground
Street Light Type	Pedestrian scale ornamental
Street Light Spaces	44' interval (as per light level) May not be needed on all streets.
Sidewalk Placement	Both sides
Sidewalk Width	8' (extension onto lot permitted)
Sidewalk Encroachment	Per Subarea

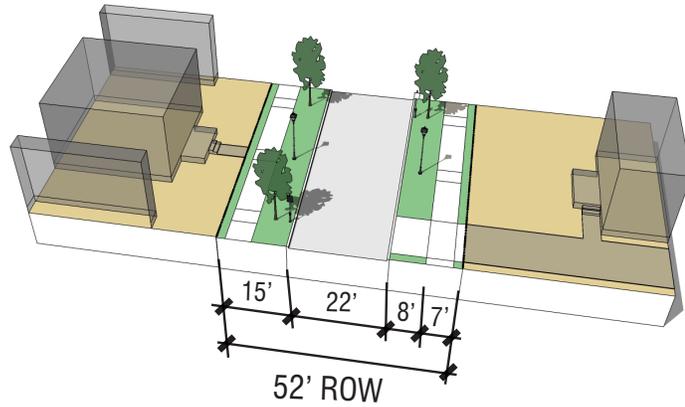
SUBAREAS

Allowed in Subarea(s)	All Subareas except WC-3
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2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE: Residential Street

STREET TYPE	FRONTAGE TYPE	
	PUBLIC	PRIVATE
• 2W-5	PF-4	PV-4/PV-4



DESCRIPTION This is a secondary street for low density residential uses with front loaded lots and along a PUD Boundary and with two-way travel in a shared yield lane intended for a lower capacity street.

STREET DESIGN STANDARDS	
Travel Lanes	22'
Parking Lanes	Parallel, one sides, posted
ROW Width	52'
Pavement Width	22'
Traffic Flow	Two Way
Curb Type	Vertical
Curb Radius (feet)	5-20'
Vehicular Design Speed	20-25 mph
Pedestrian Crossing Time	6 seconds
Road Edge Treatment	Curb
Bike Way Type	Shared lane
Bike Way Width	-
PUBLIC FRONTAGE	
Planter Type	Continuous
Planting Pattern	Trees at 44' O.C. average
Planter Strip / Box Width	Continuous
Tree Type	Vary species, drought / salt tolerant
Utilities	Underground
Street Light Type	Pedestrian scale ornamental
Street Light Spaces	44' interval (as per light level) May not be needed on all streets.
Sidewalk Placement	Both sides
Sidewalk Width	6'
Sidewalk Encroachment	None
SUBAREAS	
Allowed in Subarea(s)	All Subareas except WC-3

2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE:

Residential One-way Street

STREET TYPE	FRONTAGE TYPE
	PUBLIC PRIVATE
• 1W-2	PF-6/PF-4 PV-6/PV-4



DESCRIPTION This is a secondary street for single family residential uses along an open space or green area with internal trees and with one-way travel in one dedicated lane intended for a lower capacity street.

STREET DESIGN STANDARDS

Travel Lanes	12'
Parking Lanes	Parallel, one sides, unlined
ROW Width	50'
Pavement Width	20'
Traffic Flow	One way
Curb Type	Vertical
Curb Radius (feet)	5-20'
Vehicular Design Speed	15 mph
Pedestrian Crossing Time	5 seconds
Road Edge Treatment	Curb
Bike Way Type	Shared lane
Bike Way Width	-

PUBLIC FRONTAGE

Planter Type	Continuous
Planting Pattern	Trees at 44' O.C. average
Planter Strip / Box Width	Continuous
Tree Type	Vary species, drought / salt tolerant
Utilities	Underground
Street Light Type	Pedestrian scale ornamental
Street Light Spaces	44' interval (as per light level)
Sidewalk Placement	One side
Sidewalk Width	6'
Sidewalk Encroachment	None

SUBAREAS

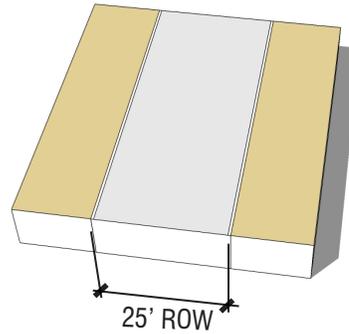
Allowed in Subarea(s)	All Subareas except WC-3
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2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE:

Commercial Access Lane

STREET TYPE	FRONTAGE TYPE	
	PUBLIC	PRIVATE
• L-1	NA	NA



DESCRIPTION This is a very slow speed access lane located at the rear of commercial, multifamily and mixed-use buildings and internal to blocks for vehicular parking access, loading and service access.

STREET DESIGN STANDARDS

Travel Lanes	25'
Parking Lanes	-
ROW Width	25'
Pavement Width	25'
Traffic Flow	Two way
Curb Type	Vertical
Curb Radius (feet)	5-20'
Vehicular Design Speed	15 mph
Pedestrian Crossing Time	7 seconds
Road Edge Treatment	Curb
Bike Way Type	Shared lane
Bike Way Width	-

PUBLIC FRONTAGE

Planter Type	-
Planting Pattern	-
Planter Strip / Box Width	-
Tree Type	-
Utilities	-
Street Light Type	-
Street Light Spaces	-
Sidewalk Placement	-
Sidewalk Width	-
Sidewalk Encroachment	-

SUBAREAS

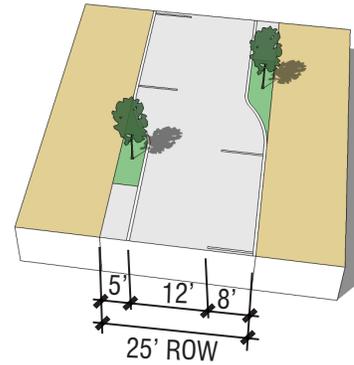
Allowed in Subarea(s)	WC-1-GL, WC-1, WC-2, WC-8, WC-12
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2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE:

Residential Access Lane

STREET TYPE	FRONTAGE TYPE	
	PUBLIC	PRIVATE
• L-2	NA	NA



DESCRIPTION This is a very slow speed access lane located at the rear of residential uses and small and medium buildings, internal to blocks for vehicular parking access, loading and service access.

STREET DESIGN STANDARDS

Travel Lanes	12' yield
Parking Lanes	Parallel, alternating
ROW Width	25'
Pavement Width	25'
Traffic Flow	Two Way
Curb Type	Vertical
Curb Radius (feet)	5-20
Vehicular Design Speed	10 mph
Pedestrian Crossing Time	5 seconds
Road Edge Treatment	Curb
Bike Way Type	Shared lane
Bike Way Width	-

PUBLIC FRONTAGE

Planter Type	-
Planting Pattern	-
Planter Strip / Box Width	5'
Tree Type	-
Utilities	-
Street Light Type	-
Street Light Spaces	-
Sidewalk Placement	-
Sidewalk Width	-
Sidewalk Encroachment	-

SUBAREAS

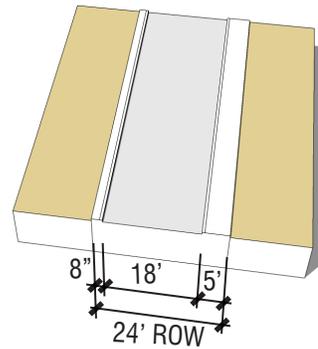
Allowed in Subarea(s)	All Subareas
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2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE:

Commercial Access Lane

STREET TYPE	FRONTAGE TYPE	
	PUBLIC	PRIVATE
• L-3	NA	NA



DESCRIPTION This is a very slow speed access lane located at the rear of buildings and internal to blocks for vehicular parking access, loading and service access.

STREET DESIGN STANDARDS

Travel Lanes	18'
Parking Lanes	-
ROW Width	25'
Pavement Width	18'
Traffic Flow	Two way
Curb Type	Vertical
Curb Radius (feet)	5-20'
Vehicular Design Speed	10 mph
Pedestrian Crossing Time	3 seconds
Road Edge Treatment	Space for Curbing
Bike Way Type	Shared lane
Bike Way Width	-

PUBLIC FRONTAGE

Planter Type	-
Planting Pattern	-
Planter Strip / Box Width	-
Tree Type	-
Utilities	-
Street Light Type	-
Street Light Spaces	-
Sidewalk Placement	-
Sidewalk Width	5'
Sidewalk Encroachment	-

SUBAREAS

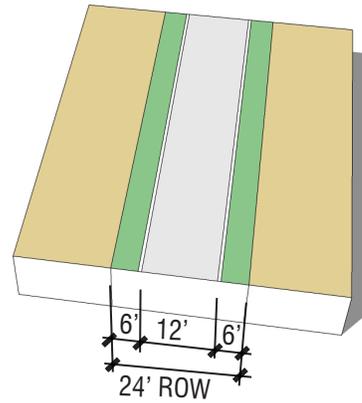
Allowed in Subarea(s)	WC-1-GL, WC-1, WC-2, WC-8, WC-10, WC-12
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2.3.3 Transportation Network

STREET ASSEMBLY | EXAMPLE:

Residential Access Lane

STREET TYPE	FRONTAGE TYPE	
	PUBLIC	PRIVATE
• L-4	NA	NA



DESCRIPTION This is a very slow speed access lane located behind single family residential and internal to blocks for vehicular parking access.

STREET DESIGN STANDARDS

Travel Lanes	12' yield
Parking Lanes	-
ROW Width	24'
Pavement Width	12'
Traffic Flow	Two Way
Curb Type	Flush
Curb Radius (feet)	5-20
Vehicular Design Speed	10 mph
Pedestrian Crossing Time	3 seconds
Road Edge Treatment	Curb
Bike Way Type	Shared lane
Bike Way Width	-

PUBLIC FRONTAGE

Planter Type	-
Planting Pattern	-
Planter Strip / Box Width	-
Tree Type	-
Utilities	-
Street Light Type	-
Street Light Spaces	-
Sidewalk Placement	-
Sidewalk Width	-
Sidewalk Encroachment	-

SUBAREAS

Allowed in Subarea(s)	All Subareas
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2.3.4 Parking and Loading Standards

Purpose & Intent

This Section provides a comprehensive set of parking and loading criteria in order to.

- Mitigate environmental, economic, and social impacts associated with parking;
- Reduce the amount of land devoted to parking;
- Reduce inefficient, dispersed, and single-use reserved parking;
- Maximize on-street parking opportunities;
- Encourage the use of alternative transportation modes and reduce vehicle use;
- Improve community livability, walkability, workability and character;
- Provide flexibility in accommodating changes in demand and use.

On-street parking will be utilized in most parts of the Woodmont Commons PUD to reduce the size of surface lots, slow the speed of vehicles and to separate vehicle traffic from pedestrian traffic

Vehicle Access

Vehicle access to all lots shall be from public or private rights-of-way by one or more curb cuts/driveways.

The location and design of curb cuts/driveways shall minimize traffic hazards and not unduly retard traffic flow in public rights-of-way.

The width of any driveway, except for radii, shall not exceed twenty-four (24) feet for most uses unless truck activity for a specific tenant area requires a wider apron or except for where the turn lanes are required by a Supplemental TIAs.

Each driveway providing site access from a Street Type or other public right-of-way shall be designed and constructed in compliance with the following:

- A. Number of driveways:
 1. Single-family dwellings and duplexes shall have one driveway per dwelling unit.
 2. Multi-family and nonresidential projects.
 - a. A multi-family or nonresidential development project on a parcel of two acres or less shall be limited to a maximum of two (2) driveways, unless the Planning Board determines that more than two (2) driveways are required to accommodate the traffic for the project.
 - b. Whenever a property has access to more than one (1) street, access should be generally be directed to the lower volume street.
- B. Driveways and sidewalks – Driveways shall not be designed to allow parked vehicles to back across a sidewalk, unless it is a residential use or is an infrequently used and signed service driveway for other uses.

2.3.4 Parking and Loading Standards

- C. Distance from street corners – Each driveway should be separated from the nearest street intersection as follows, except where the Planning Board allows less separation:
1. A minimum of 100 feet from the nearest intersection of two streets, as measured from the centerline of the driveway to the centerline of the nearest travel lane of the intersecting street; and
 2. For parcels with frontages of less than 150 feet, the minimum distance shall be seventy-five (75) feet.
- D. Driveway spacing – Driveways should be separated along a street frontage as follows.
1. For Single-family and duplex residential developments, driveways should be shared or separated by six (6) feet.
 2. For Multi-family and nonresidential developments, where two (2) or more driveways serve the same or adjacent multi-family or nonresidential development, the centerline of the driveways should be separated by approximately forty (40) to fifty (50) feet. The Planning Board may approve exceptions to this standard. This guideline does not apply to parking structures or lots located inside blocks.
- E. Driveway dimensions – Dimensions and sight distances shall adhere to reasonable safety protocols and this PUD Master Plan.
- F. Clearance from obstructions – The nearest edge of a driveway curb cut shall be at least two (2) feet from a property line unless it is a common driveway and three (3) feet from the centerline of a fire hydrant, light standard, traffic signal, utility pole, or other similar facility.
- G. Traffic safety visibility areas – Trees, streetlights, pedestrian wayfinding systems and other amenities are encouraged along all streets in the Project. There are no limitations on the placement of these features or buildings near any signalized intersections. Around unsignalized intersections, structures or landscaping are allowed, provided they do not create significant obstructions to pedestrians or excessively block drivers' vision; trees are always permitted with their lower canopy trimmed up to a minimum of five (5) feet from the ground within seventy-five (75) feet of an unsignalized intersection.
- H. Intersection Spacing – Intersections may be as close as seventy-five (75) feet for two-way streets and fifty (50) feet for one-way streets, measured centerline to centerline.
- I. Street Rights of Way – In order to allow buildings to better shape the spaces along streets and around intersections, street rights-of-way may meet without connecting radii.

Off –Street Parking Facilities

Location

- A. Parking Facilities:
1. Every lot shall contain an off-street parking area or access to off-site parking.
 2. All off-street parking shall have dedicated access to on-site lanes to a public right-of-way.
 3. All mixed-use or commercial uses shall have parking areas within twenty (20) feet of any structure with a curb or substantial bumper not less than four (4) feet from the structure, unless access is interrupted by a loading zone, fire hydrant, or other instances where circumstances render said restriction impractical.

2.3.4 Parking and Loading Standards

- B. Off-Site Parking – All projects may locate required parking away from the site of the proposed use.
1. Location of Parking – Permanent off-site parking should be located within 600 feet of residential uses and within 1,200 feet of nonresidential uses if on parking lots or within 1,500 feet of a shared parking structure for any use, unless otherwise approved by the Planning Board.
 2. Evaluation of an Alternate Proposal – In considering a request for off-site parking at distances greater than those above, the Planning Board may consider whether adequate provisions, such as shuttle service, adequate pedestrian facilities and street crossings, have been provided to bring people from the parking to the site.
 3. Guarantee of Continued Availability – Long-term required parking spaces that are off-site may be committed by a recordable lease, deed, or other easement agreement, acceptable to the Town, and which shall run with the land.
- C. Phased Development, Temporary and Off-site Parking – Due to the 20-year buildout time period expected for the Woodmont Commons PUD, future sites of buildings may be graded and prepared for use as temporary shared parking areas. Any such temporary parking areas that are assigned to earlier phases of development in the Woodmont Commons PUD may substitute for future structured or other parking as may be designated, assigned and shown to the Planning Board by the Applicant. This provision is specifically intended to allow early and especially smaller phases of development to have smaller amounts and even no off-street parking areas on the developed parcel.
- D. Buildout Ratios/Park-Once – In Use Zones WC-1 and WC-2, as well as in any future areas of similar character, uses will change over time and parking shall not be required to be updated for every change of use provided: the overall parking ratio is in the range of 2.2 or more spaces per thousand square feet of commercial GLA; the residential density does not exceed 15 (fifteen) units/acre; parking is shared; and no use is more than 1,500 feet from a shared off-street parking facility. The purpose of this is to encourage the establishment of future “park-once” areas where patrons and others arriving by vehicle will park one time and then walk amongst many uses.

Dimensional Requirements

- A. Parking Space and Lot Dimensions: Parking lots and stalls shall comply with the vehicle parking space dimensions.
- B. Loading Space Dimensions:
1. Minimum dimensions for a commercial loading space shall be determined by evaluation of the size of anticipated delivery vehicles. Standards for Off-street Loading Areas – Off-street loading areas should be sized as follows: Dimensions – Loading spaces should be eight (8) to twelve (12) feet in width, twenty (20) to forty (40) feet in length, with ten (10) to fourteen (14) feet of vertical clearance.
 2. Shared on-street loading areas are permitted and encouraged in the denser parts of the Project, in particular Subareas WC-1-GL, WC-1, WC-2, WC-10 and WC-12 and any other areas of similar character.

2.3.4 Parking and Loading Standards

3. Off-street loading spaces are encouraged. The Planning Board may modify these guidelines, where it is determined that the operating, shipping, and delivery characteristics of the use do not require the number or type of loading spaces required by this Section, or that adequate shared on-street loading spaces are provided.

Table: Vehicle Parking Space Dimensions

Angle of Parking	Space Width	Curb Length	Space Depth	One-Way Aisle Width
(in degrees)	(in feet)	(in feet per vehicle)	(in feet from curb)	(in feet)
Parallel*	7-8.5	20-22	7-8.5	9-11
30°	8-8.5	16-18	14.5-15.5	10-12
45°	8-8.5	10-14	16-18	10-12
60°	8-8.5	10-12	18-20	10-14
90°	8-8.5	7-9	17-19	10-14

* A parallel parking lane shall have a minimum width of 8 feet unless accompanied by a wider bike lane in areas of high bicycle traffic.

Off-Street Parking and Loading Standards

- A. Off-site parking facility types include the following:
 1. Surface Parking
 2. Enclosed and Covered Parking
 3. Structured Parking
- B. On-Site Parking and Access to Parking
 1. Except in Subareas WC-1-GL, WC-12 and on Flex Blocks, surface parking lots shall generally not abut any streets other than lanes and should generally be located behind buildings.
 2. Except in Subareas WC-1-GL, WC-12 and on Flex Blocks, parking, including parking garages, should generally be accessed from a lane or secondary frontage when practical.
 3. Pedestrian entrances to all parking lots and parking garages should be directly accessible from the sidewalk and/or a building entrance.
- C. Access to Adjacent Sites – Adjacent off-street parking areas on adjacent properties are encouraged to have vehicle connections to provide for convenience, safety, and efficient circulation. Shared pedestrian access between adjacent properties is also strongly encouraged.
- D. Tandem Parking – Tandem parking (vehicles parked front bumper to back bumper) is permissible in private parking lots, areas or garages where all tandem parking is attendant directed on a full-time, year-round basis. Vehicles may be parked in tandem where the parking area is serving a single dwelling (including dwellings in multiple-family developments when both tandem spaces are assigned to the same

2.3.4 Parking and Loading Standards

unit) and where the tandem parking is not more than two cars in depth. The Planning Board may grant approval of tandem parking for nonresidential developments if the Applicant demonstrates that it will not interfere with orderly parking and movement of vehicles.

1. Mechanical Parking Lifts – The Planning Board may also approve mechanical parking lifts when an attendant or valet parking agreement is provided. Proposals for mechanical parking lifts shall be on a case-by-case basis to assess their functionality and safety.

Minimum Parking and Loading Required

- A. Number of Parking Spaces – The number of parking spaces allocated for each land use shall be determined as follows:
 1. Basic Space Requirement – Each land use shall provide the number of parking spaces outlined in the Parking Space Standards Table, as applicable, except where the requirement is reduced or otherwise changed in compliance with the Reduction of Parking Requirements or Alternative Parking Standards. On-street parking shall be considered public parking and may count towards the number of public parking spaces provided. In the event that the Town or State removes such on-street parking that was allowed to count towards the minimum required, the existing use will not be required to make up the difference and the use will not be made non-conforming. Single-family detached, duplex and town-house dwellings shall provide two (2) spaces per dwelling. Accessory Dwelling Units shall provided with one (1) space per dwelling unit.
 2. Parking Space Allocation – On-street parking may be allocated to a particular Lot when a particular on-street space abuts a particular Lot. Where an on-street space abuts more than one Lot, a particular parking space may be allocated to a particular Lot only if more than 66% of the space abuts that Lot or if that space has otherwise been legally assigned to that Lot or use. On-street parking shall be counted toward fulfilling parking requirements. See also the phased development provisions for temporary parking in the Reduction of Parking Requirements below.
 - a. Use with Accessory Components – A single use with accessory components shall provide parking for each component. For example, a hotel with a restaurant shall provide the parking spaces required by the Parking Space Standards Table for a hotel (e.g., the guest rooms), and for a restaurant, except where the requirement is reduced or otherwise changed in compliance with the Reduction of Parking Requirements below.
 - b. Expansion of Structure – For additions or enlargements of existing buildings or uses that would increase the number of parking spaces required, the additional parking shall be required only for such addition, enlargement or change, and not for the pre-existing building or use.
 - c. Change in Use – For any change of occupancy or manner of operation that may increase the number of parking spaces required, no additional parking shall be required unless the Planning Board determines that the change in use without added parking would create a negative impact for neighboring properties.
 - d. Use Zones – Where the Parking Space Standards Table references Subareas, see the Land Use Plan for locations.

2.3.4 Parking and Loading Standards

- e. Rounding of Calculations – If a fractional number is obtained in calculations performed in compliance with this Chapter, one (1) additional parking space shall be required for a fractional unit of 0.50 or above, and no additional space shall be required for a fractional unit of less than 0.50.

2.3.4 Parking and Loading Standards

Table: Parking Space Standards

LAND USE	UNIT OF MEASUREMENT	SPACES REQUIRED PER UNIT OF MEASUREMENT
AGRICULTURAL		
1 Agriculture	GFA	1 per 1,000 sq. ft.
RESIDENTIAL		
2 Dwelling, Multi-Family ¹	DU	1.5 per DU
3 Dwelling, Single Family Detached ¹	DU	2 per DU
4 Dwelling, Single Family Attached ¹	DU	2 per DU
5 Dwelling, Two-Family ¹	DU	2 per DU
6 Elderly Housing	DU	2 per DU
6 Elderly Housing (Affordable)	DU	1.2 per DU
7 Mixed Use Residential	DU	1.25 per DU
8 Dwelling, with Accessory Apartment ²	DU	2 per DU, (4 total)
CIVIC USES		
9 Community Center	GFA, or seats and persons accommodated	1 space per 200 sq. ft. or 1 per 3 seats or 3 persons the facility is intended to accommodate
10 Public Facilities	GFA, or seats and persons accommodated	1 space per 200 sq. ft. or 1 per 3 seats or 3 persons the facility is intended to accommodate
13 Recreational Facilities, Public	GFA, or seats and persons accommodated	1 space per 200 sq. ft. or 1 per 3 seats or 3 persons the facility is intended to accommodate
14 Religious Facilities	Seats or linear feet of bench space	1 per each 3 seats or 6 linear ft.
15 Cultural Uses and Performing Arts	GFA, or seats and persons accommodated	1 space per 200 sq. ft. or 1 per 3 seats or 3 persons the facility is intended to accommodate
INSTITUTIONAL		
16 Assisted Living Facilities	Resident unit or bed and employees	0.5 per resident unit or bed plus two spaces per each three employees per shift
17 Nursing Home and Accessory Uses	Resident unit or bed and employees	0.5 per resident unit or bed plus one per employee
18 Hospital	Various	0.5 per bed plus one per employee per shift
ACCOMMODATION		
19 Hotels	Various	1 per room, plus 1 per 20 rooms (staff), plus 50% normally required for accessory uses
20 Boutique Hotel	Various	1 per room, plus 1 per 20 rooms (staff)
21 Bed and Breakfast Homestay	D.U and guest room	2 per D.U plus 1 per guest room
COMMERCIAL USES		
22 Business Center Development	GFA	1 per 300 sq. ft.
23 Conference/Convention Center.	GFA, or seats and persons accommodated	1 space per 200 sq. ft. or 1 per 3 seats or 3 persons the facility is intended to accommodate
24 Day Care Center, Adult	Employees and attendees	1 per employee plus 1 per 7 attendees at peak attendance plus parking for any associated busses or vans
25 Drive-Thru Window	Employees and vehicles served	2 per employee plus 1 per each vehicle served
26 Financial institution	GFA	1 per 300 sq. ft.
27 Flex Uses	GFA or employees	1 per 600 sq. ft. or 1 per employee
28 Education and Training Facilities	Faculty, students, & administrators	1 per faculty member, administrator, and employee, plus .5 per student at peak attendance
29 Group Child Care Center	Employees and attendees	1 per employee plus 1 per 8 attendees at peak attendance
30 Home Occupation	Variable	Applicant must demonstrate that parking is adequate for the proposed use, in no case less than 2 spaces in addition to those required for the residence
31 Membership Club	GFA, or seats and persons accommodated	1 space per 200 sq. ft. or 1 per 3 seats or 3 persons the facility is intended to accommodate
32 Motor Vehicle Station, Limited Service	Various	1 per 300 sq. ft. of floor area, plus 1 per accessory service bay, plus 1 per employee, plus 1 per vehicle
33 Recreation, Commercial	GFA, or seats and persons accommodated	1 space per 200 sq. ft. or 1 per 3 seats or 3 persons the facility is intended to accommodate
34 Retail Sales Establishment	GFA	1 per 300 sq. ft.
36 Professional Office	GFA	1 per 300 sq. ft.
37 Dedicated Office Building	GFA	1 per 300 sq. ft.
38 Rental Car Satellite	Various	1 per 300 sq. ft. indoor sales per office area, plus 1 per 1,500 sq. ft. outdoor storage
39 Repair Services	GFA	1 per 400 sq. ft.
40 Research or Development Laboratories	GFA	1 per 600 sq. ft.
41 Restaurant	Seats and employees	1 per 3 seats plus 1 per employee
42 Restaurant, Fast Food	Seats and employees	1 per 4 seats plus 1 per employee, plus stacking space for drive-through
43 Restaurant, with Take-out, no Drive-through	Seats and employees	1 per 4 seats plus 1 per employee
44 School, Private or Charter, Elementary and Middle	Various	1 per faculty, employee and bus if bus transportation is provided, plus 1 per each 4 seats for assembly areas
44 School, Private or Charter, High	Various	1 per faculty member, administrator, and employee, plus 1 per bus if bus transportation is provided, plus 1 per each 4 students
45 Service Establishment	GFA or employees	1 per 300 sq. ft. or 2 per employee

2.3.4 Parking and Loading Standards

3. Footnotes to the Table of Parking Standards:
 - a. Footnote 1. Elderly affordable housing is exempt from this requirement. Refer to Section 3.6 of the Londonderry Zoning Ordinance for elderly affordable requirements.
 - b. Footnote 2. Off-street parking shall be provided for at least four (4) vehicles. Garage and “piggy-back” parking is encouraged.
 - c. GFA = Gross Floor Area; DU = Dwelling Unit(s)
 - d. For uses not listed, parking and loading requirements shall be in accordance with the Londonderry Zoning Ordinance Table 2 Off-Street Parking and Loading or calculated for the closest similar use as determined by the Planning Board
 - e. When the schedule requires the number of spaces to be calculated per employee and employees are on the site in shifts, the number to be used is the number of employees present during the largest shift. In all other cases it shall mean the total number of employees on the site or who will use the site for parking at any one time.
- B. Motorcycle Parking – A parking lot with 100 or more vehicle parking spaces shall provide motorcycle parking spaces conveniently located near the main entrance to the primary structure and accessed by the same access aisles that serve the vehicle parking spaces in the parking lot.
 1. Number of Spaces Required – A minimum of one (1) motorcycle parking space shall be provided for each 100 vehicle spaces or fraction thereof.
 2. Space Dimensions – Motorcycle spaces shall have minimum dimensions of four (4) feet by seven (7) feet.
- C. Number of Loading Spaces – Off-street loading spaces guidelines are as follows:
 1. Guidelines for uses not listed shall be determined by the Planning Board based upon the *Londonderry Zoning Ordinance* for comparable uses. Loading zones in mixed-use areas may be shared.
 2. Location – Loading spaces should be:
 - a. As near as possible to the main structure and limited to the rear two-thirds of the parcel, if feasible;
 - b. Situated to ensure that loading and unloading can take place on-site without the need for vehicles to back onto public streets;
 - c. Situated to avoid adverse impacts upon neighboring residential properties.
 3. Loading Ramps – Plans for loading ramps or truck wells shall be accompanied by a profile drawing showing the ramp, ramp transitions, and overhead clearances.
 4. Specific Uses – These include the following:
 - a. A minimum of five (5) stacking spaces shall be required for restaurants with drive-up window. The distance shall be measured from the drive-up window.
 - b. A minimum of two (2) stacking spaces per car wash bay shall be required.

2.3.4 Parking and Loading Standards

- c. A minimum of four (4) stacking spaces shall be required for financial institutions having one drive-thru window and three (3) stacking spaces for each additional window.
- d. For other uses not specifically provided herein, the Planning Board (with a recommendation from the Zoning Officer) shall make a determination regarding the number of stacking spaces required.

Table: Loading Spaces

Use	Size	Loading Required
Retail commercial, industrial, manufacturing, research and development, institutional, service uses, and other allowed nonresidential uses	10,000 to 25,000 square feet	1
	25,001+ square feet	1 for each additional 25,000 square feet
Office uses	10,000 to 35,000 square feet	1
	35,001+ square feet	1 for each additional 25,000 square feet

On-Street Parking and Loading Standards

- A. On-Street Parking – Except in WC-1-GL, needed parallel or, in WC-1, WC-2 and WC-12 parallel or continuous angled parking will be allowed along streets where Commercial uses predominate. Parallel parking is permitted on any other streets. On-street parking may count towards the parking requirements for a particular use or block when located in front of the use or along the block.
- B. On-Street Loading – Shared on-street loading areas are permitted and encouraged in the denser parts of the PUD, in particular, Subareas WC-1, WC-2, WC-10 and any other areas of similar character. On-street loading may occur in dedicated and posted loading zones.

Design & Construction Standards

Parking

Surface parking design and construction to be in accordance with *Section 3.10.13 Design & Construction Standards* of the *Londonderry Zoning Ordinance*. Parking spaces within Structured Parking shall be in accordance with the following:

- A. All parking spaces shall be so marked by use of painted lines or other permanent striping materials;
- B. Circulation areas shall be designed so that vehicles can proceed safely without posing a danger to pedestrians or other vehicles and without interfering with parking areas.

Loading

- A. Striping
 1. Loading spaces shall be striped, and identified for loading only.

2.3.4 Parking and Loading Standards

2. The striping and “loading only” notations should be continuously maintained in a clear and visible manner in compliance with the approved plans.
- B. Surfacing
1. All loading areas may be surfaced with asphalt, concrete pavement, or comparable material as determined by the Planning Board and shall be graded to drain all surface water.

Alternative Parking Standards

Reduction of Parking Requirements – The number of parking spaces required by the Parking Space **Standards** Table on page 150 may be reduced or modified through the following measures or any combination of measures.

- A. Shared Parking – Where two or more uses utilizing the same parking have distinct and different hours of peak parking demand (such as a theater and a bank), the required minimum number of parking spaces may be reduced by up to the number of spaces required for the least intensive use. The Urban Land Institute’s Shared Parking, as amended, may be used as a justification for reduced parking requirements.
- B. Reduction of Parking – The Planning Board may reduce or waive the minimum number of parking spaces required based on quantitative information provided by the Applicant that documents the need for fewer spaces (e.g. sales receipts, documentation of customer frequency, information on parking standards required for the proposed land use by other cities, etc.).
- C. Waiver of Parking: Off-Hour Uses – The Planning Board may reduce or waive the minimum number of parking spaces required for a land use that has a peak parking demand that does not coincide with the general peak parking demand (generally considered the evening peak period).
- D. Waiver of Parking – The Planning Board may reduce or waive the minimum number of parking spaces required based on quantitative information provided by the Applicant that documents a reduction in parking demand.
- E. Other alternative transportation methods may be proposed including those outlined in *Section 3.10.12.5 Alternative Transportation of the Londonderry Zoning Ordinance*.

Parking for Disabled Persons

Parking for the disabled shall be in compliance with *Section 3.10.14* of the *Londonderry Zoning Ordinance* and *Section 3.11(d)* of the *Londonderry Site Plan Regulations*. Parking spaces required for the disabled shall count toward compliance with the number of parking spaces required by this Section.

“Unbundled” Parking

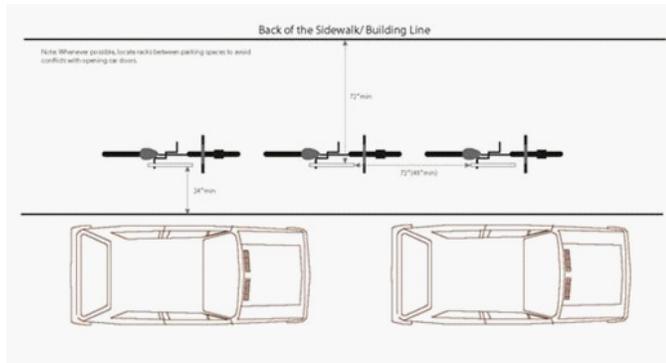
All off-street parking spaces for new multi-family projects, or in future new conversions of multifamily projects, may be leased or sold separately from the rental or purchase fees for the life of the units if approved by the Planning Board through the permitting process.

2.3.4 Parking and Loading Standards

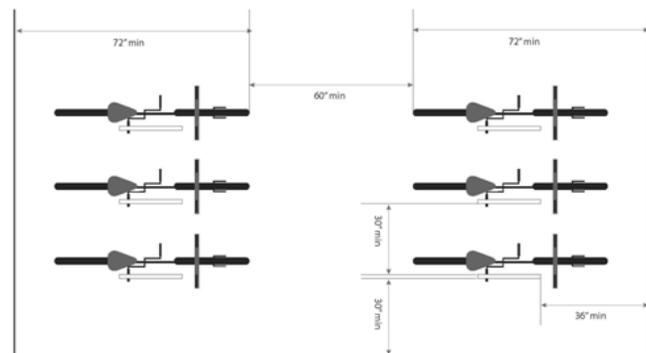
Bicycle Parking Standards

Each multi-family residential use and all nonresidential uses shall provide bicycle parking in compliance with this section.

- A. Bicycle Parking Standards – Bicycle parking should be designed for both short-term and long-term needs. The bicycle parking requirement may be met on site or may be exempted based on nearby shared facilities.
1. Short-Term Parking – This is intended for bicycle trips where bicycles will be left for a short time period (generally less than 2 hours). It requires a high degree of convenience and should be located as close to destinations as possible. Short-term bicycle parking may be located within the public right-of-way or on an internal sidewalk as per the Short-term parking diagram.
 2. Long-Term Parking – This is intended for bicycle trips where bicycles will be left for a longer periods of time (generally for several hours, overnight, or seasonally). Long-term bicycle parking is intended to serve the needs of commuters and residents, but not retail patrons. All bicycle parking stalls shall be located in accordance with the off-street or off-site parking requirements of the PUD Master Plan.



Short-term Bicycle Parking



Long-term Bicycle Parking

2.3.4 Parking and Loading Standards

- B. Number of Bicycle Spaces – Off-street bicycle parking is required whenever any new use is established or any existing use is enlarged for which more than ten (10) automobile parking spaces are required.

TABLE - BICYCLE PARKING

Auto Spaces Required	Bicycle Stalls Required
10-50	10% of auto
51-100	5% of auto
100+	3% of auto

1. Requirements are additive. Once a threshold is met the previously existing requirement shall be maintained and additional stalls shall be provided at the new level until the next threshold is met.
2. A minimum of two (2) and a maximum of twenty (20) stalls shall be provided.
3. In calculating stall requirements all decimals and fractions shall be rounded up to the nearest whole number.
4. All bicycle parking stalls shall be located within 100 feet of the primary use or as close as the closest auto space.
5. A bicycle “stall” shall include a delineated and safe parking area, and an appropriate structure to which bicycles can be locked.
6. Appropriate structure means a stand or other device constructed so as to enable the user to secure by locking the frame and one wheel of each bicycle parked therein. Racks must be easily usable with both U-locks and cable locks. Racks should support the bikes in a stable upright position so that a bike, if bumped, will not fall or roll down. Racks that support a bike primarily by a wheel, such as standard ‘wire racks’ are damaging to wheels and thus are not acceptable.
7. Bicycle parking racks, shelters or lockers must be securely anchored to the ground or to a structure.
8. Outdoor bicycle parking areas shall be surfaced with hard-surfacing material having a minimum depth of two inches, such as pavers, asphalt or concrete, unless otherwise permitted by the Planning Board.
9. Provision of additional amenities including but not limited to covered parking and shower facilities is encouraged for lots over one-hundred (100) automobile spaces but not required.

Public Safety Standards

- A. Emergency vehicle access shall be provided to buildings and occupied structures and access routes and clearances shall be indicated on the Site and/or PUD Subdivision Plan drawings at the time of submittal.
- B. Access streets and routes must be designed to withstand the live loads of fire apparatus.
- C. The Developer shall consult with the Fire Department with regards to the number of fire hydrants and the street layout and connectivity to facilitate response times within the Fire Department’s standard response district.

2.3.5 PUD Perimeter Buffers

The *Londonderry Zoning Ordinance* requires a fifty (50) foot setback for streets and structures from the perimeter of the Woodmont Commons PUD boundary which abuts existing residential uses. This Perimeter Buffer, as indicated in *Section 2.2.2 Land Use Plan*, may be crossed by streets, driveways, paths or sidewalks, may include pervious and impervious surfaces, and may include Shared Use Paths or Recreation Trails. The Perimeter Buffer is measured from the edge of the abutting right-of-way or property boundary.

If adjoining uses are compatible, the PUD Perimeter Buffer shall be interpreted to require a fifty (50) foot no-build setback, without any additional requirement to add or preserve screening (i.e. single-family next to single-family).

If the adjoining existing uses are incompatible with the uses along the edges of the Woodmont Commons PUD Boundary, then landscaping and other screening devices are required to enhance the natural vegetation within the PUD Perimeter Buffer. Existing vegetation that consists of invasive species or is in poor condition may be removed and replaced with acceptable species as defined in the Landscape Design Standards within the Londonderry Site Plan Regulations.

Three rows of apple trees, where existing, will remain along Gilcrest and the areas of Perimeter Buffer to preserve the existing rows as indicated on the Land Use Plan. When needed, apple trees will be replaced with apple trees, but using a staggered approach so that not all three rows are replaced at once.

One row of apple trees, where existing, will remain along Hovey and Pillsbury Roads within the 50 foot Perimeter Buffer as shown on the Land Use Plan. Frontage requirements may be modified to include adjustments for the location of sidewalks to accommodate said trees.

Prior to PUD Subdivision and/or PUD Site Plan approval, the existing trees shall be located and the buffer shall be recorded with deed restrictions mandating the preservation of said trees.

The Developer or its assignee shall provide a maintenance agreement for the long term care of the trees prior to any PUD Subdivision and/or PUD Site Plan approvals being issued for affected areas. The care shall include pruning requirements and the disposition of any harvested fruit.

2.3.6 Conserved Green Space and Shared Open Space Standards

The PUD Master Plan has the following allowable Open Space Types within the PUD. These Types all contribute to the minimum Shared Open Space and Conserved Green Space requirements in *Section 2.2.3 Land Use Standards*.

- Conserved Green Space;
- Green;
- Square;
- Plaza;
- Playing Field;
- Playground;
- Park.

Other Open Space Types may be permitted when the type is mutually beneficial to the Town and the PUD as determined by the Planning Board and the Developer. Each Open Space Type has different requirements and eligible features, which are described on the following pages. These Types are to be assembled with the Block and Street Types at the PUD Subdivision and/or PUD Site Plan level. As stated in *Section 2.2 Land Use Regulations*, submittals shall be as follows:

- All PUD Subdivision and/or PUD Site Plan submittals shall indicate the types and locations of Conserved Green Space and Shared Open Space(s) within the submittal in general accordance with the Land Use Plan. The locations may be adjusted according to the PUD Subdivision and/or PUD Site Plan requirements;
- At least one half acre of Shared Open Space or publicly accessible Conserved Green Space must be provided within one quarter mile of the front door of any approved building. At least one half acre of Active Open Space must be provided within one half mile of the front door of any approved building. Publicly accessible Shared Open Space and Conserved Green Space must be connected to a public street or sidewalk network;
- Shared Open Space shall be identified at the PUD Subdivision and/or PUD Site Plan level. Qualifying publicly accessible Shared Open Space and Conserved Green Space for each specific PUD Subdivision or PUD Site Plan must be identified prior to the release of certificates of occupancy for any PUD Site Plan within the PUD Subdivision;
- Shared Use Paths and Recreation Trails are included in this Section as they are components of Open Space and are not within public rights-of-way.

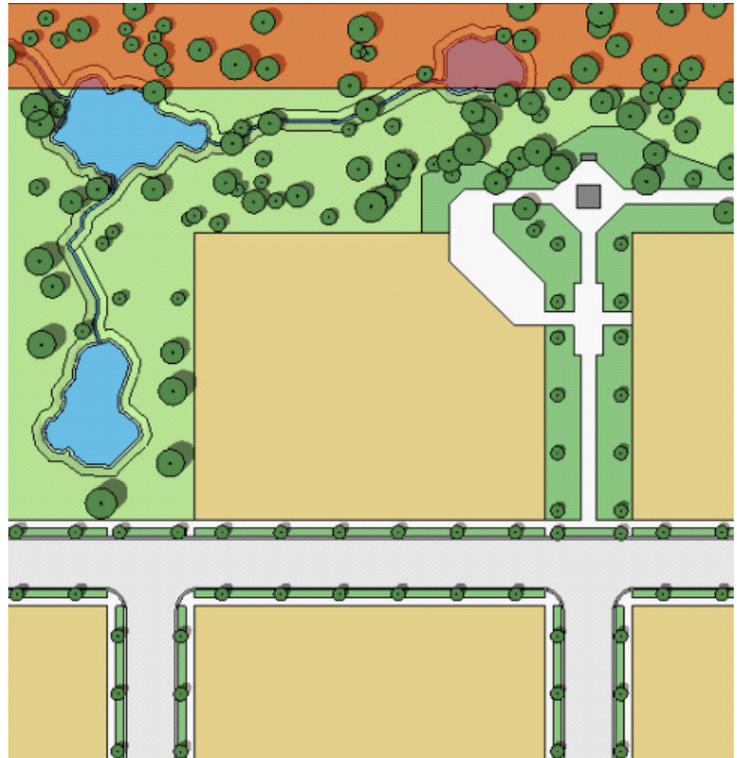
2.3.6 Conserved Green Space and Shared Open Space Standards

OPEN SPACE TYPE: Conserved Green Space

DESCRIPTION Conserved Green Space may or may not be publicly accessible. A preserve may be independent of surrounding building frontages or located completely behind buildings with no public frontage. Accessory Structures are allowable if they meet Paragraph 2.6.3.3.2.10 of Section 2.6.3 Conservation Overlay District of the Londonderry Zoning Ordinance.



Character Examples



LOCATION	
Characteristics	May be linear, following the trajectories of natural corridors
REQUIREMENTS	
Minimum Size	None
Suggested Frontage on at Least	
Publicly Accessible	Not required
Accessway required	No
Accessway(s) allowed	Trail, independent shared use path
ELIGIBLE FEATURES	
Include:	Meadows, water bodies, wetlands, woodlands, passive recreation, buffers at perimeter of property, perimeter sidewalks, bikeways, paths, trails, access, crossing streets, easements, and stormwater features
SUBAREAS	
Allowed in Subarea(s)	All Subareas

2.3.6 Conserved Green Space and Shared Open Space Standards

OPEN SPACE TYPE: Green

DESCRIPTION Open space that is publicly accessible and available for recreation.



Character Examples



LOCATION	
Characteristics	Spatially defined by landscaping and determined by configuration of street and block network
REQUIREMENTS	
Minimum Size	1/4 acre (4 acre maximum)
Suggested Frontage on at Least	1 Street
Publicly Accessible	Yes
Accessway required	Yes
Accessway(s) allowed	Perimeter sidewalk, trail, independent shared use path
ELIGIBLE FEATURES	
Include:	Naturalistically disposed lawn and trees, perimeter sidewalks, paths, trails, pocket park, civic uses, open shelters, fenced dog parks, playgrounds, community gardens, playing fields, crossing streets, easements, stormwater features
SUBAREAS	
Allowed in Subarea(s)	All Subareas

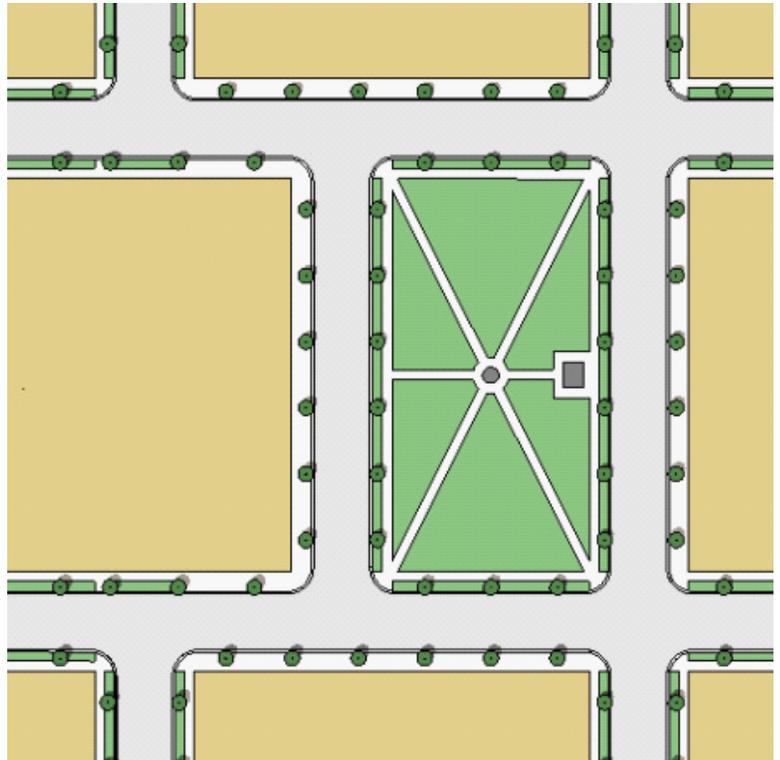
2.3.6 Conserved Green Space and Shared Open Space Standards

OPEN SPACE TYPE: Square

DESCRIPTION A stand-alone and independent publicly accessible open space available for recreation and civic purposes.



Character Examples



LOCATION	
Characteristics	Spatially defined by building frontages and located at intersections of the primary street network
REQUIREMENTS	
Minimum Size	1/4 acre (2 acre maximum)
Suggested Frontage on at Least	Street
Publicly Accessible	Yes
Accessway required	Yes
Accessway(s) allowed	Sidewalk, independent shared use path
ELIGIBLE FEATURES	
Include:	Paths, lawns, and trees formally disposed, pocket park
SUBAREAS	
Allowed in Subarea(s)	All Subareas

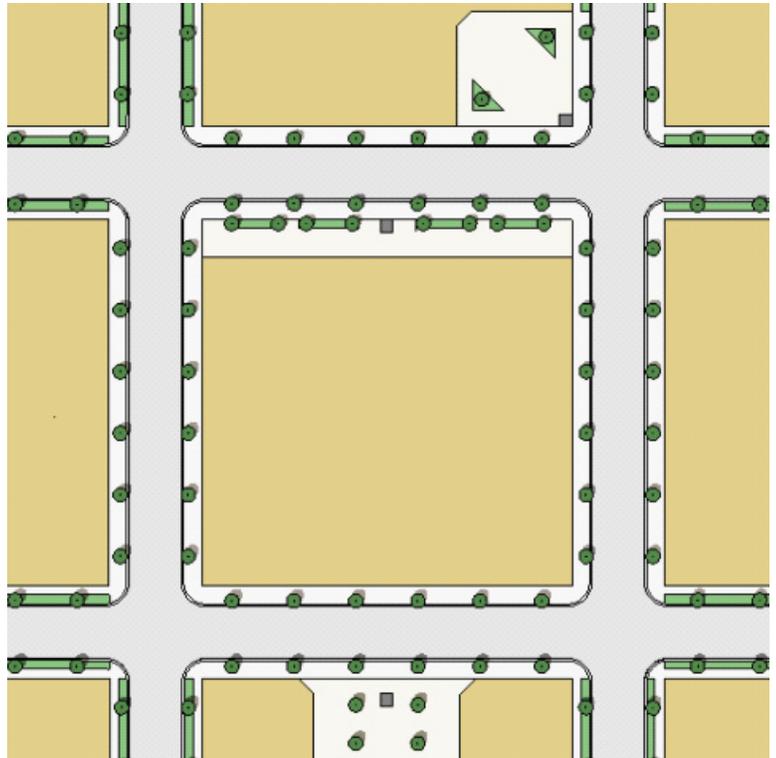
2.3.6 Conserved Green Space and Shared Open Space Standards

OPEN SPACE TYPE: Plaza

DESCRIPTION A publicly accessible open space integrated into public realm or building entries available for civic purposes and commercial activities.



Character Examples



LOCATION	
Characteristics	Spatially defined by building frontages and located at intersections of the primary street network
REQUIREMENTS	
Minimum Size	1/8 acre (2 acre maximum)
Suggested Frontage on at Least	1 Street
Publicly Accessible	Yes
Accessway required	Yes
Accessway(s) allowed	Sidewalk, independent shared use path
ELIGIBLE FEATURES	
Include:	Hardscape, pavement, landscape features, trees are optional
SUBAREAS	
Allowed in Subarea(s)	WC-1-GL, WC-1, WC-2, WC-10, WC-12

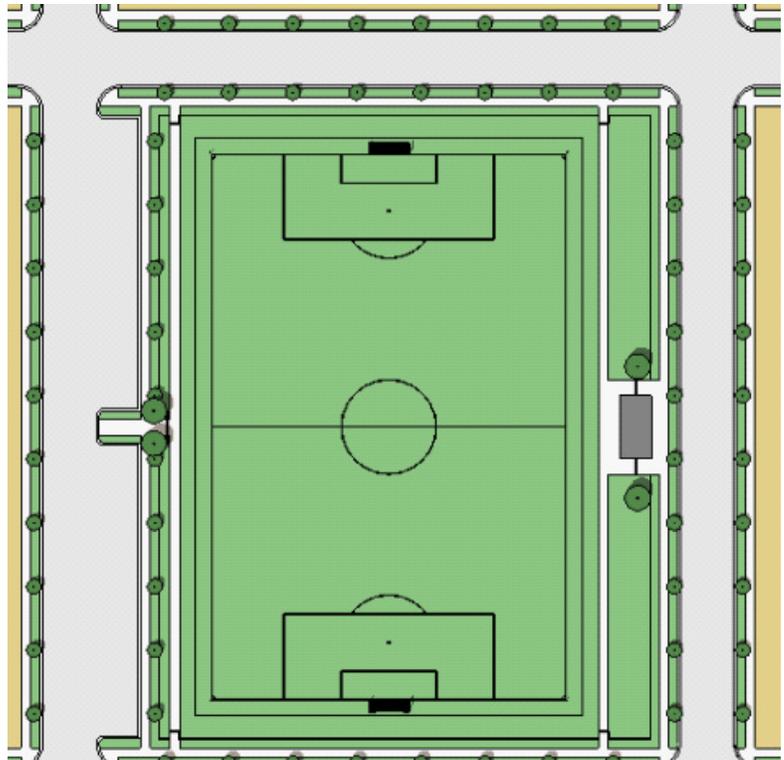
2.3.6 Conserved Green Space and Shared Open Space Standards

OPEN SPACE TYPE: Playing Field

DESCRIPTION A publicly accessible open space designed and equipped for active recreation.



Character Examples



LOCATION	
Characteristics	Located where active recreation uses would be easily accessed
REQUIREMENTS	
Minimum Size	None
Suggested Frontage on at Least	1 Street
Publicly Accessible	Yes
Accessway required	Yes
Accessway(s) allowed	Sidewalk, independent shared use path
ELIGIBLE FEATURES	
Include:	Streets, parking, associated structures and shelters, concessions and lighting. Fields may be fenced and may be included within parks.
SUBAREAS	
Allowed in Subarea(s)	WC-1, WC-2, WC-8, WC-9, WC-12

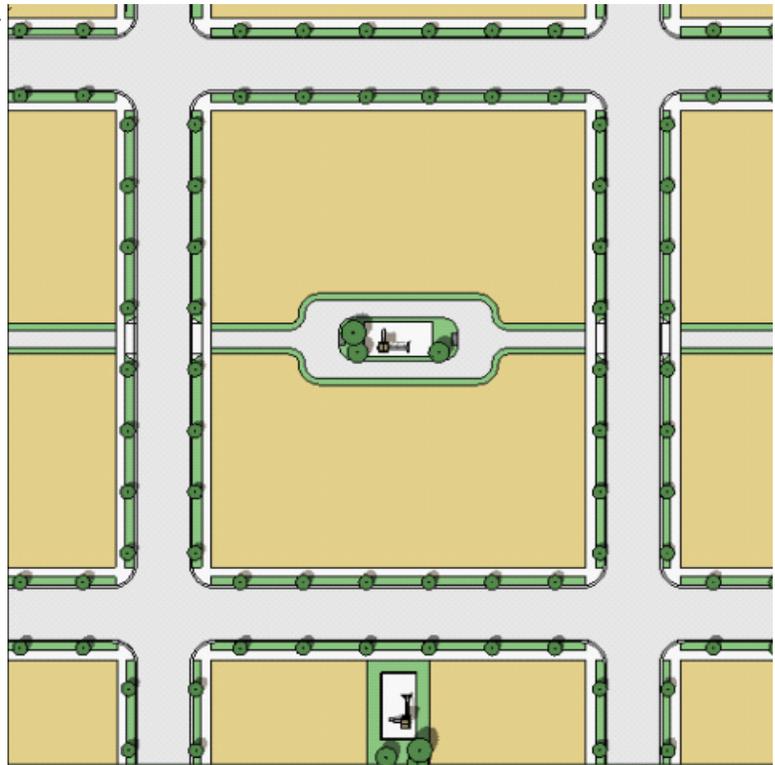
2.3.6 Conserved Green Space and Shared Open Space Standards

OPEN SPACE TYPE: Playground

DESCRIPTION A publicly accessible open space designed and equipped for recreation of children.



Character Examples



LOCATION	
Characteristics	Located where active recreation uses would be easily accessed, interspersed within residential areas and may be placed within a block or accessed via a lane
REQUIREMENTS	
Minimum Size	None
Suggested Frontage on at Least	1 Street
Publicly Accessible	Yes
Accessway required	Yes
Accessway(s) allowed	Sidewalk, trail, independent shared use path
ELIGIBLE FEATURES	
Include:	Open shelter, fence, playground equipment, parking, hardscape, and landscape features. Playgrounds may be included within parks and greens.
SUBAREAS	
Allowed in Subarea(s)	All Subareas

2.3.6 Conserved Green Space and Shared Open Space Standards

OPEN SPACE TYPE: Park

DESCRIPTION Open space available for passive recreation. A park may be independent of surrounding building frontages. Its landscape may consist of meadows, water bodies, wetlands, and woodlands, all naturalistically disposed.



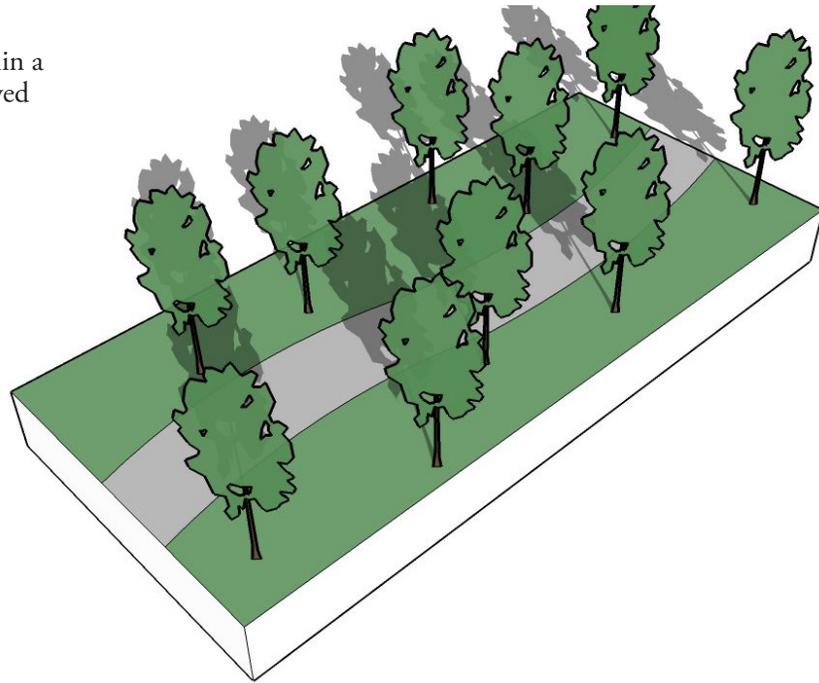
Character Examples

LOCATION	
Characteristics	Located where regulated natural features create an opportunity
REQUIREMENTS	
Minimum Size	2 acres
Suggested Frontage on at Least	May be independent of street network
Publicly Accessible	Yes
Accessway required	No
Accessway(s) allowed	Perimeter sidewalks, trail, independent shared use path
ELIGIBLE FEATURES	
Include:	Conserved Green Space, water bodies, wetlands, active recreation and buffers that may be lineal following natural corridors, boat launch, civic uses, open shelters, retail and food kiosks, fenced dog parks, playgrounds and playing fields, community gardens, access and crossing streets, easements, parking and stormwater features
SUBAREAS	
Allowed in Subarea(s)	All Subareas

2.3.6 Conserved Green Space and Shared Open Space Standards

OPEN SPACE COMPONENT: Shared Use Path

DESCRIPTION A trail for pedestrian and bicycle use within a Shared Open Space or Conserved Green Space.



Character Examples

LOCATION	
Characteristics	A combination of a pedestrian and bicycle trail in a Shared Open Space or Conserved Green Space
REQUIREMENTS	
Minimum Width (feet)	10 minimum/14 maximum
Publicly Accessible	Yes – connected to the street and pedestrian network
ELIGIBLE FEATURES	
Include:	All Open Space Types
SUBAREAS	
Allowed in Subarea(s)	All Subareas

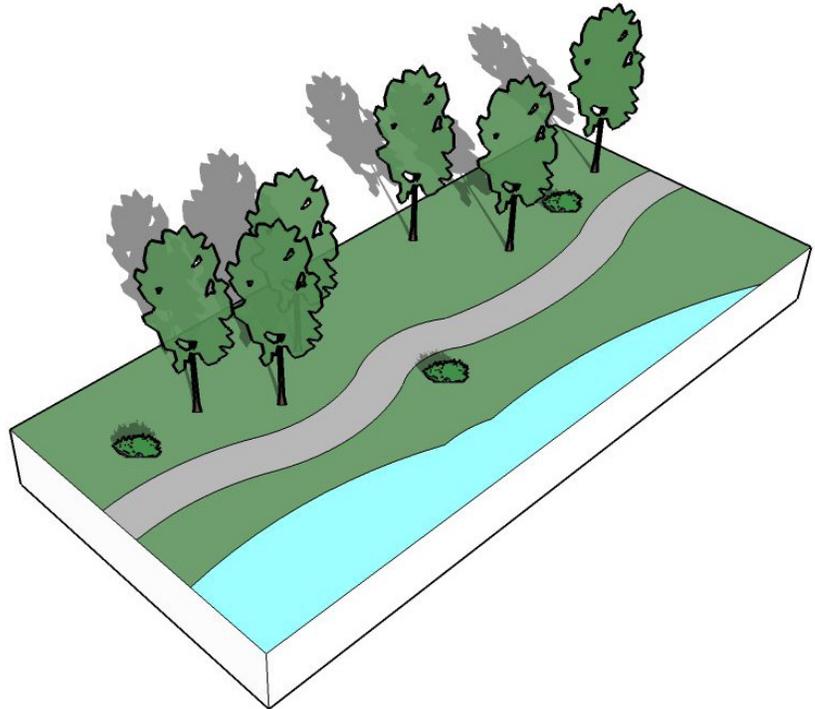
2.3.6 Conserved Green Space and Shared Open Space Standards

OPEN SPACE TYPE: Recreation Trails

DESCRIPTION A path for pedestrian use within a Shared Open Space or Conserved Green Space.



Character Examples



LOCATION	
Characteristics	Pedestrian trail in a Shared Open Space or Conserved Green Space
REQUIREMENTS	
Maximum Width (feet)	6
Publicly Accessible	Yes – connected to the street and pedestrian network
ELIGIBLE FEATURES	
Include:	All Open Space Types
SUBAREAS	
Allowed in Subarea(s)	All Subareas

2.3.7 PUD Subdivision Signage

PUD Subdivision Signage

The following parameters pertain to the installation, use, maintenance, and lighting standards of signs for PUD Subdivision submittals.

- A. Permitted Project Signs – The following signs are permitted in the Project, unless limited by the later provisions of this Section:
 1. Construction signs;
 2. Street control devices;
 3. Development signage;
 4. Signs not requiring a permit in accordance with *Section 3.11.5 of the Londonderry Zoning Ordinance*.
- B. Approval Authority – All Project signage requiring a permit shall be reviewed for conformity to this PUD Master Plan by the Zoning Administrator or appropriate authority.
- C. Appeals – The petitioner shall be entitled to appeal staff denials to the Planning Board.
- D. Measurement and calculation of area – The calculation shall be in accordance with *Section 3.11.5 of the Londonderry Zoning Ordinance*.
- E. Submittal Requirements – All individual signage applications independent of PUD Subdivision submittals shall include the following:
 1. Written approval from the Developer for proposed signage;
 2. Front, side and top elevations with sign drawn to scale;
 3. Color and material descriptions;
 4. Illumination method for the sign (if any);
 5. Landscaping for the sign (if any).

Design, Construction and Maintenance

- Signs or their supports shall not be placed in such position or manner as to obstruct or interfere, either physically or visually, with any fire alarm, police alarm, traffic signal or sign, or any devices maintained by or under public authority; or with vehicular or pedestrian ingress or egress to or from any public or private right-of-way, roadway, driveway, or sidewalk.
- All signs shall be constructed and secured so as to ensure the safety of pedestrians.
- The material and construction of any sign shall be in accordance with the requirements above and the Building Code.
- Permanent signs shall be made of durable materials.
- All signs shall be maintained in a safe condition and, together with their structural elements, shall be kept in good repair.

Street Control Devices

- Traffic Control Devices – In accordance with NHDOT Standard Specifications and the Manual on Uniform Traffic Control Devices, current as of the date of PUD Site Plan submittal, for the design, size and placement of traffic control devices. Any such signs to be placed near a roadway or an overhead sign structures shall be designed in accordance with the AASHTO standard specification for structural supports for highway traffic signs, luminaries, and traffic signals.
- Street Identification Signs – Design and sized in accordance with the Town of Londonderry standards, placement in accordance with the Manual on Uniform Traffic Control Devices, current as of the date of PUD Site Plan submittal. The Developer may select decorative sign poles and accent trim for Woodmont Com-

2.3.7 PUD Subdivision Signage

mons or for a Subarea within the development. Street signs also may include custom Woodmont Commons logos. The style, materials and finish shall be submitted to the Town for review and approval at the time of the first PUD Site Plan submittal for the Development or the Subarea.

- Regulatory, Warning & Guide Signs – Included stop signs, speed limit signs, parking signs, route signs, etc. in accordance with NHDOT Standard Specifications and the Manual on Uniform Traffic Control Devices, current as of the date of PUD Site Plan submittal, for the design, size and placement of signage. The Developer may select decorative sign poles and accent trim for Woodmont Commons or for a Subarea within the development. The style, materials and finish shall be submitted to the Town for review and approval at the time of the first PUD Site Plan submittal for the Development or the Subarea.
- Pavement Marking – In accordance with the Manual on Uniform Traffic Control Devices, current as of the date of PUD Site Plan submittal, for the design, size and placement of pavement marking for streets.

Development Signage

Identification Signage

- Gateway signs – These signs are permitted for the primary commercial entrances into the Woodmont Commons Development. The signs shall be freestanding ground signs and may include the Woodmont Commons name and or logo or other community branding. The sign area shall not exceed sixteen (16) square feet and signs may be located on each side of an entrance or within a median. Signs shall not be permitted at perimeter entrances abutting AR-I zoning. The signs may be illuminated using ground-mounted lights.



Examples of Gateway Signs



2.3.7 PUD Subdivision Signage

- Pole mounted directory signs – Pole mounted directory signs shall not exceed eight (8) square feet per side, not including the decorative pole and frame. The sign shall be at least one (1) foot from the edge of the street curb and seven feet above any walking surface. They shall be spaced at least twenty (20) feet apart.



Example of Pole Mounted Directory Sign

- Ground mounted directory signs – Ground mounted directory signs shall not exceed eighteen (18) square feet per side, not including the decorative stanchions. The sign shall be at least one (1) foot from the edge of the street curb. Signs shall be limited to eight (8) feet high, not including decorative stanchions.



Example of Ground Mounted Directory Sign

- The copy on the directional signage may include the Woodmont Commons name and/or logo, directional information and business names and logos. The number of signs shall be indicated on PUD Site Plan submittals. Directional signs may be externally illuminated. The Developer shall be responsible for the maintenance of all directional signage.

Informational Signs

- Wayfinding – Vehicular directional signage for wayfinding within the Development may be included within the public right-of-way within the WC-1-GL, WC-1, WC-2, WC-8 and WC-12 Subareas.
- Banners – The Developer or their assignee may establish a banner program for the street light poles within the WC-1-GL, WC-1, WC-2, WC-8 and WC-12 Subareas. The banners shall be a minimum of seven (7) feet above any walking surface and may project out eighteen (18) inches from the pole. The copy on the banners may include the Woodmont Commons name and or logo or other community branding and seasonal or event information. Banners shall not be illuminated. The Developer shall be responsible for the maintenance of all banners. Banners shall not require a permit.

2.3.8 PUD Subdivision Lighting

The standards are designed to reduce the impacts of glare, light trespass and over lighting and to promote safety, security and energy conservation. Lighting should emphasize both public and private features and destinations by using a minimum amount of light to meet these objectives. It should provide needed illumination of the site and, at the same time, prevent undesired off-site glare.

The purpose of the exterior lighting standards is to create a well-balanced, integrated lighting plan for rights-of-way and public properties that enhances vehicular and pedestrian visibility while minimizing lighting glare and contrast. The intent of the PUD Subdivision Lighting Standards is to provide consistent systems of pedestrian lighting that add to the character, aesthetic appeal, and safety, and thereby promote greater pedestrian activity. Outdoor lighting for PUD Subdivision Plans shall consist primarily of pedestrian-scale Streetlights.

Applicability

These outdoor lighting standards for PUD Subdivision and/or PUD Site Plans apply to the installation of new outdoor lighting fixtures or the replacement of any existing outdoor light fixtures located within the public rights-of-way for the Woodmont Commons PUD. The standards are designed to reduce the impacts of glare, light trespass and over lighting and to promote safety, security and energy conservation.

Guidelines

Street lighting shall be designed to cast light downward. Cut-off and/or shielded fixtures shall be used to reduce the impacts of glare on adjacent uses. The maximum mounting height of a non-building mounted lighting fixture above the ground shall be twenty (20) feet in residential areas and twenty-five (25) feet in nonresidential areas.

The placement of fixtures should provide a coordinated and organized appearance that works with

placement of street trees, curb cuts, signage and other features to contribute to the overall continuity of the streetscape.

Light sources should generally be metal halide or LED. Low wattage high-pressure sodium may be desirable in some residential settings where glare may be an issue.

Outdoor lighting is used to illuminate flags, monuments, signs, landscapes and other features must use cut-off or directionally shielded light fixtures that are aimed and controlled so that the light is substantially confined to the object to be illuminated. Such lighting must be shielded to prevent:

- Glare for drivers;
- Light trespass beyond the Woodmont Commons property line.

All outdoor lighting shall be subject to the Outdoor Lighting Standards of the *Londonberry Site Plan Regulations* except as amended herein:

- Internally lit signs are acceptable provided that they meet the requirements of *Section 2.4.4 PUD Site Plan Signage*;
- Adjacent to residential property at the perimeter of the Woodmont Commons PUD, no direct light source will be visible at the property line at ground level or above except as permitted for street lights and traffic control and safety devices, for streets, paths and trails;
- Development signage lighting may remain on after business hours.

Athletic Field Lighting

Sports field lighting systems are designed and evaluated based on existing standards that are readily available and generally accepted. These standards are:

- Illuminating Engineering Society of North America (IESNA);

2.3.8 PUD Subdivision Lighting

- Sports and Recreational Area Lighting (IESNA RP-6);
- Lighting for Exterior Environments (IESNA RP-33);
- Guide for Photometric Measurements of Area and Sports Lighting Installations (IESNA LM-5);
- Light Trespass: Research, Results and Recommendations (IESNA TM-11);
- National Little League Association Standards and Safety Audit.

If fields are to be illuminated, the requirements are summarized below.

- A. Illuminating Engineers Society of North America (IESNA) specifications shall become the standard for onfield performance, and that a qualified engineer with IESNA experience shall be retained to prepare the design the lighting.
- B. With respect to off-field lighting performance, the lighting shall be designed as per the following:
 1. That spill light shall be limited to the maximum allowed by IESNA publications TM-11 and RP-33 using the methods for measurement outlined in these publications;
 2. That glare shall be controlled by limiting the amount of candlepower as calculated from the worst-case aimed fixture;
 3. That sky glow shall be controlled by limiting the amount of uplight from the fixtures to that allowed from an IESNA cutoff classification.

2.3.9 PUD Subdivision Landscape

Landscaping within each of the public rights-of-way is used to provide an attractive setting for the Woodmont Commons PUD, enhance the quality of the environment, provide shade for pedestrians, reduce heat island effects, slow the vehicular traffic, and soften building and structure edges.

PUD Subdivision Plan landscaping shall be in accordance with the Landscape Design Standards of the *Londonderry Site Plan Regulations* except as modified herein and as follows:

The landscaping along the streets and travelled ways in the Woodmont Commons PUD include trees, landscaped medians and planting strips.

Street trees shall be planted within the street right-of-way generally and approximately as are illustrated in *Section 2.3.3 Transportation Network*. However, street trees may be shifted or eliminated along any street section as long as the number trees equals an average spacing of fifty (50) feet on center. No street trees shall be required within twenty-five (25) feet of an intersection. The size of trees to be planted in this area shall be a minimum of 2 ½ inch caliper at time of planting. Branching height of street trees shall not be less than seven (7) feet above grade when planted. Street trees must not be closer than twenty-five (25) feet apart at time of planting. Street trees may be located within planting strips or planting wells with tree grates. Trees shall not be required along sides of streets abutting tree preservation areas or open space where internal trees are planted and visible from the street.

In addition, landscaping will be governed by the following principles:

- Landscaping may be laid out in formal rows or informal drifts;
- Plant material located within twenty (20) feet of any road or other paved area will generally consist of species which are tolerant of roadway deicing methods and otherwise planted and maintained so as to maintain a pleasing public realm;

- Landscaping shall be designed so that it does not substantially interfere with vehicular or pedestrian sight distances as to create a public safety hazard;
- Alleys and lanes do not require street trees;
- Street trees are not required in areas where orchard-like planting is preserved or planted;
- Street trees are not required where existing trees are preserved in quantities sufficient to satisfy the street tree requirements;
- Median trees are not required but may be planted. There is no minimum or maximum spacing requirements;
- Invasive plant species will not be permitted.

Medians and Planting Strips – In addition to trees, medians and planting strips shall be covered with a minimum thickness of four (4) inches of suitable topsoil and be subsequently planted with grass seed, sod, or other vegetative cover.

Signage Landscaping – The ground mounted signage landscaping area shall be a minimum of a four (4) foot wide area surrounding each monument or pedestal sign base.

Street tree sizes and layout shall be in accordance with this Section.

All areas disturbed by construction and slated to remain vegetated shall be covered with a minimum thickness of four (4) inches of suitable topsoil and be subsequently planted with grass seed, sod, or other vegetative cover. Trees may be introduced into open spaces and may be used to satisfy the number of street trees required above.

2.3.10 Cultural Resources

The cultural resources on the Woodmont Common property include stone walls along existing property lines. The walls along the Woodmont Common exterior boundary will be preserved. Relocation of internal walls shall be permitted. Portions of stone walls disturbed by development shall be rebuilt in accordance with *Section 3.09c under 11h* of the *Londonderry Site Plan Regulations* so that they are visible by the public.

A portion of the site's agricultural heritage will be preserved within the buffer along Gilcreast Road.

Parcels 22 and 42 are not included within the Woodmont Commons PUD and, as such, are not subject to the provisions of this Master Plan. It is noted that the buildings on these properties are considered historic by the Town of Londonderry.

2.3.11 Utility Infrastructure

The Woodmont Commons PUD shall be served by public and private utilities as required for the specific tenant uses identified in the PUD Subdivision and/or PUD Site Plan applications. As described in *Section 3.05* of the *Londonderry Subdivision Regulations* dated June 10, 2009, the Developer shall provide a Utility Clearance Letter from each applicable non-municipal utility provider. Other accommodations with municipally owned utilities shall be coordinated directly with staff from the Department of Public Works. The location of planned public and private utilities shall not be subject to the location requirements at the end of *Section 3.05*; the utilities shall be located within the planned roadway or utilities rights-of-way or easements, whether publicly or privately held, in a manner that allows for continued maintenance of the utility system.

Water

- Approved Methods – The applicant shall identify water service to or through each phase of the PUD Subdivision upon submission of a PUD Subdivision application as identified in *Section 3.06* of the *Londonderry Subdivision Regulations*. This shall be coordinated with Pennichuck Water Works Company (PWWC) or any other licensed entity providing municipal water service to the Woodmont Commons PUD.
- Performance and Operational Standards – The applicant shall provide written evidence that the domestic water supply volume and pressure and fire flow requirements are satisfactory for the proposed PUD Subdivision and does not compromise the minimum service requirements for abutting residences and businesses as defined by PWWC and the Londonderry Fire Department.

Sewer

- Approved Methods – The applicant shall identify a municipal sanitary sewer system service to each area of the PUD Subdivision as required in *Section 3.07* of the *Londonderry Subdivision Regulations* and consistent with the conceptual layout provided in the PUD Master Plan.
- Performance and Operational Standards – The applicant shall provide the sewer capacity analysis for each PUD Subdivision unless waived by the Londonderry Department of Public Works due to site-specific conditions.

Stormwater Management

- Approved Methods – The applicant shall identify a storm drain system for PUD Subdivision as identified in *Section 3.08* of the *Londonderry Subdivision Regulations* and consistent with site layout and treatment measures provided in the PUD Master Plan.
- Performance and Operational Standards – The applicant shall provide analysis and stormwater treatment, infiltration, and/or detention measures that mitigate the identified impacts from the PUD Subdivision. The documentation provided to the Town of Londonderry shall be coordinated with the documents submitted to the New Hampshire Department of Environmental Services for those portions of the project that require their review based on the State's written thresholds for an Alteration of Terrain (AOT) Permit, if applicable. Consistent with the general requirements of the Town's standards for storm drain systems and the State of New Hampshire's Alteration of Terrain Permit Regulations (ENV-WQ 1500), the post-development runoff rates shall not exceed the pre-development runoff rates at downstream or down gradient locations.

2.4 PUD SITE PLAN REGULATIONS AND STANDARDS

2.4.1 General

The PUD Site Plan Regulations and Standards regulate how the land within the Woodmont Commons PUD will be developed at the building and lot level. These regulations and standards apply to the developable areas as indicated on the Land Use Plans.

Note that the Compliance Alternatives in *Section 2.2.1 Description* apply to the PUD Site Plan Regulations and Standards.

2.4.2 Buildings and Lots

The placement and location of building and parking shall be regulated by the Lot Types and the vertical built form is regulated by the Building Types. In addition, the Building Types include both architectural standards and architectural guidelines. The standards are mandatory but the guidelines are not. The guidelines are provided in order to inform developers of the design objectives and guide the review of all submittals with the goal of creating building designs that are sensitive to their context within the community.

Specific Lot Types permitted in Subareas are indicated for each type, but the uses permitted in Subareas are regulated by the Allowable Uses table.

The building placement on the lot is controlled by both the front setback and the frontage occupancy. The front facade of the structure must be between the minimum and maximum setback lines. Encroachments may be permitted into the setback and in some cases, over the setback in accordance with *Section 2.4.3 Encroachment and Shared Use Agreements*.

The placement of parking on the lot indicates how far behind the primary façade the parking space, lot or structure must be located. It is contingent upon the building's placement within the setback. Note that for single family and for duplex dwellings the parking

setback is for the garage or regular overnight parking and that short term parking is permitted within the setback on front loading driveways.

Lots

There may be special circumstances affecting lots that may require a deviation from the lot standards. This includes naturally occurring conditions such as but not limited to slope, unstable soil conditions and preservation of existing trees. Any such circumstances shall be clearly noted on the site plan submittal for consideration by the staff and Planning Board. Approval of any modification to the site plan requirements is at the discretion of the Planning Board.

Architectural Guidelines

Building design should reinforce community character of the Woodmont Commons PUD and should be visually and physically compatible with adjacent uses. Public façades, ground floors, entrances, storefront windows, and signage and lighting should be designed to articulate the pedestrian scaled architecture and create visual interest. Buildings should transition in scale and height as they approach properties of smaller scale.

The Architectural Standards and Guidelines are included with each Building Type and supersede *Section 3.12 Building and General Appearance Design Standards* of the *Londonderry Site Plan Regulations*.

Rooftop mechanical equipment shall be screened from view from streets and public frontages. Screening may be provided by extending the parapet of the building; by a roof; or by enclosing the mechanical equipment with a roof equipment screen.

Scale and Proportion Guidelines

Scale in buildings is relative to size. It refers to how we perceive the size of the building elements relative to other forms, and to the human body. There are two

2.4.2 Buildings and Lots

types of scale addressed by the guidelines. They are overall scale and human scale.

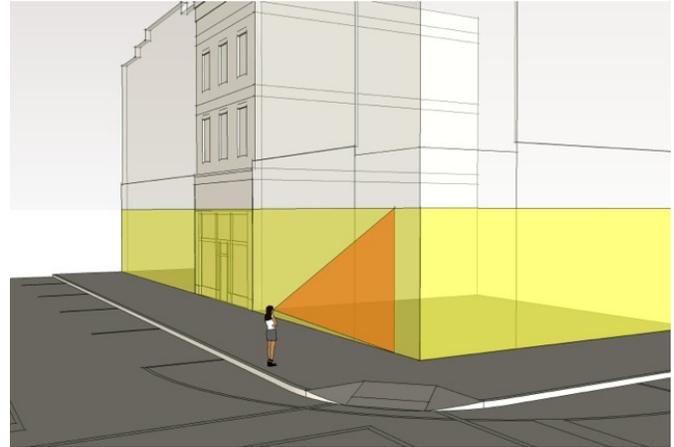
Overall scale is the perception of a building from a distance. For example, a building may be segmented into a base, middle and top to improve the perception of the building from a distance. Human scale is the legibility of elements when one is very close to a building. The detailing of the storefront in a commercial building is a good example. Good buildings should incorporate both types of scale simultaneously with an emphasis on human scale.

It is important that up close, buildings possess a level of refinement that is tangible. Detaining within the pedestrian cone of vision not only provides comfort by allowing one to judge the size of a space, but also give the building a human scale.

Proportion refers to the relationship of the parts to each other or to the whole. This can include the relationship of two ratios such as height to width. When building elements are in proportion, their relative sizes are the same.

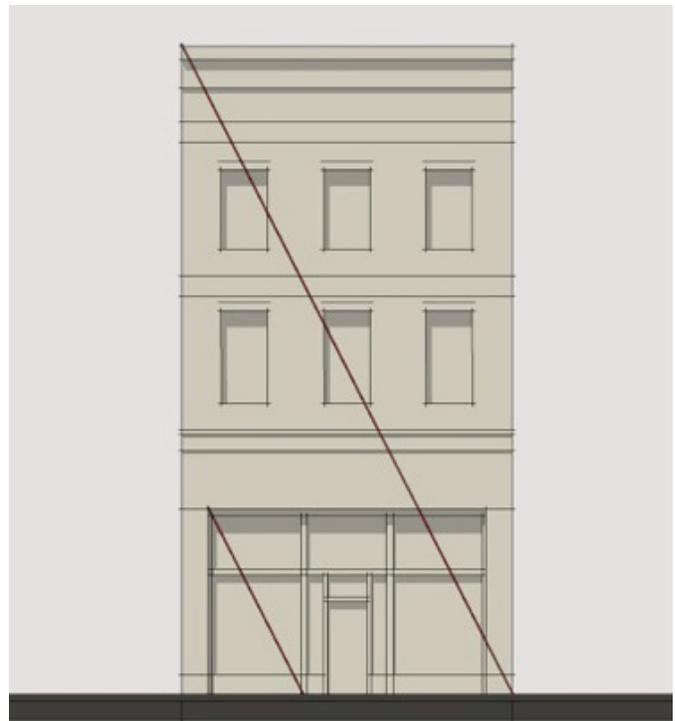
A building's architectural features can be used to organize the perceived mass of larger buildings. Building features such as columns, piers, roof lines and brick patterns can divide and create vertical orientation on a large surface. This can refer to the overall building facade as well as openings for windows and doors within it.

Vertical proportions are preferred and consistent proportioning systems are encouraged, preserving a width-to-height ratio along a facade. This ratio can differ among facades, but once chosen should be used for the entire facade. Once these proportions have been established, the windows and doors should reinforce the vertical orientation of the composition.



Human Scale

Refinement most effective up close within the pedestrian cone of vision.



Vertical Proportion

An example of a vertical proportioning system used for the facade and the windows.

2.4.2 Buildings and Lots

Accessory Structures

Accessory Structures may be allowed on any Lot Type in accordance with the definition of Accessory Structure in *Section 4.7 Definitions* of the *Londonderry Zoning Ordinance*.

List of Building and Lot Types

	PAGE NUMBERS	
	BUILDING	LOT
SINGLE-FAMILY DETACHED	178	179
SINGLE-FAMILY ATTACHED	180	181
TWO-FAMILY	182	183
MULTI-FAMILY	184	185
SMALL BUILDING	186	187
MEDIUM BUILDING	188	189
LARGE BUILDING	190	191
CIVIC BUILDING	192	193
DEDICATED OFFICE	194	195
LARGE FORMAT RETAIL	196	197
INSTITUTIONAL BUILDING	198	199
PARKING STRUCTURE	200	201
FLEX	202	203

2.4.2 Buildings and Lots

BUILDING TYPE: Single-Family Detached

DESCRIPTION Dwelling units that are consistent with traditional residential development patterns. Cottage Courts are allowed. Façade orientation to the street, walkways to entrances and landscaping to define street edges and open spaces contribute to the neighborhood scale. Site may have a detached or attached accessory structure.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

Ground Floor Limitations	Residential
Building Height	
Primary Building Height (maximum)	3 stories / 35'
Ground Floor Height (feet)	Not applicable
Upper Floor Height (feet)	Not applicable
Finished Floor Elevation (above grade at front façade)	18" minimum
Main Roof Pitch	4:12 min / 12:12 max
Flat Roofs Permitted	No

TRANSPARENCY AND ARTICULATION

Ground Floor Transparency (% of façade)	Not applicable
Upper Floor Transparency (% of façade)	Not applicable
Street Frontage Wall Length Without Offset (feet)	Not applicable
Street Frontage Wall Offset – length/depth (feet)	Not applicable

BUILDING ENTRANCES

Street Facing Entry Required	Yes
Entrance Spacing (maximum feet)	Not applicable

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street with pedestrian access and entry or to a common court in the case of Cottage Court units. Vehicular access may be provided at the frontyard, sideyard or rear yard

RELATIONSHIP TO STREET

Primary building façade shall be oriented to address the street including the building entry. Semi-private outdoor space shall be oriented to the street if provided at the frontyard

TREATMENT OF GROUND FLOOR

None

SCALE AND PROPORTION

Massing of building shall be consistent with neighboring homes and existing development. Buildings shall have a pedestrian scale in building height and bulk

VERTICAL AND HORIZONTAL EMPHASIS

None

LOCATION AND ARTICULATION OF ENTRY

Building entry shall be oriented to the principal street. The building massing shall reinforce the prominence of the entry as a building feature

PLACEMENT OF WINDOWS AND DOORS

Windows shall be placed in a regular pattern of punched openings. Windows and doors shall be scaled to reinforce a residential and pedestrian character

ROOF FORM

Main roof shall be pitched. Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs

ALLOWABLE LIGHTING TYPES

Building lighting, site lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

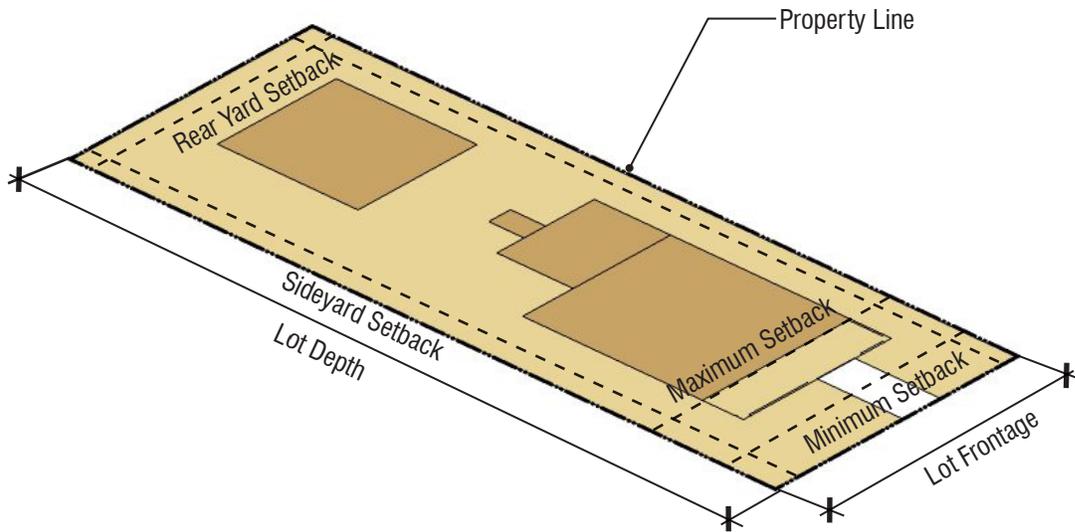
Balcony, bay window, porches, stoops, stairs

ENCROACHMENTS IN PUBLIC RIGHT OF WAY

None

2.4.2 Buildings and Lots

LOT TYPE: Single-Family Detached



LOT STANDARDS (APPLY AS SHOWN BELOW UNLESS AFFECTED BY A PERIMETER BUFFER AS SHOWN IN SECTION 2.2.2 LAND USE PLAN)

Lot Frontage (feet)	40 minimum
Lot Depth (feet)	80 minimum
Lot Area (square feet)	3,200 minimum

BUILDING PLACEMENT ON LOT

Front Yard Setback (feet)	10 minimum/25 maximum
Side Yard Setback (feet)	10 minimum
Rear Yard Setback (feet)	5 minimum
Side-Street Yard (feet on corner lots)	20 minimum
Frontage Occupancy (%)	30% minimum

PARKING PLACEMENT ON LOT

Front Parking Setback from primary façade (feet)	20 minimum
Side and Rear Parking Setback (feet)	5 minimum

SUBAREAS

Allowed in Subarea(s)	All Subareas except WC-1-GL and WC-3
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2.4.2 Buildings and Lots

BUILDING TYPE: Single-Family Attached



DESCRIPTION A small free-standing building that may have a detached or attached accessory garage structure. The detached building type shall be oriented to the street with an articulated primary entry.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

GROUND FLOOR LIMITATIONS Residential

BUILDING HEIGHT

Primary Building Height (maximum) 3 stories / 35'

Ground Floor Height (feet) Not applicable

Upper Floor Height (feet) Not applicable

Finished Floor Elevation (above grade at front façade) 18" minimum

Main Roof Pitch 4:12 min / 12:12 max

Flat Roofs Permitted No

TRANSPARENCY AND ARTICULATION

Ground Floor Transparency (% of façade) Not applicable

Upper Floor Transparency (% of façade) Not applicable

Street Frontage Wall Length Without Offset (feet) Not applicable

Street Frontage Wall Offset – length/depth (feet) Not applicable

BUILDING ENTRANCES

Street Facing Entry Required Yes

Entrance Spacing (maximum feet) Not applicable

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street with pedestrian access and entry. Vehicular access shall be provided at the side yard or rear of the site

RELATIONSHIP TO STREET

Primary building façade shall be oriented to address the street along with the building entry. Semi-private outdoor space shall be oriented to the street if provided at the frontyard

TREATMENT OF GROUND FLOOR

None

SCALE AND PROPORTION

Architectural elements shall be employed to reduce overall scale of continuous blocks and to articulate a residential scale

VERTICAL AND HORIZONTAL EMPHASIS

None

LOCATION AND ARTICULATION OF ENTRY

Building entry shall be oriented to the principal street

PLACEMENT OF WINDOWS AND DOORS

Windows shall be placed in a regular pattern of punched openings. Windows and doors shall be scaled to reinforce a residential character

ROOF FORM

Main roof shall be pitched. Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs

ALLOWABLE LIGHTING TYPES

Building lighting, site lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

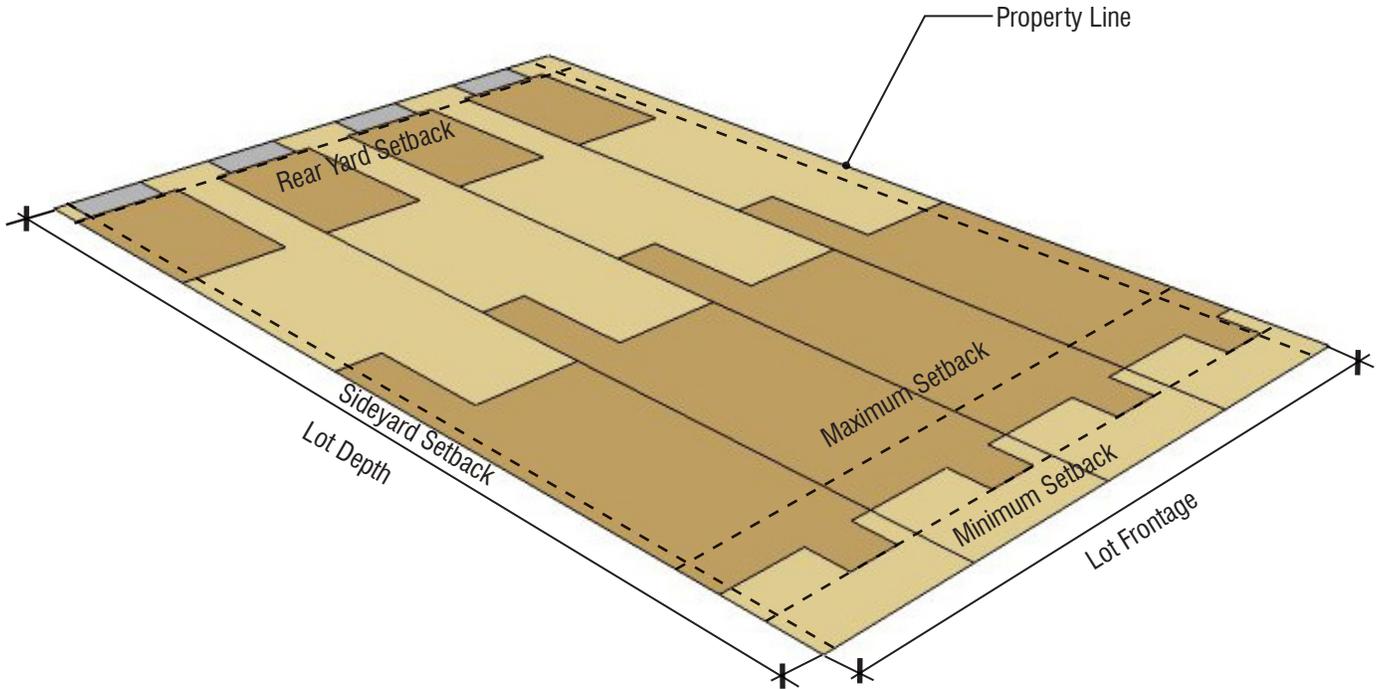
Balcony, bay window, porches, stoops, stairs

ENCROACHMENTS IN PUBLIC RIGHT OF WAY

None

2.4.2 Buildings and Lots

LOT TYPE: Single-Family Attached



LOT STANDARDS APPLY AS SHOWN BELOW UNLESS AFFECTED BY A PERIMETER BUFFER AS SHOWN IN SECTION 2.2.2 LAND USE PLAN)

Lot Frontage (feet)	18 minimum/30 maximum
Lot Depth (feet)	45 minimum
Lot Area (square feet)	810 minimum

BUILDING PLACEMENT ON LOT

Front Yard Setback (feet)	5 minimum/25 maximum
Side Yard Setback (feet)	0 or 5 at corners
Rear Yard Setback (feet)	5
Side-Street Yard (feet on corner lots)	20 minimum
Frontage Occupancy (%)	30% minimum

PARKING PLACEMENT ON LOT

Front Parking Setback from primary façade (feet)	Not applicable; parking in rear of lot
Side and Rear Parking Setback (feet)	5 minimum

SUBAREAS

Allowed in Subarea(s)	WC-1, WC-2, WC-4, WC-6, WC-8, WC-9, WC-10, WC-11, WC-12
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2.4.2 Buildings and Lots

BUILDING TYPE: Two-Family

DESCRIPTION A small free-standing residential building with two dwelling units that may have a detached or attached accessory garage. The primary building type shall be oriented to the street with an articulated primary entry.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

GROUND FLOOR LIMITATIONS Residential

BUILDING HEIGHT

Primary Building Height (maximum) 3 stories / 35'

Ground Floor Height (feet) Not applicable

Upper Floor Height (feet) Not applicable

Finished Floor Elevation (above grade at front façade) 18" minimum

Main Roof Pitch 4:12 min / 12:12 max

Flat Roofs Permitted No

TRANSPARENCY AND ARTICULATION

Ground Floor Transparency (% of façade) Not applicable

Upper Floor Transparency (% of façade) Not applicable

Street Frontage Wall Length Without Offset (feet) Not applicable

Street Frontage Wall Offset – length/depth (feet) Not applicable

BUILDING ENTRANCES

Street Facing Entry Required Yes

Entrance Spacing (maximum feet) Not applicable

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be configured to appear as a single family residence

RELATIONSHIP TO STREET

Primary building façade shall be oriented to address the street including the building entry

TREATMENT OF GROUND FLOOR

None

SCALE AND PROPORTION

Massing of building shall be consistent with neighboring homes and existing development. Buildings shall have a pedestrian scale in building height and bulk

VERTICAL AND HORIZONTAL EMPHASIS

None

LOCATION AND ARTICULATION OF ENTRY

Building entry shall be oriented to the principal street

PLACEMENT OF WINDOWS AND DOORS

Windows shall be placed in a regular pattern of punched openings. Windows and doors shall be scaled to reinforce a residential and pedestrian character

ROOF FORM

Main roof shall be pitched. Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs

ALLOWABLE LIGHTING TYPES

Building lighting, site lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

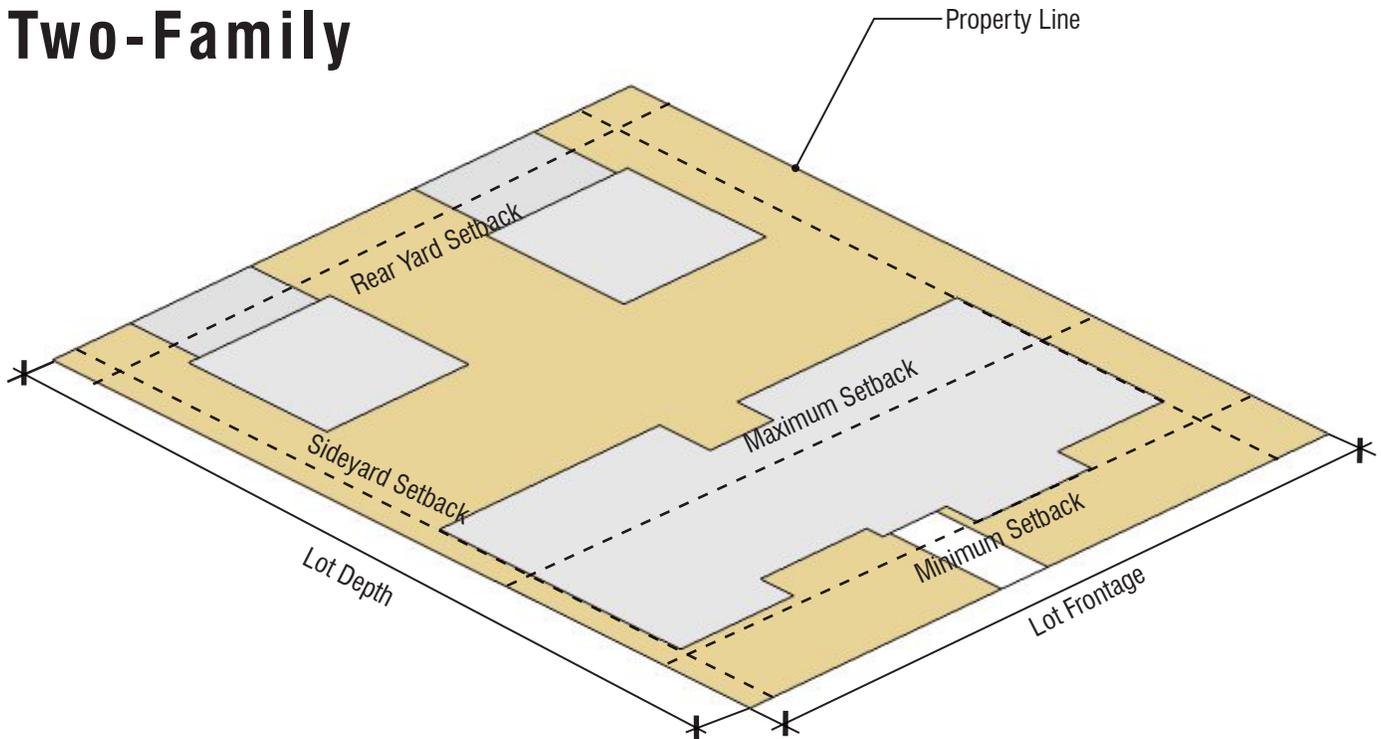
Balcony, bay window, porches, stoops, stairs

ENCROACHMENTS IN PUBLIC RIGHT OF WAY

None

2.4.2 Buildings and Lots

LOT TYPE: Two-Family



LOT STANDARDS APPLY AS SHOWN BELOW UNLESS AFFECTED BY A PERIMETER BUFFER AS SHOWN IN SECTION 2.2.2 LAND USE PLAN)

Lot Frontage (feet)	40 minimum
Lot Depth (feet)	80 minimum
Lot Area (square feet)	3,200 minimum

BUILDING PLACEMENT ON LOT

Front Yard Setback (feet)	10 minimum/25 maximum
Side Yard Setback (feet)	5 minimum
Rear Yard Setback (feet)	5 minimum
Side-Street Yard (feet on corner lots)	20 minimum
Frontage Occupancy (%)	30% minimum

PARKING PLACEMENT ON LOT

Front Parking Setback from primary façade (feet)	20 minimum
Side and Rear Parking Setback (feet)	5 minimum

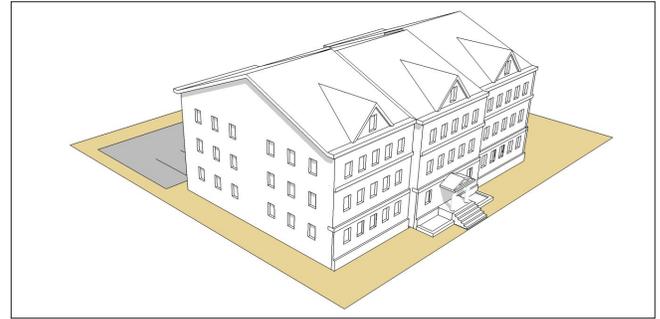
SUBAREAS

Allowed in Subarea(s)	WC-1, WC-2, WC-4, WC-6, WC-8, WC-9, WC-10, WC-11, WC-12
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2.4.2 Buildings and Lots

BUILDING TYPE: Multi-family

DESCRIPTION A free-standing building that may have a detached or attached accessory garage structure. The detached building type shall be oriented to the street with an articulated primary entry. No single multi-family building shall contain more than twenty-four (24) dwelling units.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

GROUND FLOOR LIMITATIONS	Residential
BUILDING HEIGHT	
Primary Building Height (maximum)	3 stories / 35' 4 stories/50' in WC-1, WC-2 and WC-12
Ground Floor Height (feet)	Not applicable
Upper Floor Height (feet)	Not applicable
Finished Floor Elevation (above grade at front façade)	Not applicable
Main Roof Pitch	4:12 min / 12:12 max
Flat Roofs Permitted	Yes

TRANSPARENCY AND ARTICULATION

Ground Floor Transparency (% of façade)	Not applicable
Upper Floor Transparency (% of façade)	Not applicable
Street Frontage Wall Length Without Offset (feet)	100 maximum
Street Frontage Wall Offset – length/depth (feet)	4/8 minimum

BUILDING ENTRANCES

Street Facing Entry Required	Yes
Entrance Spacing (maximum feet)	100

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street with pedestrian access and entry. Building shall clearly delineate primary entrance

RELATIONSHIP TO STREET

Primary building façade shall be oriented to address the street

TREATMENT OF GROUND FLOOR

Ground floor shall be articulated with a varied material to anchor the structure

SCALE AND PROPORTION

Proportion and configuration of building elements shall be used to reduce the overall scale of multi-family dwellings

VERTICAL AND HORIZONTAL EMPHASIS

Horizontal building elements, material changes, and articulation of façades shall be used to de-emphasize the vertical nature of multi-family buildings

LOCATION AND ARTICULATION OF ENTRY

Building shall clearly delineate primary entrance; all entries shall be covered

PLACEMENT OF WINDOWS AND DOORS

Windows shall be placed in a regular pattern that is coordinated to reduce the scale of larger building masses

ROOF FORM

Main roof shall be pitched. Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs, site signage

ALLOWABLE LIGHTING TYPES

Building lighting, site lighting, parking lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

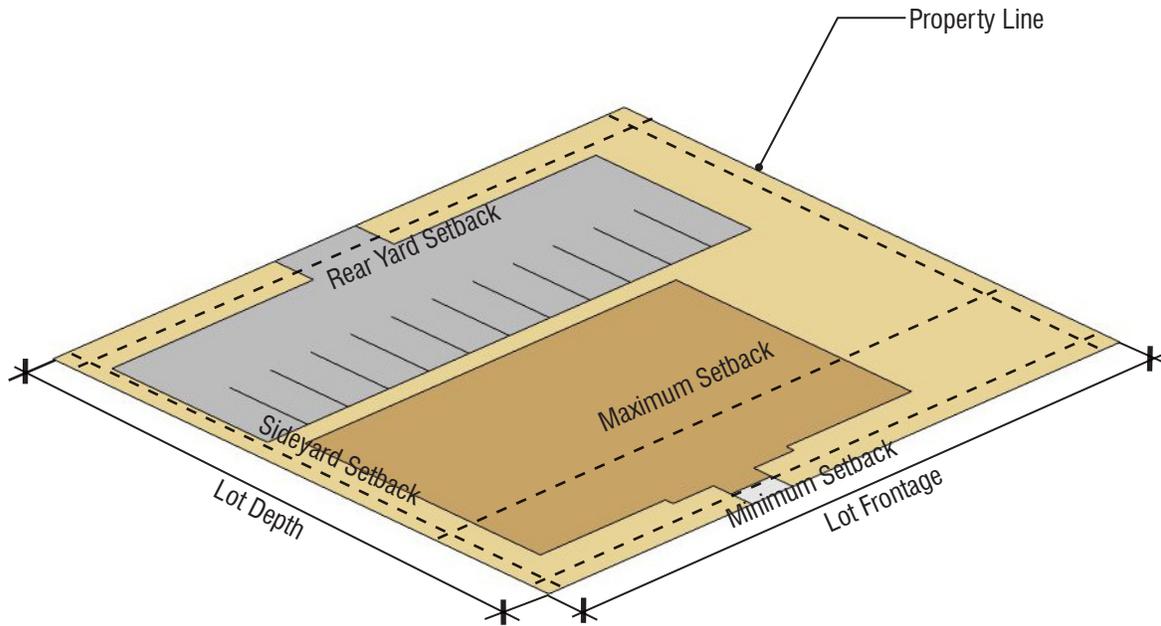
Porches, stoops, stairs, terraces, bay windows, balconies, awnings, signage, lighting, gallery, forecourt

ENCROACHMENTS IN PUBLIC RIGHT OF WAY

None

2.4.2 Buildings and Lots

LOT TYPE: Multi-family



LOT STANDARDS

Lot Frontage (feet)	40 minimum
Lot Depth (feet)	80 minimum
Lot Area (square feet)	3,200 minimum

BUILDING PLACEMENT ON LOT

Front Yard Setback (feet)	5 minimum/25 maximum
Side Yard Setback (feet)	5 minimum
Rear Yard Setback (feet)	5 minimum
Side-Street Yard (feet on corner lots)	5 minimum
Frontage Occupancy (%)	30% minimum

PARKING PLACEMENT ON LOT

Front Parking Setback from primary façade (feet)	Not applicable; parking in rear of lot
Side and Rear Parking Setback (feet)	5 minimum

PARKING PLACEMENT ON SIDE STREETS

Front Lot Setback (feet)	80 maximum
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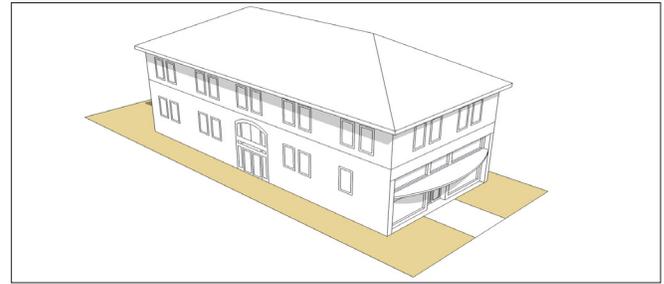
SUBAREAS

Allowed in Subarea(s)	WC-1, WC-2, WC-8, WC-9, WC-10, WC-11, WC-12
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2.4.2 Buildings and Lots

BUILDING TYPE: Small Building

DESCRIPTION A small scale building designed to accommodate multiple uses including residential or office upper floors with retail or service ground floor uses. Flex Use is limited to 5,000 square feet per use, per unit. Ground floor uses are intended to address the street with entries, and storefronts. Small Buildings may be located on all Block Types. A Small Building has a maximum of 10,000 square feet per floor.



Character Examples

ARCHITECTURAL STANDARDS

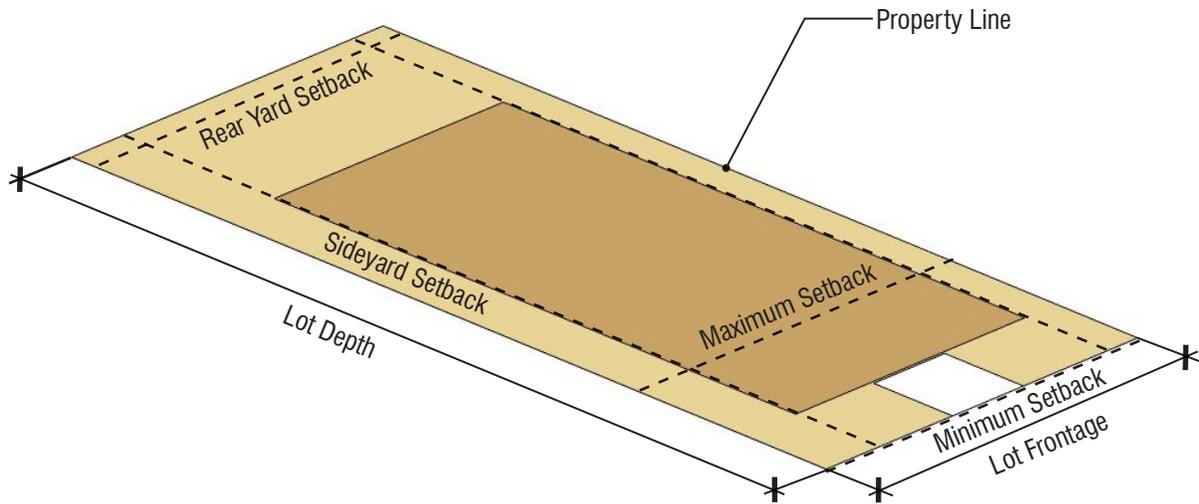
PLACEMENT OF SPECIFIC USES		TRANSPARENCY AND ARTICULATION	
GROUND FLOOR LIMITATIONS	Retail, service, commercial	Ground Floor Transparency (% of façade)	30% minimum
BUILDING HEIGHT		Upper Floor Transparency (% of façade)	20% minimum
Primary Building Height (maximum)	1 story (16' minimum) / 3 stories (35' maximum)	Street Frontage Wall Length Without Offset (feet)	50 maximum
Ground Floor Height (feet)	10 residential / 12 other uses minimum	Street Frontage Wall Offset – length/depth (feet)	5/1 minimum
Upper Floor Height (feet)	9 minimum	BUILDING ENTRANCES	
Finished Floor Elevation (above grade at front façade)	Not applicable	Street Facing Entry Required	Yes
Main Roof Pitch	12:12 maximum	Entrance Spacing (maximum feet)	75
Flat Roofs Permitted	Yes		

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION	PLACEMENT OF WINDOWS AND DOORS
Building shall be oriented to the street with pedestrian access and entry. Building shall clearly delineate primary entrance	Windows shall be placed in a regular pattern that is coordinated to reduce the scale of larger building masses
RELATIONSHIP TO STREET	ROOF FORM
Primary building façade shall be oriented to address the street and frame and contribute to street activity	Roof style and pitch may vary
TREATMENT OF GROUND FLOOR	ALLOWABLE SIGNAGE TYPES
Ground floor shall provide an active façade, including transparent windows, that contributes to the public realm	Address signs, Directional, site, building-attached, building-awning, and building-window
SCALE AND PROPORTION	ALLOWABLE LIGHTING TYPES
Proportion and configuration of building elements shall be used to reduce the overall scale of building	Building lighting, storefront lighting, site lighting, parking lighting and pedestrian lighting
VERTICAL AND HORIZONTAL EMPHASIS	ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)
Horizontal building elements, material changes, and articulation of façades shall be used to reduce building mass and perceived height	For residential uses: porches, stoops, stairs, terraces, bay windows, balconies, awnings, signage, lighting, forecourt
LOCATION AND ARTICULATION OF ENTRY	ENCROACHMENTS IN PUBLIC RIGHT OF WAY
Building shall clearly delineate separate entries for separate uses	Gallery, awnings signage, lighting, sidewalk dining, displays

2.4.2 Buildings and Lots

LOT TYPE: Small Building



LOT STANDARDS

Lot Frontage (feet)	50 minimum / 75 maximum
Lot Depth (feet)	Not applicable
Lot Area (square feet)	5,000 minimum / 7,500 maximum

BUILDING PLACEMENT ON LOT

Front Yard Setback (feet)	0 minimum / 25 maximum
Side Yard Setback (feet)	0 minimum / 15 maximum
Rear Yard Setback (feet)	5 feet minimum
Side-Street Yard (feet on corner lots)	0 minimum / 15 maximum
Frontage Occupancy (%)	50% minimum

PARKING PLACEMENT ON LOT

Front Parking Setback from primary façade (feet)	8 minimum / no maximum
Side and Rear Parking Setback (feet)	0 feet

SUBAREAS

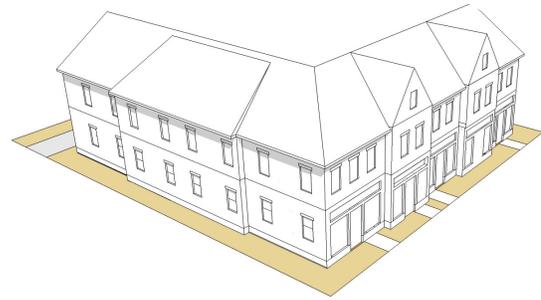
Allowed in Subarea(s)	WC-1-GL, WC-1, WC-2, WC-4, WC-6, WC-8, WC-10, WC-12
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ADDITIONAL REGULATIONS

Other	Streets, Landscaping, Lighting, Signage, Stormwater
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2.4.2 Buildings and Lots

BUILDING TYPE: Medium Building



DESCRIPTION A medium scale building designed to accommodate multiple uses including residential or office upper floors with retail, mixed use or service ground floor uses. Flex Use is limited to 5,000 square feet per use, per unit. Ground floor uses are intended to address the street with entries and storefronts. A Medium Building has a maximum of 25,000 square feet per floor.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

GROUND FLOOR LIMITATIONS Retail, service, mixed use

BUILDING HEIGHT

Primary Building Height (maximum) 1 story (16' minimum) / 4 stories (50' maximum)

Ground Floor Height (feet) 10 residential / 12 other uses minimum

Upper Floor Height (feet) 9 minimum

Finished Floor Elevation (above grade at front façade) Not applicable

Main Roof Pitch 12:12 maximum

Flat Roofs Permitted Yes, at 2 stories or more

TRANSPARENCY AND ARTICULATION

Ground Floor Transparency (% of façade) 30% minimum

Upper Floor Transparency (% of façade) 20% minimum

Street Frontage Wall Length Without Offset (feet) 50 maximum

Street Frontage Wall Offset – length/depth (feet) 5/1 minimum

BUILDING ENTRANCES

Street Facing Entry Required Yes

Entrance Spacing (maximum feet) 75

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street with pedestrian access and entry. Building shall clearly delineate primary entrance

RELATIONSHIP TO STREET

Primary building façade shall be oriented to address the street and frame and contribute to street activity

TREATMENT OF GROUND FLOOR

Ground floor shall provide an active façade, including transparent windows, that contributes to the public realm

SCALE AND PROPORTION

Proportion and configuration of building elements shall be used to reduce the overall scale of building

VERTICAL AND HORIZONTAL EMPHASIS

Horizontal building elements, material changes, and articulation of façades shall be used to reduce building mass and perceived height

LOCATION AND ARTICULATION OF ENTRY

Building shall clearly delineate separate entries for separate uses

PLACEMENT OF WINDOWS AND DOORS

Windows shall be placed in a regular pattern that is coordinated to reduce the scale of larger building masses

ROOF FORM

Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs, directional, site, building-attached, building-awning, and building-window

ALLOWABLE LIGHTING TYPES

Building lighting, storefront lighting, site lighting, parking lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

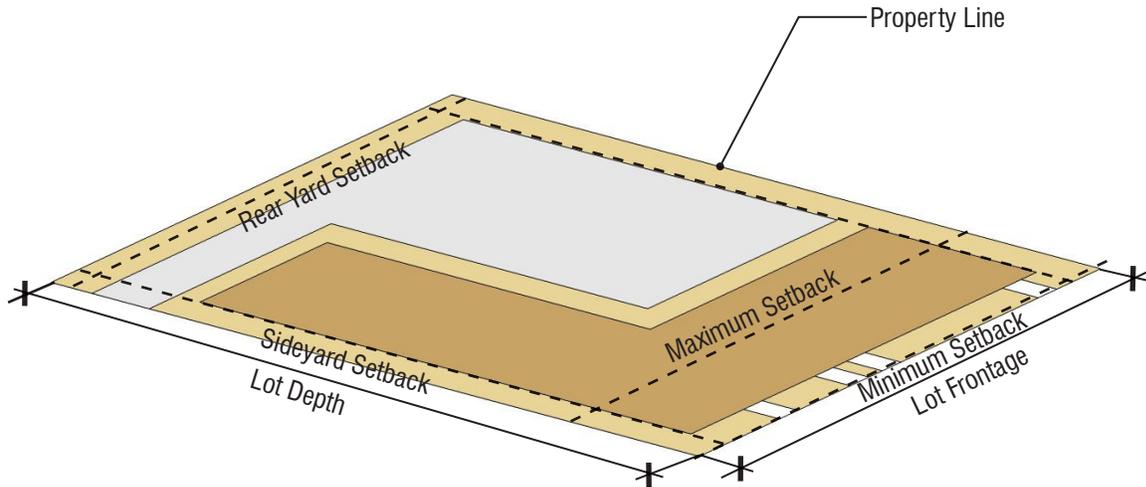
For residential uses: Porches, stoops, stairs, terraces, bay windows, balconies, awnings, signage, lighting, forecourt

ENCROACHMENTS IN PUBLIC RIGHT OF WAY

Gallery, awnings signage, lighting, sidewalk dining, displays

2.4.2 Buildings and Lots

LOT TYPE: Medium Building

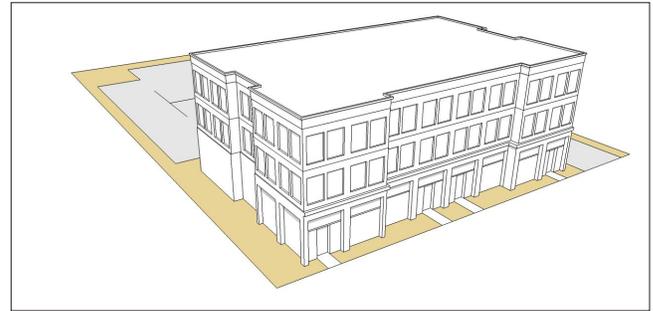


LOT STANDARDS	
Lot Frontage (feet)	75 minimum / 150 maximum
Lot Depth (feet)	100 minimum / 250 maximum
Lot Area (square feet)	7,500 minimum / 37,500 maximum
BUILDING PLACEMENT ON LOT	
Front Yard Setback (feet)	0 minimum / 25 maximum
Side Yard Setback (feet)	0 minimum / 15 maximum
Rear Yard Setback (feet)	5 feet minimum
Side-Street Yard (feet on corner lots)	0 minimum / 15 maximum
Frontage Occupancy (%)	60% minimum
PARKING PLACEMENT ON LOT	
Front Parking Setback from primary façade (feet)	8 minimum / no maximum
Side and Rear Parking Setback (feet)	0 feet
SUBAREAS	
Allowed in Subarea(s)	WC-1-GL, WC-1, WC-2, WC-10, WC-12
ADDITIONAL REGULATIONS	
Other	Streets, Landscaping, Lighting, Signage, Stormwater

2.4.2 Buildings and Lots

BUILDING TYPE: Large Building

DESCRIPTION A large scale building designed to accommodate multiple uses including residential or office upper floors with retail, mixed use or service ground floor uses. Flex Use is limited to 5,000 square feet per use, per unit. Ground Floor uses are intended to address the street with entries and storefronts. A Large Building has a minimum of 25,000 square feet per floor. In WC-1 only, a Large Building may be an office building with ancillary uses on the ground floor.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

GROUND FLOOR LIMITATIONS None

BUILDING HEIGHT
Primary Building Height (maximum) 50 maximum

Ground Floor Height (feet) 10 residential / 14 other uses minimum

Upper Floor Height (feet) -

Finished Floor Elevation (above grade at front façade) -

Main Roof Pitch -

Flat Roofs Permitted Yes

TRANSPARENCY AND ARTICULATION

Ground Floor Transparency (% of façade) 30% minimum

Upper Floor Transparency (% of façade) 20% minimum

Street Frontage Wall Length Without Offset (feet) 60 maximum articulation at corners; 120 mid-block

Street Frontage Wall Offset – length/depth (feet) 10/2 minimum, 4-/20 maximum

BUILDING ENTRANCES

Street Facing Entry Required Yes

Entrance Spacing (maximum feet) 80 maximum

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street with pedestrian access and entry. Building shall clearly delineate primary entrance

RELATIONSHIP TO STREET

Primary building façade shall be oriented to address the street and frame and contribute to street activity

TREATMENT OF GROUND FLOOR

Ground floor shall provide an active façade, including transparent windows, that contributes to the public realm

SCALE AND PROPORTION

Architectural features and treatments shall be used to reduce the overall scale and mass of the building. Façades visible from a street should be articulated to reduce the mass of the building. Ground floor façades that face a public street shall have galleries, arcades, display windows, entry areas, and/or awnings, or other such features along no less than 60% of their horizontal length

PLACEMENT OF WINDOWS AND DOORS

Windows shall be placed in a regular pattern that is coordinated to reduce the scale of larger building masses

ROOF FORM

Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs, directional, site, building-attached, building-awning, and building-window

ALLOWABLE LIGHTING TYPES

Building lighting, storefront lighting, site lighting, parking lighting and pedestrian lighting

2.4.2 Buildings and Lots

VERTICAL AND HORIZONTAL EMPHASIS

Horizontal building elements, material changes, and articulation of façades shall be used to reduce building mass and perceived height

LOCATION AND ARTICULATION OF ENTRY

Building shall clearly delineate separate entries for separate uses

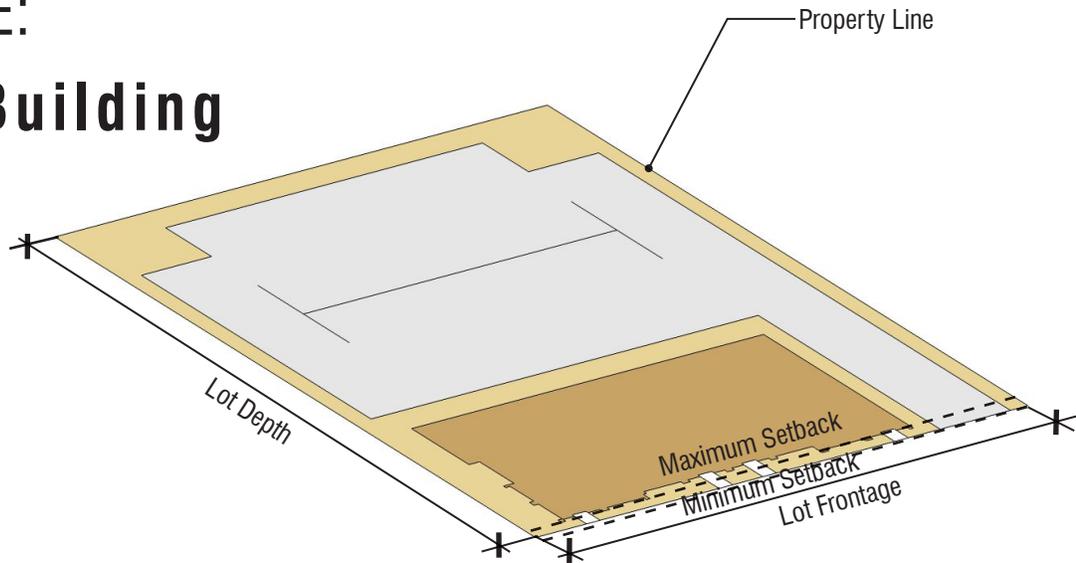
ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

For residential uses: Porches, stoops, stairs, terraces, bay windows, balconies, awnings, signage, lighting, forecourt

ENCROACHMENTS IN PUBLIC RIGHT OF WAY

Gallery, awnings signage, lighting, sidewalk dining, displays

**LOT TYPE:
Large Building**



LOT STANDARDS

Lot Frontage (feet)	150 minimum / no maximum
Lot Depth (feet)	Not applicable
Lot Area (square feet)	Not applicable

BUILDING PLACEMENT ON LOT

Front Yard Setback for primary building (feet)	0 minimum / 2 maximum; 20% of façade may be set back 12 feet
Side Yard Setback (feet)	0
Rear Yard Setback (feet)	0
Side-Street Yard (feet on corner lots)	0
Frontage Occupancy (%)	50% minimum

PARKING PLACEMENT ON LOT

Front Parking Setback from primary façade (feet)	6 minimum / no maximum
Side and Rear Parking Setback (feet)	0; parking lots may be shared with adjoining buildings

SUBAREAS

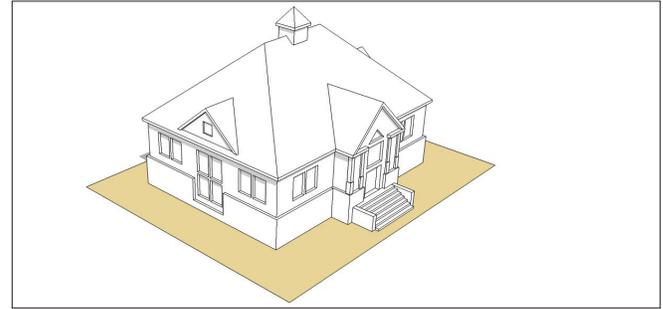
Allowed in Subarea(s)	WC-1-GL, WC-1, WC-2, WC-12
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ADDITIONAL REGULATIONS

Other	Streets, Landscaping, Lighting, Signage, Stormwater
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2.4.2 Buildings and Lots

BUILDING TYPE: Civic Building



DESCRIPTION A free-standing building that accommodates civic uses in Neighborhood or Village Center blocks. The building should be sited so as to give a sense of prominence in relation to the surrounding areas.



Character Examples

ARCHITECTURAL STANDARDS

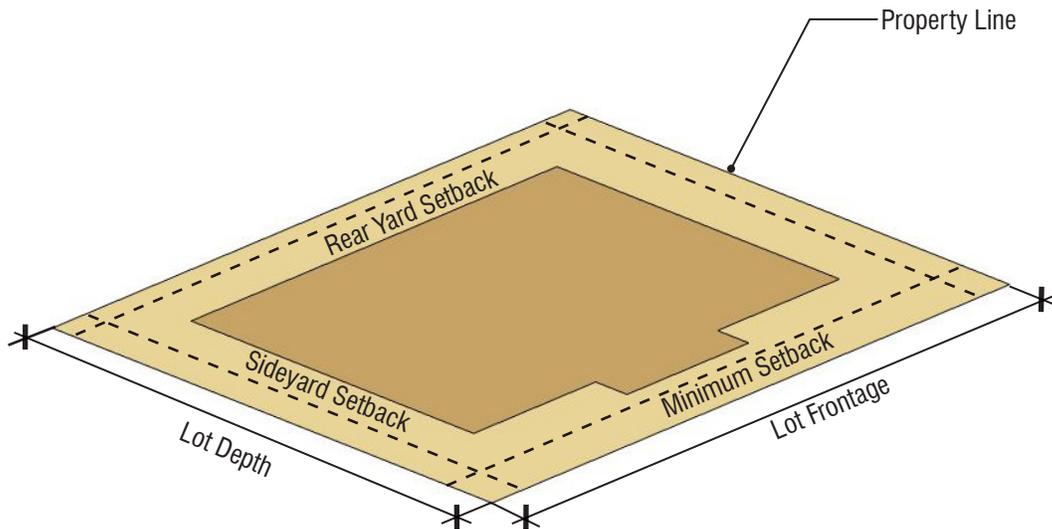
PLACEMENT OF SPECIFIC USES		TRANSPARENCY AND ARTICULATION	
GROUND FLOOR LIMITATIONS	Civic	Ground Floor Transparency (% of façade)	Not applicable
BUILDING HEIGHT		Upper Floor Transparency (% of façade)	Not applicable
Primary Building Height (maximum)	50 maximum in Subareas WC-1-GL, WC-1, WC-2 and WC-12; 35 maximum in all other Subareas where allowable	Street Frontage Wall Length Without Offset (feet)	Not applicable
Ground Floor Height (feet)	12 minimum	Street Frontage Wall Offset – length/depth (feet)	Not applicable
Upper Floor Height (feet)	9 minimum	BUILDING ENTRANCES	
Finished Floor Elevation (above grade at front façade)	Not applicable	Street Facing Entry Required	Yes
Main Roof Pitch	Not applicable	Entrance Spacing (maximum feet)	Not applicable
Flat Roofs Permitted	Yes		

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION	PLACEMENT OF WINDOWS AND DOORS
Building shall be oriented to the street with pedestrian access and entry. Building shall clearly delineate primary entrance	None
RELATIONSHIP TO STREET	ROOF FORM
Primary building façade shall be oriented to address the street	Roof style and pitch may vary
TREATMENT OF GROUND FLOOR	ALLOWABLE SIGNAGE TYPES
None	Address signs, directional, site, building-attached
SCALE AND PROPORTION	ALLOWABLE LIGHTING TYPES
Proportion and configuration of building elements shall be used to emphasize the importance of civic and cultural uses	Building lighting, site lighting, parking lighting and pedestrian lighting
VERTICAL AND HORIZONTAL EMPHASIS	ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)
None	Porches, stoops, stairs, awnings, signage, lighting, forecourt, balcony, terrace, gallery
LOCATION AND ARTICULATION OF ENTRY	ENCROACHMENTS IN PUBLIC RIGHT OF WAY
Main entry shall be clearly articulated and be part of a primary building feature	None

2.4.2 Buildings and Lots

LOT TYPE: Civic Building



LOT STANDARDS

Lot Frontage (feet)	Not applicable
Lot Depth (feet)	Not applicable
Lot Area (square feet)	Not applicable

BUILDING PLACEMENT ON LOT

Front Yard Setback (feet)	10 minimum / no maximum
Side Yard Setback (feet)	10 minimum / no maximum
Rear Yard Setback (feet)	5 minimum / no maximum
Side-Street Yard (feet on corner lots)	-
Frontage Occupancy (%)	-

PARKING PLACEMENT ON LOT

Front Parking Setback from primary façade (feet)	6 minimum / no maximum
Side and Rear Parking Setback (feet)	0; parking lots may be shared with adjoining buildings

SUBAREAS

Allowed in Subarea(s)	WC-1-GL, WC-1, WC-2, WC-3, WC-6, WC-8, WC-9, WC-10, WC-11, WC-12
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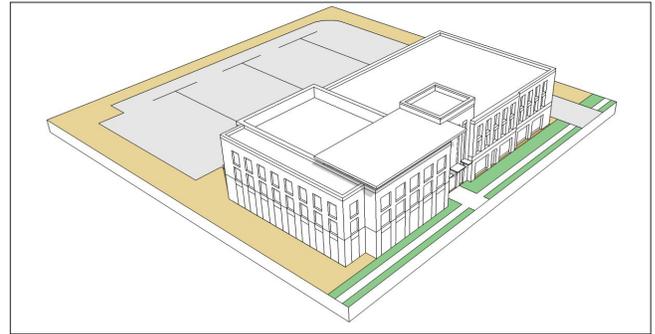
ADDITIONAL REGULATIONS

Other	Streets, Landscaping, Lighting, Signage, Stormwater
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2.4.2 Buildings and Lots

BUILDING TYPE: Dedicated Office

DESCRIPTION A large floor plate commercial building designed for office use. Ancillary uses such as a bank, cafeteria, restaurant, dry cleaner or retail shop may be added to the ground floor. Building design mitigates overall building scale and minimizes large blank walls and façades from the street. Site design integrates parking into well landscaped and screened areas.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

GROUND FLOOR LIMITATIONS	Commercial
BUILDING HEIGHT	
Primary Building Height (maximum)	50 maximum
Ground Floor Height (feet)	Not applicable
Upper Floor Height (feet)	Not applicable
Finished Floor Elevation (above grade at front façade)	Not applicable
Main Roof Pitch	8:12 maximum
Flat Roofs Permitted	Yes

TRANSPARENCY AND ARTICULATION

Ground Floor Transparency (% of façade)	30% minimum
Upper Floor Transparency (% of façade)	20% minimum
Street Frontage Wall Length Without Offset (feet)	75
Street Frontage Wall Offset – length/depth (feet)	10/5

BUILDING ENTRANCES

Street Facing Entry Required	Yes
Entrance Spacing (maximum feet)	100

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street and configured to provide active entries and lobbies at the street frontage

RELATIONSHIP TO STREET

Primary building façade and building entry shall be oriented to address the street

TREATMENT OF GROUND FLOOR

Ground floor shall be articulated with a varied material to anchor the structure

SCALE AND PROPORTION

Proportion and configuration of building elements shall be used to reduce the overall scale of building

VERTICAL AND HORIZONTAL EMPHASIS

Horizontal building elements, material changes, and articulation of façades shall be used to reduce building mass and perceived height

LOCATION AND ARTICULATION OF ENTRY

Building entry shall be oriented to the principal street

PLACEMENT OF WINDOWS AND DOORS

Windows shall be placed in a regular pattern that is coordinated to reduce the scale of larger building masses

ROOF FORM

Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs, directional, site, building-attached, and building-window

ALLOWABLE LIGHTING TYPES

Building lighting, site lighting, parking lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

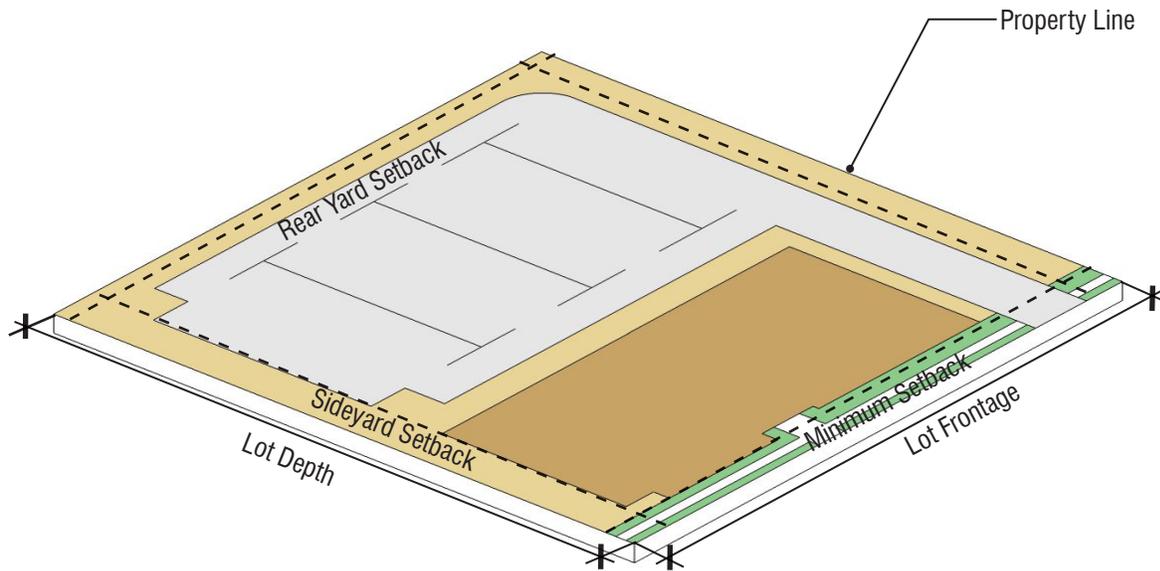
Forecourt, balcony, terrace

ENCROACHMENTS IN PUBLIC RIGHT OF WAY

Gallery, awnings, signage, lighting

2.4.2 Buildings and Lots

LOT TYPE: Dedicated Office



LOT STANDARDS

Lot Frontage (feet)	50 minimum / 350 maximum
Lot Depth (feet)	Not applicable
Lot Area (square feet)	Not applicable

BUILDING PLACEMENT ON LOT

Front Yard Setback (feet)	10 minimum / no maximum
Side Yard Setback (feet)	10 minimum / no maximum
Rear Yard Setback (feet)	5 minimum / no maximum
Side-Street Yard (feet on corner lots)	10 minimum / no maximum
Frontage Occupancy (%)	40% minimum

PARKING PLACEMENT ON LOT

Front Parking Setback from primary façade (feet)	6 minimum / no maximum
Side and Rear Parking Setback (feet)	0; parking lots may be shared with adjoining buildings

SUBAREAS

Allowed in Subarea(s)	WC-2, WC-12
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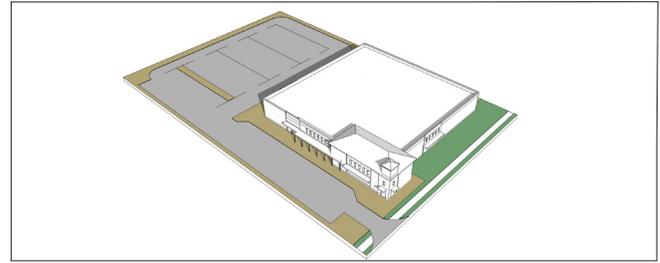
ADDITIONAL REGULATIONS

Other	Streets, Landscaping, Lighting, Signage, Stormwater
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2.4.2 Buildings and Lots

BUILDING TYPE: Large Format Retail

DESCRIPTION Large footprint retail building designed to provide interior open retail space. Building design mitigates overall building scale and minimizes large blank walls and façades from the street. Site design integrates parking into well landscaped and screened areas. This type may include other uses as a mixed-use building.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

GROUND FLOOR LIMITATIONS	Retail
BUILDING HEIGHT	
Primary Building Height (maximum)	50 maximum
Ground Floor Height (feet)	16 minimum
Upper Floor Height (feet)	Not applicable
Finished Floor Elevation (above grade at front façade)	Not applicable
Main Roof Pitch	8:12 maximum
Flat Roofs Permitted	Yes

TRANSPARENCY AND ARTICULATION

Ground Floor Transparency (% of façade)	30% minimum
Upper Floor Transparency (% of façade)	Not applicable
Street Frontage Wall Length Without Offset (feet)	75
Street Frontage Wall Offset – length/depth (feet)	10/5

BUILDING ENTRANCES

Street Facing Entry Required	Yes
Entrance Spacing (maximum feet)	100

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street and configured to provide active entries and lobbies at the street frontage

RELATIONSHIP TO STREET

Primary building façade shall be oriented to address the street and frame and contribute to street activity

TREATMENT OF GROUND FLOOR

Ground floor shall be articulated with a varied material to anchor the structure

SCALE AND PROPORTION

Architectural features and treatments shall be used to reduce the overall scale and mass of the building. Façades visible from a street should be articulated to reduce the mass of the building. Ground floor façades that face a public street shall have galleries, arcades, display windows, entry areas, and/or awnings, or other such features along no less than 60% of their horizontal length

VERTICAL AND HORIZONTAL EMPHASIS

Vertical building elements shall be used to reduce horizontal building masses

LOCATION AND ARTICULATION OF ENTRY

Building entry shall be oriented to the principal street

PLACEMENT OF WINDOWS AND DOORS

Windows shall be used to provide a regular rhythm on façades and to orient the structure to a pedestrian scale. No uninterrupted façade of more than 50 feet shall occur

ROOF FORM

Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs, directional, site, building-attached

ALLOWABLE LIGHTING TYPES

Site lighting, parking lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

Porch, gallery, terrace, balcony, forecourt

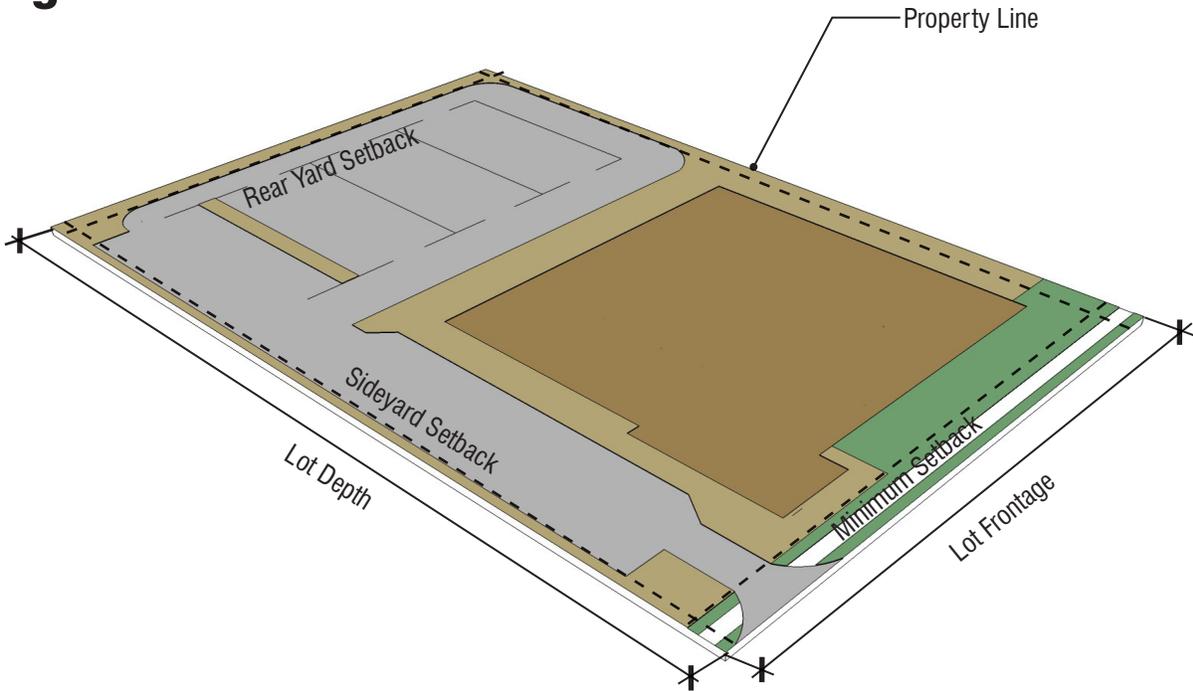
ENCROACHMENTS IN PUBLIC RIGHT OF WAY

Gallery, awnings signage, lighting

2.4.2 Buildings and Lots

LOT TYPE:

Large Format Retail



LOT STANDARDS

Lot Frontage (feet)	150 minimum
Lot Depth (feet)	150 minimum
Lot Area (square feet)	20,000 minimum

BUILDING PLACEMENT ON LOT

Front Yard Setback (feet)	10 minimum / no maximum
Side Yard Setback (feet)	10 minimum / no maximum
Rear Yard Setback (feet)	5 minimum
Side-Street Yard (feet on corner lots)	20 minimum
Frontage Occupancy (%)	30% minimum

PARKING PLACEMENT ON LOT

Front Parking Setback from primary façade (feet)	10' from face of building
Side and Rear Parking Setback (feet)	0; parking lots may be shared with adjoining buildings

SUBAREAS

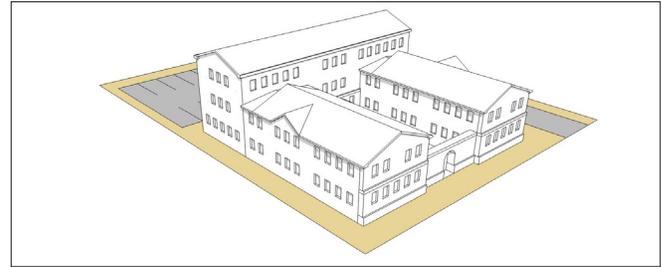
Allowed in Subarea(s)	WC-1-GL, WC-1, WC-2, WC-12
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ADDITIONAL REGULATIONS

Other	Streets, Landscaping, Lighting, Signage, Stormwater
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2.4.2 Buildings and Lots

BUILDING TYPE: Institutional Building



DESCRIPTION Institutional building for assisted living, nursing home, elderly housing and hospital. Building design should be compatible with the intent of the Subarea and should minimize large blank walls and façades from the street. Site design integrates parking into well landscaped and screened areas. In WC-12 only, Institutional Lots may be combined to accommodate a hospital use as allowed by the Allowable Densities Table.



Character Examples

ARCHITECTURAL STANDARDS

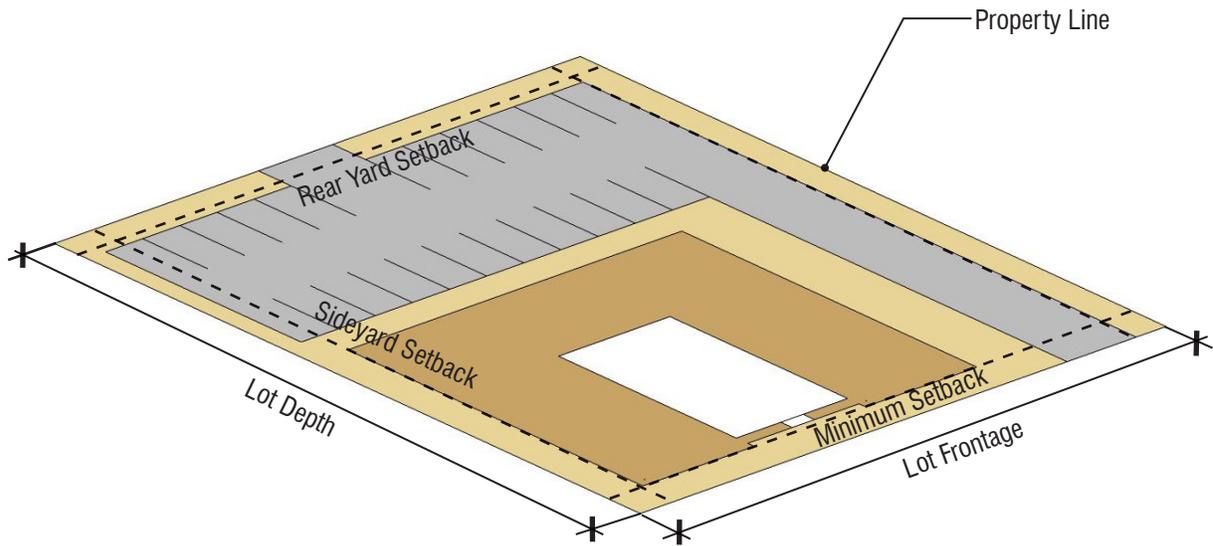
PLACEMENT OF SPECIFIC USES		TRANSPARENCY AND ARTICULATION	
GROUND FLOOR LIMITATIONS	None	Ground Floor Transparency (% of façade)	None
BUILDING HEIGHT		Upper Floor Transparency (% of façade)	None
Primary Building Height (maximum)	35 maximum in WC-4, WC-6 and WC-8 / 50 maximum in WC-1, WC-2, WC-12	Street Frontage Wall Length Without Offset (feet)	60
Ground Floor Height (feet)	-	Street Frontage Wall Offset – length/depth (feet)	10/5
Upper Floor Height (feet)	-	BUILDING ENTRANCES	
Finished Floor Elevation (above grade at front façade)	-	Street Facing Entry Required	Yes
Main Roof Pitch	-	Entrance Spacing (maximum feet)	80
Flat Roofs Permitted	Yes		

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION	PLACEMENT OF WINDOWS AND DOORS
Building shall be oriented to the street and configured to provide active entries and lobbies at the street frontage	Windows shall be placed in a regular pattern that is coordinated to reduce the scale of larger building masses
RELATIONSHIP TO STREET	ROOF FORM
Primary building façade and building entry shall be oriented to address the street	Roof style and pitch may vary
TREATMENT OF GROUND FLOOR	ALLOWABLE SIGNAGE TYPES
Ground floor shall be articulated with a varied material to anchor the structure	Address signs, directional, site, building-attached, and building-window
SCALE AND PROPORTION	ALLOWABLE LIGHTING TYPES
Proportion and configuration of building elements shall be used to reduce the overall scale of building	Building lighting, site lighting, parking lighting and pedestrian lighting
VERTICAL AND HORIZONTAL EMPHASIS	ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)
Horizontal building elements, material changes, and articulation of façades shall be used to reduce building mass and perceived height	Porch, gallery, terrace, balcony, forecourt, stoop
LOCATION AND ARTICULATION OF ENTRY	ENCROACHMENTS IN PUBLIC RIGHT OF WAY
Building entry shall be oriented to the principal street	Gallery, awnings, signage, lighting

2.4.2 Buildings and Lots

LOT TYPE: Institutional Building



LOT STANDARDS

Lot Frontage (feet)	100 maximum
Lot Depth (feet)	300 maximum
Lot Area (square feet)	5,000 minimum

BUILDING PLACEMENT ON LOT

Front Yard Setback (feet)	10 minimum
Side Yard Setback (feet)	10 minimum
Rear Yard Setback (feet)	5 minimum
Side-Street Yard (feet on corner lots)	-
Frontage Occupancy (%)	-

PARKING PLACEMENT ON LOT

Front Parking Setback from primary façade (feet)	20 minimum
Side and Rear Parking Setback (feet)	0; parking lots may be shared with adjoining buildings

SUBAREAS

Allowed in Subarea(s)	WC-1, WC-2, WC-6, WC-8, WC-10, WC-12
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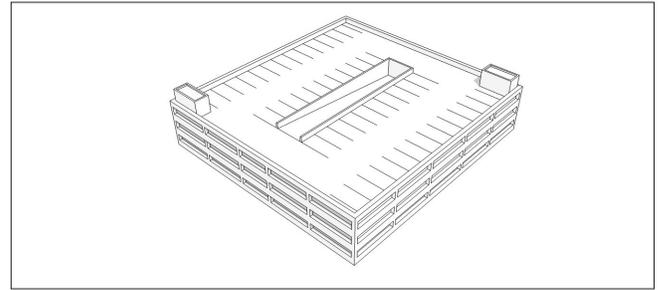
ADDITIONAL REGULATIONS

Other	Streets, Landscaping, Lighting, Signage, Stormwater
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2.4.2 Buildings and Lots

BUILDING TYPE: Parking Structure

DESCRIPTION Parking structures provide off-site parking within the block and can be combined with Village Center Blocks and Small, Medium and Large Building Types connected to the Parking Structure to create integrated development. Buildings should have frontage on the primary street, screening the Parking Structure. Screening on secondary streets can also be accomplished with landscaping.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

GROUND FLOOR LIMITATIONS	None
BUILDING HEIGHT	
Primary Building Height (maximum)	50 maximum
Ground Floor Height (feet)	Not applicable
Upper Floor Height (feet)	-
Finished Floor Elevation (above grade at front façade)	-
Main Roof Pitch	-
Flat Roofs Permitted	Yes

TRANSPARENCY AND ARTICULATION

Ground Floor Transparency (% of façade)	None
Upper Floor Transparency (% of façade)	None
Street Frontage Wall Length Without Offset (feet)	Not applicable
Street Frontage Wall Offset – length/depth (feet)	Not applicable

BUILDING ENTRANCES

Street Facing Entry Required	No
Entrance Spacing (maximum feet)	Not applicable

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Parking structure shall be placed at the interior of blocks or shall be articulated to integrate with surrounding façades

RELATIONSHIP TO STREET

Parking structure shall provide convenient vehicle and pedestrian access to the street

TREATMENT OF GROUND FLOOR

The ground floor shall be articulated to integrate with surrounding façades

SCALE AND PROPORTION

Architectural elements shall be used to reduce the overall scale and massing of the parking structure

VERTICAL AND HORIZONTAL EMPHASIS

Horizontal building elements, material changes, and articulation of façades shall be used to reduce building mass and perceived height

LOCATION AND ARTICULATION OF ENTRY

Main pedestrian and vehicle entries shall be separated, oriented to the street, and clearly marked

PLACEMENT OF WINDOWS AND DOORS

None

ROOF FORM

Not applicable

ALLOWABLE SIGNAGE TYPES

Address signs, directional, site, building-attached

ALLOWABLE LIGHTING TYPES

Site lighting, parking lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

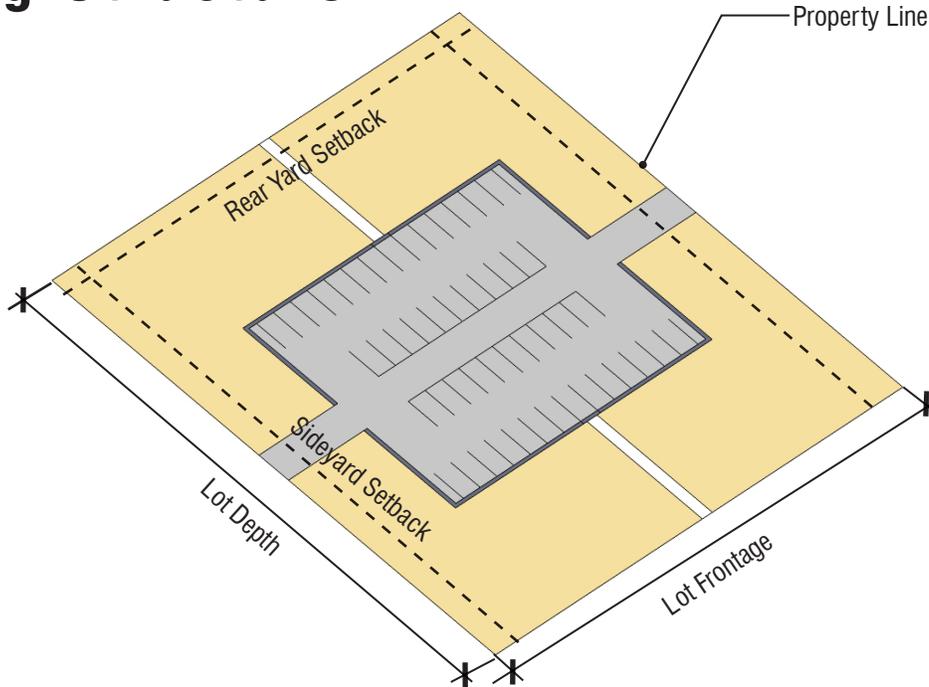
None

ENCROACHMENTS IN PUBLIC RIGHT OF WAY

None

2.4.2 Buildings and Lots

LOT TYPE: Parking Structure



LOT STANDARDS

Lot Frontage (feet)	Not applicable
Lot Depth (feet)	Not applicable
Lot Area (square feet)	Not applicable

BUILDING PLACEMENT ON LOT

Front Yard Setback (feet)	Not applicable, internal to lot
Side Yard Setback (feet)	10 minimum
Rear Yard Setback (feet)	5 minimum
Side-Street Yard (feet on corner lots)	-
Frontage Occupancy (%)	-

PARKING PLACEMENT ON LOT

Front Parking Setback from primary façade (feet)	Not applicable
Side and Rear Parking Setback (feet)	Not applicable

SUBAREAS

Allowed in Subarea(s)	WC-1-GL, WC-1, WC-2, WC-12
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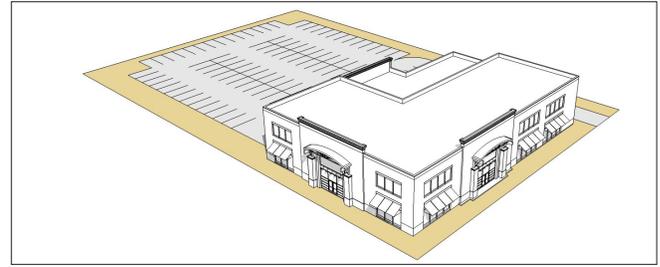
ADDITIONAL REGULATIONS

Other	Streets, Landscaping, Lighting, Signage, Stormwater
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2.4.2 Buildings and Lots

BUILDING TYPE: Flex

DESCRIPTION A large footprint commercial use, mixed-use or Flex Use building and lot that integrate flexible and potentially large scale building space into a landscaped lot with parking. Lots may be combined to form larger lots that may share building or parking uses.



Character Examples

ARCHITECTURAL STANDARDS

PLACEMENT OF SPECIFIC USES

GROUND FLOOR LIMITATIONS Commercial, Light Industrial

BUILDING HEIGHT

Primary Building Height (maximum)	50 maximum
Ground Floor Height (feet)	Not applicable
Upper Floor Height (feet)	Not applicable
Finished Floor Elevation (above grade at front façade)	Not applicable
Main Roof Pitch	8:12 maximum
Flat Roofs Permitted	Yes

TRANSPARENCY AND ARTICULATION

Ground Floor Transparency (% of façade)	30% minimum
Upper Floor Transparency (% of façade)	Not applicable
Street Frontage Wall Length Without Offset (feet)	100
Street Frontage Wall Offset – length/depth (feet)	20/5 minimum

BUILDING ENTRANCES

Street Facing Entry Required	Yes
Entrance Spacing (maximum feet)	150

ARCHITECTURAL GUIDELINES

BUILDING DISPOSITION AND CONFIGURATION

Building shall be oriented to the street and configured to provide active entries and lobbies at the street frontage

RELATIONSHIP TO STREET

Primary building façade shall be oriented to address the street and frame and contribute to street activity

TREATMENT OF GROUND FLOOR

Ground floor shall be articulated with a varied material to anchor the structure

SCALE AND PROPORTION

Architectural features and treatments shall be used to reduce the overall scale and mass of the building. Façades visible from a street should be articulated to reduce the mass of the building. Ground floor façades that face a public street shall have galleries, arcades, display windows, entry areas, and/or awnings, or other such features along no less than 60% of their horizontal length

VERTICAL AND HORIZONTAL EMPHASIS

Vertical building elements shall be used to reduce horizontal building masses

LOCATION AND ARTICULATION OF ENTRY

Building entry shall be oriented to the principal street

PLACEMENT OF WINDOWS AND DOORS

Windows shall be used to provide a regular rhythm on façades and to orient the structure to a pedestrian scale. No uninterrupted façade of more than 50' shall occur

ROOF FORM

Roof style and pitch may vary

ALLOWABLE SIGNAGE TYPES

Address signs, directional, site, building-attached

ALLOWABLE LIGHTING TYPES

Site lighting, parking lighting and pedestrian lighting

ENCROACHMENTS INTO FRONT SETBACK AND SIDEYARDS FACING THE STREET (SECTION 2.4.3)

Gallery, awnings signage, lighting, terrace, balcony, forecourt

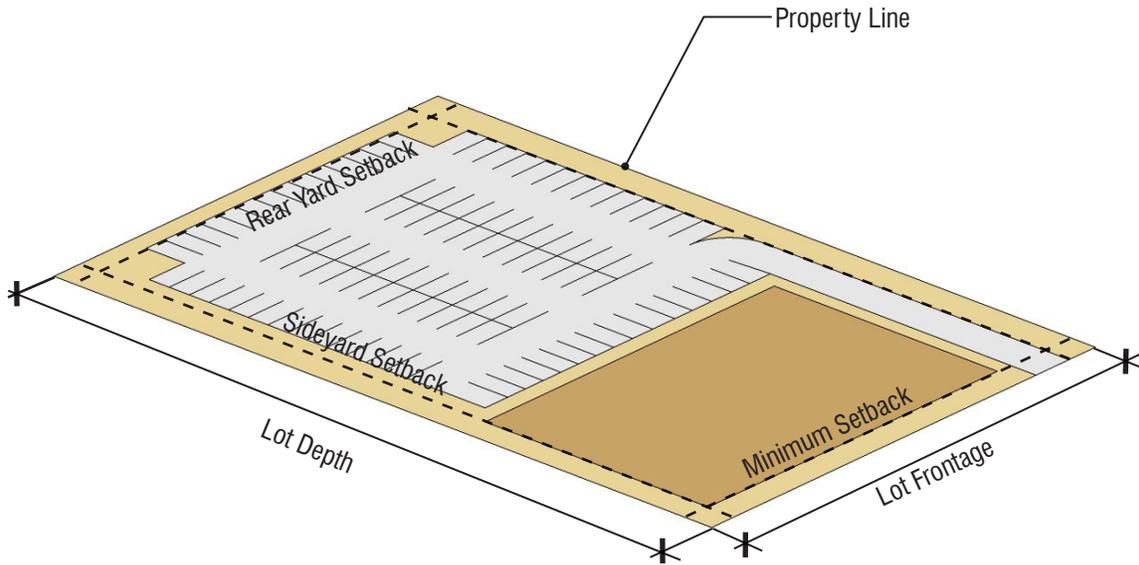
ENCROACHMENTS IN PUBLIC RIGHT OF WAY

None

2.4.2 Buildings and Lots

LOT TYPE:

Flex



LOT STANDARDS

Lot Frontage (feet)	150 minimum
Lot Depth (feet)	150 minimum
Lot Area (square feet)	20,000 minimum

BUILDING PLACEMENT ON LOT

Front Yard Setback (feet)	10 minimum / no maximum
Side Yard Setback (feet)	10 minimum / no maximum
Rear Yard Setback (feet)	5 minimum
Side-Street Yard (feet on corner lots)	20 minimum
Frontage Occupancy (%)	30% minimum

PARKING PLACEMENT ON LOT

Front Parking Setback from primary façade (feet)	10' from face of building
Side and Rear Parking Setback (feet)	0; parking lots may be shared with adjoining buildings

SUBAREAS

Allowed in Subarea(s)	WC-1-GL, WC-1 WC-2, WC-12
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ADDITIONAL REGULATIONS

Other	Streets, Landscaping, Lighting, Signage, Stormwater
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2.4.3 Encroachment and Shared Use Agreements

Encroachments

The building façade must be located at or behind the setback within build-to zone as required by the Lot Types. Some building elements are permitted to encroach into the setback and into the front yard and side yards facing streets. Other building element encroachments are permitted within the public or private right-of-way.

- A. Encroachments into front setbacks and side setbacks facing streets – The following building elements are permitted to encroach into the setback but not beyond the lot line into the right-of-way where permitted by a specific Building Type:

1. Porches	No restrictions
2. Stoops	No restrictions
3. Stairs or ramps and handrails	No restrictions
4. Awnings and canopies	No restrictions with a vertical clearance of seven (7) feet minimum
5. Porte Cocheres	No restrictions
6. Terraces	Eight (8) feet maximum
7. Bay windows	Five (5) feet maximum
8. Balconies	Five (5) feet maximum
9. Signage	Six (6) feet maximum
10. Lighting	Three (3) feet maximum, unless lighting is for an allowed sign under 9. Signage directly above.
11. Driveways	For restrictions, see <i>Section 2.3.3 Transportation Network</i>
12. Drop-offs	Allowable for the following uses: Dedicated Office Buildings, Hotels, Civic and Institutional

- B. Encroachments into right-of-way – The following building elements are permitted to encroach into the right-of-way where permitted by a specific Building Type:

1. Awnings and canopies	Eight (8) feet maximum with a vertical clearance of seven (7) feet minimum
2. Galleries	Eight (8) feet maximum with a vertical clearance of seven (7) feet minimum, two (2) feet minimum behind the curb, private streets only
3. Signage	Six (6) feet maximum

2.4.3 Encroachments and Shared Use Agreements

- | | |
|--------------|---|
| 4. Lighting | Three (3) feet maximum, unless lighting is for an allowed sign under 3. Signage directly above. |
| 5. Driveways | For restrictions, see <i>Section 2.3.3 Transportation Network</i> |

C. Other encroachments – Other encroachments into front and side yards, where permitted by the building code, to include eaves, downspouts, gutters and other similar building elements.

Encroachment and Shared Use Agreements

Shared Use and Encroachment Agreements are allowed in order to minimize land disturbance and the impact to street sections, to encourage joint trenching agreements, and to maximize the beneficial use of easements for Shared Use Paths or Recreation Trails. Such agreements are allowed with approval from the Planning Board during the PUD Subdivision and/or PUD Site Plan submittal process.

Any encroachment into a Town of Londonderry right-of-way or easement requires an encroachment and maintenance agreement with the Town before such encroachment is made.

2.4.4 PUD Site Plan Signage

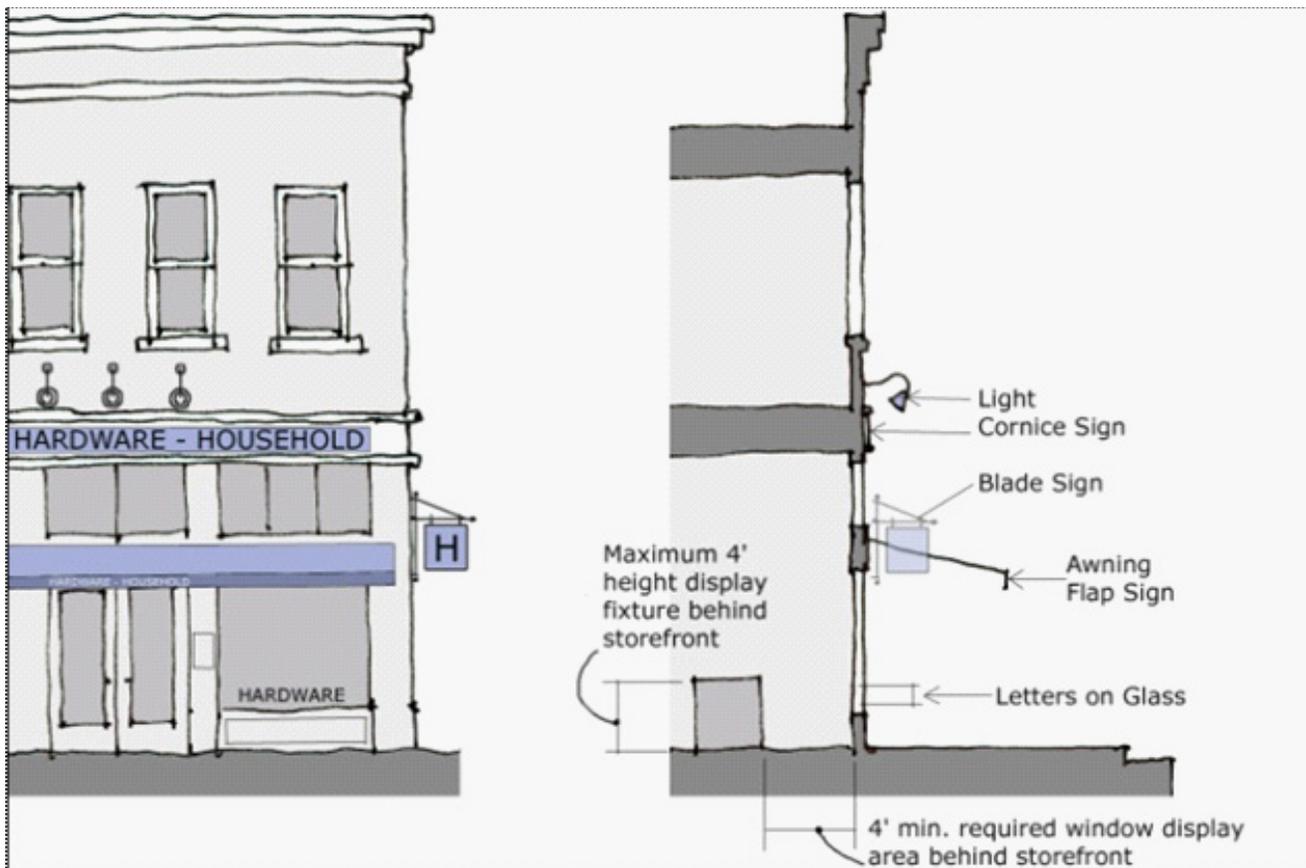
The following parameters pertain to the installation, use, maintenance, and lighting standards of signs for PUD Site Plan submittals or signage submittals within and relating to business and mixed-use areas within the Subareas. Signage for residential uses will be subject to *Section 3.11* of the *Londonderry Zoning Ordinance*, unless such Section conflicts with the PUD Master Plan, in which case the PUD Master Plan provisions shall prevail.

Permitted PUD Site Plan Signs

- A. Without limitation of the later provisions of this Section, the following signs are allowable at the PUD Site Plan level:
1. Construction signs;
 2. Parking signs;
 3. Portable valet signs;
 4. Rear entry sign except along alleys;
 5. Flags;
 6. Temporary portable signs in the public right-of-way;
 7. Temporary sign advertising nonresidential property;
 8. Temporary special event signs;
 9. Wall signs;
 10. Blade signs;
 11. Canopy and awning signs;
 12. Pole mounted & directory signs;
 13. A-frame signs;
 14. Window signs;
 15. Address signs;
 16. Building signs;
 17. Monument signs;
 18. Churches, schools, community centers, and health care facilities, buildings housing government functions, and municipal, county and state utilities signs;
 19. Multi-family residential signs;
 20. Temporary signs for churches and private schools;
 21. Traffic/direction signs on grounds of public, charitable or religious institutions;
 22. Signs not requiring a permit in accordance with *Section 3.11.5* of the *Londonderry Zoning Ordinance*.
- B. Approval Authority – The Zoning Administrator shall review all signage requiring a permit for conformity to this PUD Master Plan.

2.4.4 PUD Site Plan Signage

- C. Appeals – The petitioner shall be entitled to appeal staff denials to the Planning Board.
- D. Measurement and calculation of area – The calculation shall be in accordance with *Section 3.11.5* of the *Londonderry Zoning Ordinance* except that sign height may exceed ten (10) feet as indicated below.
- E. Submittal Requirements – All signage applications shall include the following:
1. Written approval from the Developer for proposed signage;
 2. If the applicant is not the owner of the property on which the sign(s) is to be erected, written permission from the owner or authorized representative stating that the sign(s) may be erected on the parcel;
 3. The street name and street number of the building to which the sign(s) is to be erected;
 4. A site or plat plan of the property showing accurate placement of the permanent sign(s);
 5. Front, side and top elevations with sign(s) drawn to scale;
 6. Color and material descriptions;
 7. Illumination method for the sign(s) (if any);
 8. Landscaping for the sign (if any).



Example of building signage locations

2.4.4 PUD Site Plan Signage

General Sign Guidelines

All exterior business signs located within the PUD shall conform to the following guidelines:

- A. Signs are used to
 1. Establish identities of tenants;
 2. Contribute to a lively visual atmosphere;
 3. Give clear functional information.
- B. Graphic signs should be simple and clear and reinforce the character of the subject business and overall character of the PUD;
- C. Materials, colors, placement and shape must be clearly differentiated from public safety, traffic and regulatory signage;
- D. Repetitive use of words or symbols as a sign element is not permitted except if part of the business name or except for a single band of letters inside a glass storefront;
- E. No sign, except halo where allowed, may be internally lit, except in Subareas WC-1, WC-2, WC-1-GL, WC-8, WC-10 and WC-12;
- F. No sign or related outdoor lighting fixture shall be so placed as to focus light directly into any oncoming traffic or any street;
- G. All illuminated detached signs shall be so illuminated by an underground electrical service;
- H. Animated, moving, flashing and noise making signs are prohibited;
- I. Changeable electronic message board signs are prohibited;
- J. Signs painted directly on the building exterior walls shall not be permitted;
- K. Iridescent materials or signs, flashing lights, and Day-Glo colors are not permitted;
- L. Service doors to tenant spaces will have only standard identification, i.e., tenant's name address and number, hours of delivery and operation;
- M. Sign maintenance is the sole responsibility of the sign owner. Signs must be kept in good repair;
- N. Signs must also be maintained in a safe condition and pose no safety hazard and must meet all electrical and structural codes;
- O. Signage may not extend beyond the roof line, except in the WC-1-GL, WC-1, WC-2 and WC-12 Sub-areas.
- P. Signs must be posted no more than thirty (30) days before the event and remain in place no more than forty-five (45) days total.

2.4.4 PUD Site Plan Signage

Design, Construction and Maintenance

- A. Signs or their supports shall not be placed in such position or manner as to obstruct or interfere, either physically or visually, with any fire alarm, police alarm, traffic signal or sign, or any devices maintained by or under public authority or with vehicular or pedestrian ingress or egress to or from any public or private right-of-way, roadway, driveway, or sidewalk.
- B. All signs shall be constructed and secured so as to ensure the safety of pedestrians.
- C. The material and construction of any sign shall be in accordance with the requirements above and the Building Code.
- D. Permanent signs shall be made of durable materials.
- E. All signs shall be maintained in a safe condition and, together with their structural elements, shall be kept in good repair to the satisfaction of the Zoning Administrator.

Business Sign Design Standards

The following signs shall be permitted for all businesses within the Woodmont Commons PUD.

- A. Wall Signage – Wall Signs may be permanently installed parallel to the store front for ground floor businesses as follows:
 1. Wall signs should align with major architectural elements, such as doors and windows. Ornamental elements, such as moldings, pilasters, arches, clerestory windows, roof eaves, or cornice lines should be used as a frame;
 2. All businesses are permitted one Wall Sign on the façade, unless the building is on a corner lot, in which case one sign on each street is allowed;
 3. Wall signs should not extend above the cornice line or into or above roof areas, unless they function as an integral part of the façade or roof design or are a faithful accent to existing architectural details or forms;
 4. Businesses shall be permitted one (1) square-foot of signage per one (1) lineal-foot of street frontage, except in the WC-1-GL, WC-2 and WC-12 Subareas where the formula is twice that;



Examples of wall sign (halo above; band below)



2.4.4 PUD Site Plan Signage

5. Letters may also be printed or etched on the same surface as the background, which is then affixed to the wall;
 6. Band signs may be illuminated with halo lighting or other indirect light sources and may be internally illuminated in Subareas listed above.
- B. Blade Signs – Blade signs may be permanently installed perpendicular to the store front for ground floor and upper level businesses as follows:
1. Blade signs shall be designed to complement the business activities or the adjacent building;
 2. One blade sign shall be permitted on each façade having frontage. No more than two blade signs shall be permitted on any individual business;
 3. Blade signs shall only display the business name, business type or logo. Third party advertising is not permitted;
 4. Ground level blade signs shall not exceed twelve (12) square feet per side;
 5. Upper level blade signs shall not exceed six (6) square feet per side;
 6. All signs shall clear eight (8) feet above the adjacent sidewalk;
 7. Blade signs shall not project vertically above the roof line or parapet wall of a building;
 8. All blade signs shall be horizontally spaced fifteen (15) feet minimum from the nearest blade sign;
 9. Blade signs shall not project more than six (6) feet from a building wall;
 10. Blade signs may be illuminated with halo lighting or externally illuminated;
 11. Mounting hardware, such as supports and



Examples of blade signs

2.4.4 PUD Site Plan Signage

brackets, shall complement the design of the sign, the building or both.

C. Canopy and Awning Signs – Canopy design shall be in keeping with the design of the building.

1. Awnings shall be sloped with a vertical flap at the bottom. Rounded, curved or other shapes are not permitted;
2. Canopy signs shall not exceed two-and-a-half (2 1/2) feet in height and the combination of wall and canopy signs shall not exceed the maximum area permitted in Wall Signs above. Canopy signs may extend above the roof line of the canopy but shall not extend above the roof line of the building;
3. Awning signs are limited to two (2) surfaces and shall not exceed twenty (20) percent of the total awning area. Signs on awnings may be on the sloped portion or on the flap;
4. Awnings may have open 'sides' or 'ends';
5. Awnings may be either fixed or operable;
6. Color combinations for awning or canopy signs are limited to three colors and should be simple. Lettering color and background color should contrast for legibility. Subtle bands of color are appropriate for awnings; more complex patterns or textures should generally not be permitted;
7. Canopy signs may be illuminated with halo lighting;
8. Awnings shall not be internally lit but may be illuminated with indirect light sources;
9. Awnings shall be made of canvas or other coated fiber textiles similar in durability and appearance. Permanent awnings as architectural features are also permitted;
10. Plastic awnings are prohibited;
11. Signs may be attached, painted, or printed upon a canopy or awning.



Example of canopy sign



Examples of awning signs

2.4.4 PUD Site Plan Signage

D. Pole Mounted & Freestanding Directory Signs – Pole-mounted and/or other forms of freestanding signs shall be permitted in village centers as follows:

1. Eight (8) square feet maximum per side for pole-mounted signs and eighteen (18) square feet per side for directory signs, not including decorative stanchions;
2. Business logos are permitted;
3. Signs shall be limited to eight (8) feet height, not including decorative stanchions;
4. Project signs not are permitted within the public right-of-way;
5. Directory signs may be externally illuminated;
6. Pole signs shall be spaced at least twenty (20) feet apart;
7. All such signs shall be constructed and secured so as to ensure the safety of pedestrians;
8. Signs shall be constructed of durable materials that complement the design of the adjacent buildings;
9. Directory signs or kiosks for private arcades or buildings should be on private property, located in publicly accessible courts, accessways, or passages. Only one directory sign or kiosk is allowed per building entrance.



Example of pole mounted sign



Example of directory sign

2.4.4 PUD Site Plan Signage

- E. A-Frame Signs – A-Frame Signs in accordance with this Section shall be exempt from Temporary portable signs such as menu boards for restaurants, and special promotional signs for stores shall be permitted in the public right-of-way and between the building and street curb subject to the following:
1. Maximum area of one side shall be seven (7) square feet per side for a total of fourteen (14) square feet;
 2. Maximum sign height shall be forty-two (42) inches;
 3. Maximum letter height shall be eight (8) inches;
 4. One sign is permitted per business, unless the building is on a corner lot and has multiple entrances;
 5. Signs shall not be illuminated;
 6. Signs must be placed immediately in front of the business, within the width of store frontage and not, for example, at the street corner in front of other businesses. For businesses located in an arcade or plaza, a portable sign may be placed at the street entrance to the arcade or plaza;
 7. A-frame signs shall be spaced at least twenty (20) feet apart;
 8. The sign shall be at least one (1) foot from the street curb and placed so that it does not unduly retard pedestrian traffic flow;
 9. All such signs shall be constructed and secured so as to ensure the safety of pedestrians;
 10. Signs shall be maintained in good condition and removed each day prior to close of business;
 11. Signs shall be constructed of durable materials that complement the design of the adjacent building;



Example of A-frame sign

2.4.4 PUD Site Plan Signage

12. Signs shall be stored indoors after hours of operation.

F. Window Signs – Window signs shall be permitted in all business storefronts as follows:

1. Letters and logos shall be silk-screened or of a custom vinyl appliqué. Letters and logos shall be affixed to back of glass and up to six (6) inches maximum height;
2. One window sign per window is permitted;
3. One suspended sign, hung a minimum thirty-six (36) inches behind glass display window is permitted at each display window within the storefront. Flat signs, clear glass, and/or acrylic are encouraged. Maximum height for a horizontal sign is twelve (12) inches and maximum size for a logo object sign is eighteen (18) inches by eighteen (18) inches;
4. Window signs may list information including services and/or products sold on the premises, phone numbers, and operating hours;
5. Temporary Promotional signs are permitted in each business for up to ten (10) days per month and shall not exceed twenty-five (25) percent of the total storefront window area.

G. Address Signs – Address signs containing the numerical street address are permitted near the principal entrance of all businesses or near the mailbox if applicable.

1. Address sign numerals shall be of uniform size and may be between four (4) inches and six (6) inches in height;
2. Address signs shall be easily visible by using colors or materials that contrast with their background.

H. Construction Signs – Construction signs are permitted as follows:

1. One (1) sign not exceeding forty-eight (48)



Example of window sign



Example of address sign

2.4.4 PUD Site Plan Signage

square feet on each site providing construction information, including but not limited to the architect(s), owner(s) and/or contractor(s). Construction signs are permitted within the site or off-premises within 800 feet of the site of the ongoing construction. Any off-premises construction sign shall remain with the project. No more than four (4) off-premises signs shall be permitted for any single construction project (i.e. one building or housing subdivision). Construction signs are to be maintained during actual construction and promptly removed within seven (7) days after issuance of all certificates of occupancies.

Highway Corridor Signage

In Subareas WC-2 and WC-12, permitted monument and building signage shall be as follows:

- A. Building Signage – Building signage shall conform to the following standards:
 1. Total sign area shall be limited to two (2) square feet per one (1) linear foot of building length, but shall not exceed 350 total square feet per building;
 2. Building signs shall be independent letters pinned to the building elevation, which may be internally illuminated;
 3. Corporate logos and colors shall be permitted;
 4. Ground mounted signs are permitted;
 5. Affixed building signs shall not be taller, or extend higher, than the building on which they are mounted.



Example of building sign

2.4.4 PUD Site Plan Signage

- B. Monument Signage – The PUD shall be permitted a total of 8 freestanding monument identification features as follows:
1. Monument signs shall be located along the east side of I-93, the west side of I-93, along the median of the boulevards, or along Garden Lane, on property within or outside of the PUD;
 2. Each sign shall be limited to a maximum of 350 square feet per side or 700 square feet total, plus architectural allowances such as walls and columns;
 3. The architectural allowances of thirty-six (36) inches on the top of the sign and forty-eight (48) inches on each side of the sign shall not count toward the maximum allowed sign area, and may exceed the maximum height by thirty-six (36) inches. The architectural allowance shall not include any portion of text, logo, or other advertising material;
 4. Maximum sign maximum height shall be thirty-five (35) feet per sign as measured from the adjacent ground to the top of the sign;
 5. Signs shall be halo lit, externally or internally illuminated with cutout letters;
 6. All monument signs shall be spaced 200 feet minimum distance from each other;
 7. The ground area between the support base for monument signs shall be planted with appropriate landscaping materials with the intent of creating an aesthetically pleasing and visually compatible landscaped area around the sign.



Example of monument sign

2.4.5 PUD Site Plan Lighting

Description

These standards are designed to reduce the impacts of glare, light spill onto adjoining properties and unnecessary lighting and to promote safety, security and energy conservation.

Purpose

The purpose of these PUD Site Plan exterior lighting standards is to create a well-balanced, integrated lighting plan for private properties that enhances vehicular and pedestrian visibility while minimizing lighting glare and contrast. Exterior lighting is intended to light building architecture and site areas so as to accentuate design features and promote security in an attractive and understated manner that minimizes off-site impacts. Lighting should emphasize both public and private features and destinations by using a minimum amount of light to meet these objectives. It should provide needed illumination of the site and, at the same time, prevent undesired off-site glare.

Applicability

Outdoor lighting standards are to be addressed at the time of PUD Site Plan submittals. They apply to the installation of new outdoor lighting fixtures or the replacement of any existing outdoor light fixtures located within the public right-of-way for the Woodmont Commons PUD.

Standards

All outdoor lighting shall be subject to *Section 3.13 Outdoor Lighting Standards of the Londonderry Site Plan Regulations* except as amended herein:

- A. Internally lit signs are acceptable provided that they meet the requirements of *Section 2.4.4 PUD Site Plan Signage*;
- B. Significant public or civic buildings may be illuminated with up-lighting;

- C. Adjacent to residential property at the perimeter of the Woodmont Commons PUD, no bulb or lamp will be visible at the property line at ground level or above unless to illuminate a path or walkway;
- D. All non-essential lighting is required to be turned off after business hours except in mixed-use areas and for monument signs along I-93, leaving only the necessary lighting for security. In mixed-use areas, storefront display lighting, parking lot lighting and sign lighting may remain on since they contribute to the illumination of walkways and the safety and security of the residents. (“Non-essential” can apply to: display, aesthetic, parking and sign lighting).

Outdoor lighting used to illuminate flags, monuments, signs, public or civic buildings, landscapes and other features must use cut-off or directionally shielded light fixtures that are aimed and controlled so that the light is substantially confined to the object to be illuminated. Such lighting must be shielded to prevent glare for drivers or light trespass beyond the Woodmont Commons PUD Boundary.

Parking lot lighting shall be designed to cast light downward. Cut-off and shielded fixtures shall be used to reduce the impacts of glare on adjacent uses. The maximum mounting height of a lighting fixture above the ground shall be twenty (20) feet in residential areas and twenty-five (25) feet in mixed-use and nonresidential areas. In Subarea WC-1-GL and in Subarea WC-12 where the fixtures are located more than one hundred feet from the Woodmont Commons PUD Boundary maximum mounting height of a lighting fixture above the ground shall be thirty-five (35) feet. The maximum height of parking lot lights on the top level of parking decks shall be twenty (20) feet. Internal lights in open sided parking decks shall be full cut-off lighting fixtures.

Light sources should generally be metal halide or LED. Low wattage high-pressure sodium may be desirable in some residential settings where glare may be an issue.

Building lighting, other than decorative fixtures at entrances and at architectural features, and walkway lighting shall be full cut-off lighting fixtures.

2.4.6 PUD Site Plan Landscape

Description

PUD Site Plan Standards for landscaping are designed to create an attractive setting for the Woodmont Commons PUD.

Purpose

Landscape Standards enhance the quality of the environment, provide shade for pedestrians, reduce heat island effects, screen parking and utilities and soften building and structure edges.

Applicability

These landscape standards apply to landscaping for PUD Site Plans within private lots in the Woodmont Commons PUD.

Standards

PUD Site Plan landscaping shall be in accordance with the following:

- A. The Woodmont Commons PUD is designed as an integrated mixed-use walkable community. Screening shall not be required between uses, to soften the visual impact of building, or in any front setback within the boundary of the Woodmont Commons PUD;
- B. Preservation of existing vegetation may include but not be limited to areas where access or utilities are not required, Conserved Green Space and along major storm drainage features where access or utilities are not required. Vegetation may be removed to permit public access trails and structures;
- C. Screening along public rights-of-way within the Woodmont Commons PUD shall only be required to be three (3) feet in height at parking areas to permit increased visibility and security within the parking areas in front of or beside

buildings. Screening may be accomplished with Landscaping, walls or a combination thereof;

- D. No landscaped screening is required for parking lots within the interior of blocks and parking decks located behind buildings;
- E. Other parking lots may be landscaped in compliance with the following guidelines:
 1. Each parking lot shall provide landscaping within and/or around the parking lot at a minimum ratio of five percent (5%) of the gross area of the parking lot. Landscaping shall be evenly dispersed throughout each parking lot. Orchard-style planting (placement of trees in uniformly-spaced rows) is encouraged for larger parking areas;
 2. Internal parking lot landscaping as required above, shall contain one (1) deciduous shade tree for every fifteen (15) parking spaces. Trees shall be distributed throughout the parking lot as evenly as possible. Trees shall be set back at least five (5) feet minimum from the face of the curb. Tree placement and parking lot lighting shall not conflict;
 3. Perimeter shade trees shall not be required where buildings or street trees are present;
 4. Where perimeter shade trees are required, they shall be provided around the perimeter of parking areas at a minimum ratio of one (1) tree per fifty (50) feet of parking lot perimeter, unless existing trees disrupt this ratio;
 5. Parking lot screening is not required except as indicated above or where the parking lot abuts the Woodmont Commons PUD boundary.

Screening shall be required along the PUD Perimeter Buffer where non-residential uses or residential uses of higher density in the Woodmont Commons PUD back up to residential uses outside the Woodmont Commons PUD boundary. Existing landscaping,

2.4.6 PUD Site Plan Landscape

topography and other natural or cultural features may count towards the required screening. In other circumstances, where uses on both sides are the same or are compatible, then the landscape treatment, site improvements and architecture may be planned to complement each other and enhance views to, from and across the sites.

The ground mounted signage landscaping area shall be a minimum of four (4) foot wide area surrounding each monument or pedestal sign base. All sign bases shall be planted with shrubs and groundcover with a minimum height of eighteen (18) inches at time of planting.

2.4.7 PUD Site Plan Stormwater Management

Description

The PUD Site Plan Standards for stormwater are provided to allow the design and implementation of stormwater management systems consistent with the design principles of the Woodmont Commons PUD.

Purpose

These stormwater standards protect local watersheds and are consistent with low impact design. Compact development, such as the Woodmont Commons PUD, is beneficial to the management of stormwater runoff by reducing the amount of impervious surfaces.

Applicability

The PUD Master Plan will abide by *Sections 3.05 and 3.06* of the *Londonderry Site Plan Regulations* regarding the design and implementation of storm water management systems except that vegetative swales and rain gardens intended to assist in the treatment of storm water run-off will be allowed by right and subject to design review by staff.

Chloride Management

Beaver Brook has been declared an impacted watershed in need of corrective action due to Chloride loading, most notably resulting from winter maintenance activities. *Section 3.3 Chloride Management* discusses the mitigation measures for chloride management in this watershed.

2.5 ADMINISTRATION

2.5.1 Procedures

The PUD Master Plan serves as the land use regulations for the properties located within the PUD Boundary as outlined in *Section 2.1 Introduction*.

The process for administering the provisions of this PUD Master Plan is as follows:

- A. The Developer, or their assignee, shall be responsible for the coordination of the Land Use Plan and the amount of authorized development within the Woodmont Commons PUD. As such the Developer may be the developer of all or portions of the project, or shall be the first point of contact for all other applicants intending to develop within the PUD Master Plan.
- B. The review and approval of PUD Subdivision and PUD Site Plan submittals within the Woodmont Commons PUD shall be administered by the Town of Londonderry in accordance with the PUD Master Plan and the applicable portions of the *Londonderry Zoning Ordinance, Subdivision Regulations and Site Plan Regulations*. In addition, the following provisions shall apply:
 1. All projects shall be subject to review and acceptance by the Developer prior to submittal to the Town of Londonderry. This acceptance is a pre-requisite for review of any submittal by the Planning Department and all submittals shall be accompanied by written acceptance from the Developer;
 2. All approved projects shall count against the total amount of authorized development for the Woodmont Commons PUD. The Developer shall track and record the amount of development completed to date, the amount of development under review or construction, the amount of development previously approved but not under construction, and the proposed amount of development in the current application as compared to the authorized development amounts for each Subarea and for the total development as listed in the Allowable Densities Table in *Section 2.2 Land Use Regulations*. Said accounting shall be reported to the Planning Department upon request, but at least annually.
- C. PUD Subdivision Application Procedures and Approval Process:
 1. An applicant preparing to create a PUD Subdivision shall follow the process outlined in the *Londonderry Subdivision Regulations* except that the Initial Inquiry shall be mandatory;
 2. The Developer shall review and accept the application prior to the Formal Application Submission and submit an acceptance letter to the Planning Department.
- D. PUD Site Plan Application Procedures and Approval Process
 1. An applicant preparing to create a PUD Site Plan shall follow the process outlined in the *Londonderry Site Plan Regulations* except that the Initial Inquiry shall be mandatory;
 2. The Developer shall review and accept the application prior to the Formal Application Submission and submit an acceptance letter to the Planning Department.
- E. Return of Development Capacity
 1. Once approval has been granted, a developer may disclaim its permit and return the authorized development capacity to the Woodmont Commons PUD.

2.5.2 Modifications and Amendments to the PUD Master Plan

A “minor modification and amendment” to the PUD Master Plan may, upon the request of the Developer, be approved by the Planning Board without following the administrative process set forth in *Section 2.8.3* of the *Londonderry Zoning Ordinance*. Instead, any proposed “minor modification and amendment” shall be reviewed by the Planning Board for good cause shown and consistency with the spirit and intent of the PUD Master Plan and the *Londonderry Zoning Ordinance*.

A “minor modification or amendment” shall mean any and all proposed changes to the PUD Master Plan that does NOT:

1. Add or delete more than 10 acres of real property to the PUD, provided any such additional real property is contiguous to the PUD and is to be regulated by the same PUD Regulations and Standards applicable to the immediately adjacent and contiguous subarea;
2. Alter the “Allowable Uses Table,” or the uses designated as allowable for each Subarea, stated in *Section 2.2.3 Land Use Standards*;
3. Alter the “Land Use Density Table” stated in *Section 2.2.3 Land Use Standards*;
4. Alter the “Waivers/Modifications/Compliance with Specified Development Standards” stated in *Section 2.1.4 Applicability*;
5. Alter the “Block Types – Size and Dimension” criterion stated in *Section 2.3.2 Block Types*; or,
6. Alter the “Building Types – Building Height, Primary Building Height (maximum)” criterion stated in *Section 2.4.2 Building and Lots*.

Proposed amendments to the PUD Master Plan that do not qualify as “Minor Modifications and Amendments,” as the same are defined above, shall be considered in accordance with *Section 2.8.3.9* of the *Londonderry Zoning Ordinance*. Specifically, the Developer “may apply to amend all or a portion of an approved PUD following the same process applicable to the establishment of a PUD,” including compliance with *Section 2.8.3* of the *Londonderry Zoning Ordinance*.

2.6 FORMS AND RECORDS

There are two required forms for this PUD Master Plan – a submittal form in the form of an Acceptance Letter and a Tracking Form that is used to track all projects, their status relative to submittal, approval and rescission and the amount of development capacity used.

Once approval has been granted, a developer may disclaim its permit and return the approved development capacity to the Woodmont Commons PUD. Such rescission will be tracked using the tracking form and reported to the Town on an annual basis as described below.

An example of each form can be found in *Section 4.7 Sample Forms*.

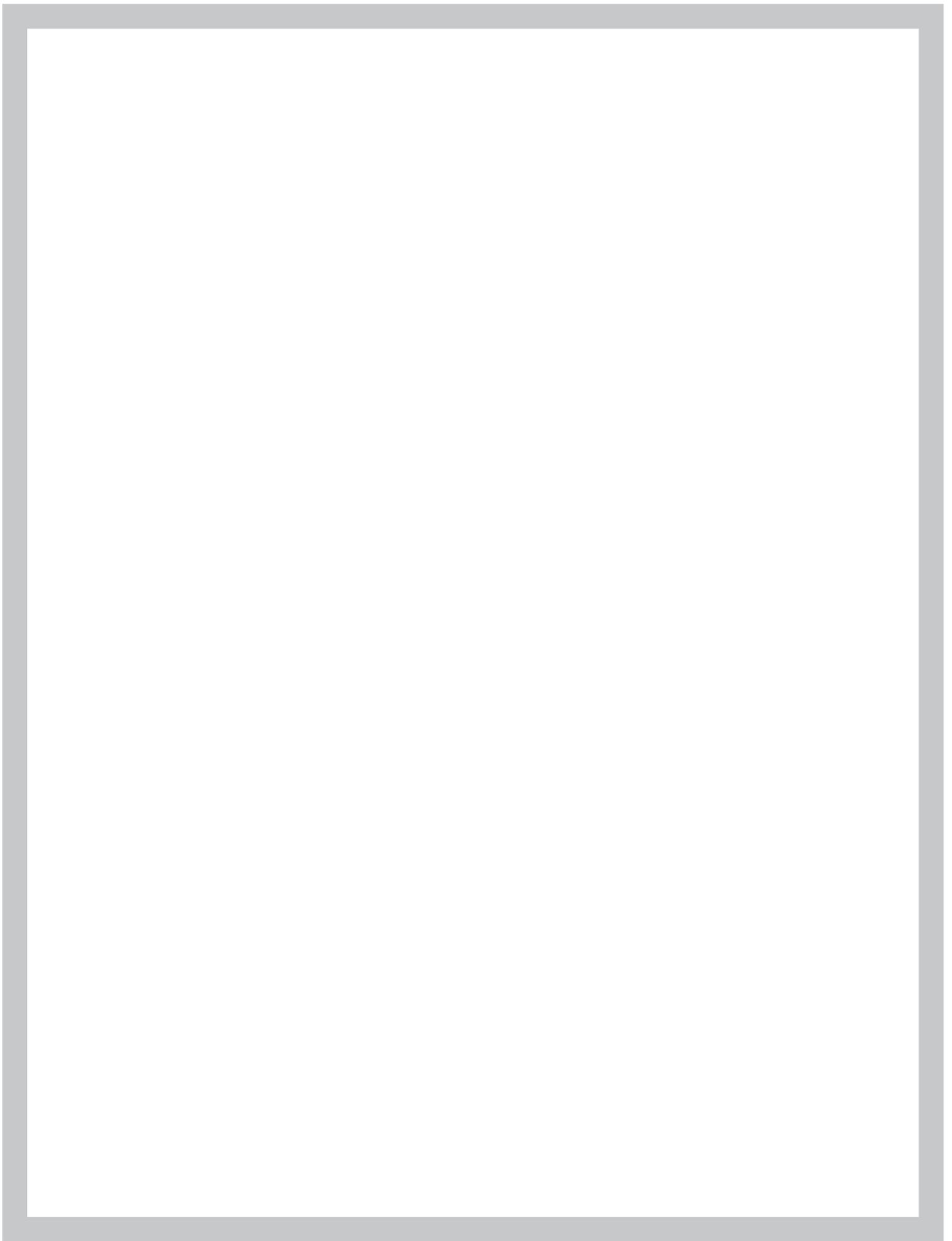
The Tracking Form shall also be provided to Town Staff on an annual basis confirming the amount of development maximums that has been used to date and any changes over the past year.

2.6.1 Submittal Form

An Acceptance Letter from the Developer must accompany any submittal for PUD Subdivision or PUD Site Plan review. The form must be signed by the Developer, confirming compliance with the PUD Master Plan prior to submittal to the Town. An example of this form can be found in *Section 4.7.1 Sample Submittal Form*. This form may be modified with notification to the Planning Board and Town of Londonderry staff.

2.6.2 Tracking Forms

A Tracking Form must be submitted with each application for PUD Site Plan review. This form must be signed by the Developer and will confirm that the project is consistent with the allowable densities and Open Space minimums under *Section 2.2 Land Use Regulations*. An example of this form can be found in *Section 4.7.2 Sample Tracking Form*. This form may be modified with notification to the Planning Board and Town of Londonderry staff.





3.0 Mitigation and Improvement Requirements

WOODMONT COMMONS

PLANNED UNIT DEVELOPMENT MASTER PLAN

SEPTEMBER 2013

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3.1 TRANSPORTATION

The following measures may be considered by an applicant for PUD Subdivision and/or Site Plan approval and the Planning Board in order to mitigate the impacts of new vehicle trips to the surrounding roadway network within the study area:

- New roadway connections within the PUD to distribute traffic to locations with a higher level of reserve capacity;
- Roadway widening to add through lanes or auxiliary turn lanes accessing side streets or major access driveways;
- Installation of, or enhancements to, traffic signals to improve delays for side street movements or improve the flow of traffic between adjacent traffic signals;
- Construction of sidewalk or bicycle facilities;
- Traffic calming measures to reduce the speed of traffic in particular areas or influence motorists to use other more appropriate facilities;
- Installation of signs and pavement markings to enhance pedestrian and bicycle safety;
- Installation of lane use, directional, and way-finding signs to better define traffic operations or improve motorist identification of major destinations within the PUD, such as a parking facility;
- Site enhancements to accommodate future transit service, if and when available, and pedestrian and bicycle connections to the existing Park-n-Ride facility.

The conceptual improvement graphics provided within the PUD Master Plan and its attachments may be used as a guide for future consideration, but does not prohibit the introduction of new or refined concepts that provide a comparable mitigated condition.

3.1.1 Thresholds for Physical Mitigation Measures

Alterations may be warranted at an intersection to mitigate the impacts of the project when one or more of the following criteria are satisfied under Build conditions:

- The overall level-of-service (LOS) for the intersection is degraded by one letter grade with an average intersection delay increase of at least 15 seconds as a result of the development phase(s) (Note: a degradation of a highly efficient operation to LOS C (or better) shall not require mitigation);
- The overall LOS for a signalized intersection is materially degraded to LOS E or F;
- The LOS of an intersection within the PUD boundary is degraded by at least two letter grades (Note: Within the limits of the PUD, there may be intersection operations that are purposefully planned to operate at LOS F in order to control cut-through or speeding potential. A comfortable level of congestion is appropriate for a dense mixed-use environment.)
- The volume-to-capacity (v/c) ratio for the overall intersection exceeds 1.00;
- The LOS of any major movement through the intersection reaches LOS F or the v/c ratio for any major movement exceeds 1.00 and the resultant queues are expected to impact an upstream intersection;
- The volume of traffic on that movement exceeds the thresholds for mitigation, such as through installation of a traffic signal or construction of turning lanes (Note: high delays for an unsignalized left-turn from a side street shall not require any mitigation if the volumes are not sufficient to warrant a specific traffic control change.)

3.1.1 Thresholds for Physical Mitigation Measures

The need for improvements at a particular location assumes that the traffic volumes at that location exceed thresholds to satisfy the warranting conditions of a specific mitigation measure, such as the installation of a traffic signal or roadway widening to provide additional lanes. Furthermore, the need for mitigation measures is dependent upon the physical constraints of the roadway or intersection, including, but not limited to, right-of-way limits and proximity to wetlands or historic resources.

If the applicant implements physical mitigation measures that are projected to mitigate the impacts of the project at an intersection (overall LOS and volume-to-capacity ratios), as identified for the entire PUD in the MTIA (or as a similarly scaled improvement), then the applicant shall not be required to perform supplemental improvements to the intersection. In this manner, the funds expended by the applicant to build intersection capacity as mitigation for the PUD will not be eroded by unanticipated background traffic growth.

In the event that an applicant builds a portion of the master planned traffic improvements during an introductory phase of the PUD, the Town shall grant a proportional level of trip generation for the PUD until such time as the balance of the improvements are constructed. As some of the improvements are located within State Highway right-of-way, the New Hampshire Department of Transportation will ultimately make the determination of the adequacy of mitigation within their jurisdiction.

In the updated traffic studies that will be performed as part of the PUD Subdivision and/or PUD Site Plan review process, the applicant shall present a construction access and truck routing scheme that minimizes disruption to the surrounding public roadway network.

3.2 UTILITIES

3.2.1 Mitigation Methods

The following utility improvement measures may be considered by the applicant for PUD Subdivision and/or Site Plan approval and the Planning Board in order to mitigate the impacts of additional water use or wastewater generation to the existing systems:

- Upgrading water lines for increased flow to the PUD;
- Upgrading water pump stations servicing the PUD or the adjacent area;
- Installation of water storage facilities for reserve water supply and improved water pressures;
- Upgrading sanitary sewer facilities to accommodate the additional wastewater flow through the PUD or to the adjacent system. The improvements will be limited to the lines between the various portions of the PUD and the Derry Wastewater Treatment Facility;
- Replacing or upgrading sewer pump stations to accommodate the design flows for each PUD Subdivision or to improve the efficiency of operations for existing flows;
- Introducing or improving flow monitoring devices in the existing system.

The specific methods required for each PUD Subdivision shall be determined in consultation with Town staff, the water supply licensee and other private utility companies.

3.2.2 Thresholds

The threshold for improvements is highly dependent upon the specific uses proposed in each phase of development as reflected in a PUD Subdivision and/or PUD Site Plan review. The applicant shall identify specific flows, conveyance paths and required mitigation in an infrastructure facilities report provided at the time of each major PUD Subdivision. The applicant will review the proposed improvements and any associated reserve capacity for the PUD with Town Engineering and DPW staff prior to submission to the Planning Board.

3.3 CHLORIDE MANAGEMENT

Beaver Brook has been declared an impacted watershed in need of corrective action due to Chloride loading, most notably resulting from winter maintenance activities. Londonderry has a Salt Reduction Plan (SRP) that provides a procedural framework for the Town of Londonderry to continuously strive to improve winter maintenance operations while effectively and efficiently using road salt during the snow and ice removal operations. Londonderry will provide winter maintenance to town roadways, parking lots and sidewalks in accordance with Londonderry's Winter Maintenance Snow and Ice Control Policy while striving to minimize adverse impacts to the environment by:

- Adhering to the procedures contained within the SRP;
- Committing to ongoing winter maintenance staff training and education;
- Reporting fiscal year salt usage to New Hampshire Department of Environmental Services (NHDES);
- Re-evaluating the effectiveness of the SRP as needed to incorporate new technologies or changes in procedures.

Under the existing SRP, the Town has identified four strategies to help meet the Total Maximum Demand Load allocation within the Beaver Brook Watershed. The current load is estimated at 854.47 tons/year imported to the watershed. The town's reduction goals are 6% for year one, 12% for year two, and 18% for year three to be achieved from the following strategies.

- Equipment upgrade pilot project;
- Improved calibration;
- Private sector outreach;
- Improved weather system monitoring.

NHDES will monitor the progress made by each community impacting the Beaver Brook watershed and may require modifications to each plan.

In recognition of the Beaver Brook Watershed chloride reduction plan for Londonderry, and that approximately 40% of the chloride impact is a result of winter operations on private roadways and parking lots, the Woodmont Commons PUD will be guided by the following actions:

- Private Maintenance Contracting Equipment Upgrades and Training – the Woodmont Commons PUD will serve as a model for private sector participation by committing to contract with snow removal and maintenance providers who have been trained and are knowledgeable of Best Management Practices (BMP) for snow removal with reduced salt application. These practices are published and are updated through the UNH – Technology Transfer (T2) program. All private contractors employed by Woodmont Commons PUD roadway maintenance will use only pre-approved methods for spreading abrasives on private roadways and parking lots. When salt is applied, it will be applied only in accordance with best management practices and will include pre-treatment and ground speed controlled spreaders as described in the NHDES *August 2011 Salt Reduction Implementation Plan for Beaver Brook Watershed*;
- Improved Weather System Monitoring – the Woodmont Commons PUD will coordinate weather information gathered by both the town and NHDOT for use by maintenance contractors. In doing so, contractors will be able to make more informed decisions as to when and to what extent chloride is applied to roadways, sidewalks and parking lots;
- Increased Mechanical Removal Capabilities – Woodmont Commons PUD will endeavor to use mechanical removal means on a more frequent basis using more manpower and equipment than traditionally used to shorten plowing intervals and reduce the need for salting to break up hard packed snow. Shortened maintenance routes with shorter service inter-

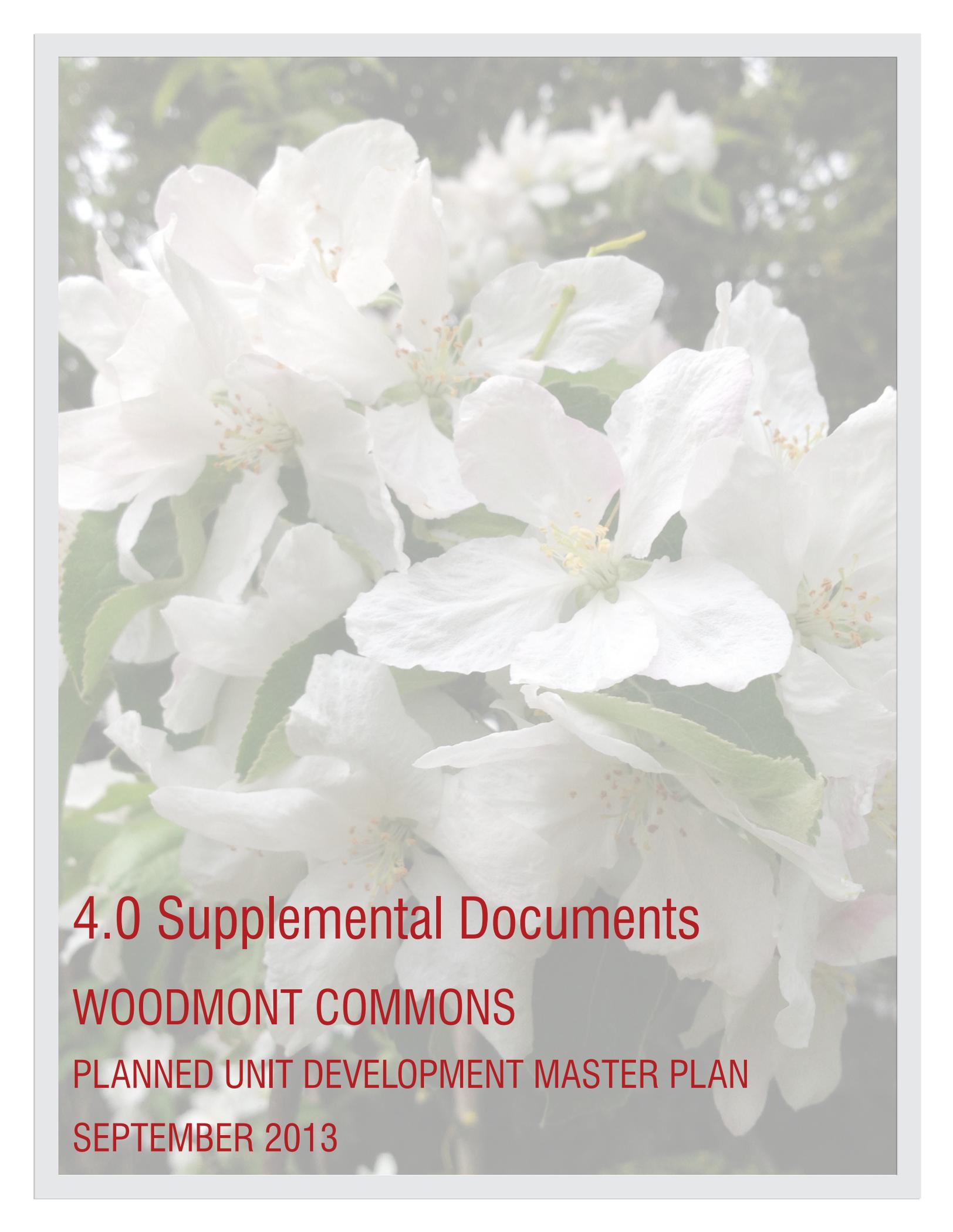
vals will be used to stay ahead of snowfall, thus reducing the need to sand and salt after snow and ice packing has already occurred;

- Public Awareness Campaign – Woodmont Commons PUD will inform all future developers, grantees, and tenants of the need to reduce salt application on roadways, parking lots and sidewalks.

The above described methods will be published in an operations manual with sample specifications used to retain private road snow removal contractors. The manual will serve as a model framework that could be adopted by other private developments for their roadways and parking lots as part of an overall outreach program. The manual, as with the Town's Salt Reduction Plan, is intended to be a dynamic document modified as required based on knowledge gained from past practices and technological advancement to reflect chloride best management practices.

3.4 RELATIONSHIP TO THE DEVELOPMENT AGREEMENT

For a discussion of relative financial responsibilities for mitigation measures pertaining to roadway and transportation improvements, wastewater collection and treatment facilities, potable water distribution lines and facilities, and recreational and community facilities, please refer to Sections VI and VII of the Woodmont Commons Development Agreement. A copy of this document is provided in *Section 4.5 Development Agreement*.



4.0 Supplemental Documents

WOODMONT COMMONS

PLANNED UNIT DEVELOPMENT MASTER PLAN

SEPTEMBER 2013

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4.1 UPDATED ABUTTERS LIST

The following pages contain the abutters list as of December 31, 2012.

Parcel ID	Owner(1)	Owner Address	City	State	Zip
007 065 1	KELLEY ROBERT E + JEAN C	87 GILCREAST RD	LONDONDERRY	NH	03053
007 065 2	KELLEY ROBERT E + JEAN C	87 GILCREAST RD	LONDONDERRY	NH	03053
007 065 3	SMITH KELLEY RAE REVOC TRUST	PO BOX 1105	LONDONDERRY	NH	03053
007 067 0	LEEMEN CORPORATION	220 N MAIN ST	NATICK	MA	01760
007 068 0	CAROLINA REALTY PARTNERS INC	625 AMHERST ST	NASHUA	NH	03063
007 068 1	NH SIX REALTY TRUST C/O HAJJAR MGMT CO, INC. CHARLES C. HAJJAR, TRUSTEE	30 ADAMS ST	MILTON	MA	02186
009 064 6	KIM JONG S + YOUN MI	62 PILLSBURY RD	LONDONDERRY	NH	03053
010 001 0	NEW HARBOR PROPERTIES INC.	1F COMMONS DR SUITE 39	LONDONDERRY	NH	03053
010 001 1	GILCREAST ROAD LLC	P.O. BOX 1330	LONDONDERRY	NH	03053
010 001C 0	LONDONDERRY COMMONS	PO BOX 465	LONDONDERRY	NH	03053
010 001C 14	CALVIN RICHARD A REVOC TRUST	6 MOUNTAIN FARM RD	BOW	NH	03304
010 001C 15	Anschi, LLC	1C Commons Drive, Suite 16	LONDONDERRY	NH	03053
010 001C 16	Anschi, LLC	1C Commons Drive, Suite 16	LONDONDERRY	NH	03053
010 001C 17	1C COMMONS DR UNIT 17 LLC	1C COMMONS DR UNIT 17	LONDONDERRY	NH	03053
010 001C 18	JTLB LLC	1C COMMONS DR UNIT 18	LONDONDERRY	NH	03053
010 001C 19	LOT 11-C-700 LLC	PO BOX 439	LONDONDERRY	NH	03053
010 001C 20	LOT 11-C-700 LLC	PO BOX 439	LONDONDERRY	NH	03053
010 001C 21	LOT 11-C-700 LLC	PO BOX 439	LONDONDERRY	NH	03053
010 001C 22	LOT 11-C-700 LLC	PO BOX 439	LONDONDERRY	NH	03053
010 001C 23	LOT 11-C-700 LLC	PO BOX 439	LONDONDERRY	NH	03053
010 001L 0	LC LONDONDERRY LLC	1E COMMONS DR UNIT 30	LONDONDERRY	NH	03053
010 001 2	NEW HARBOR PROPERTIES INC.	1F COMMONS DR SUITE 39	LONDONDERRY	NH	03053
010 002 0	VACCO RICHARD P + MARJEANNE B	78 GILCREAST RD	LONDONDERRY	NH	03053
010 003 0	HAUSSLER JOHN A & MARY	PO BOX 912	LONDONDERRY	NH	03053-0912
010 005 0	TRAVASSOS BRANDON A	91 GILCREAST RD	LONDONDERRY	NH	03053
010 006 0	HOGAN MATTHEW A & MARCELA SMITH-HOGAN	93 GILCREAST RD	LONDONDERRY	NH	03053
010 007 0	DAHL EDWARD F	95 GILCREAST RD	LONDONDERRY	NH	03053
010 008 0	MACASKILL WAYNE + DARLENE	97 GILCREAST RD	LONDONDERRY	NH	03053
010 009 0	KUKOWSKI JOHN E + CAROL A	99 GILCREAST RD	LONDONDERRY	NH	03053
010 010 0	EMERY EARL C + ALBERTA M	101 GILCREAST RD	LONDONDERRY	NH	03053
010 011 0	FOLEY RONALD T + EMILY C	103 GILCREAST RD	LONDONDERRY	NH	03053
010 012 0	WOODMONT ORCHARDS INCORPORATED	80 NASHUA RD	LONDONDERRY	NH	03053
010 013 0	SUGAR PLUM HILL LLC	100 ANDOVER BY-PASS STE 300	NORTH ANDOVER	MA	01845
010 013C 1A	ELLIOT MARK J	107A GILCREAST RD	LONDONDERRY	NH	03053
010 013C 1B	HEGARTY FRANK G + BERNADETTE	107B GILCREAST RD	LONDONDERRY	NH	03053
010 014 10	AUGERI ROBERT + KIMBERLY	49 PILLSBURY RD	LONDONDERRY	NH	03053
010 014 6	INSERRA JOYCE I & NANCY SALVUCCI	57 PILLSBURY RD	LONDONDERRY	NH	03053
010 014 7	DALPHOND KATHLEEN M	55 PILLSBURY RD	LONDONDERRY	NH	03053

Parcel ID	Owner(1)	Owner Address	City	State	Zip
010 014 8	DALRYMPLE PHILLIP + PRISCILLA	PO BOX 1045	LONDONDERRY	NH	03053
010 014 9	SMITH CRAIG C + DORIS	51 PILLSBURY RD	LONDONDERRY	NH	03053
010 014A 0	NEWELL JEFFREY D	104 GILCREAST RD	LONDONDERRY	NH	03053
010 016 10	OBRIEN DAVID W & LORRAINE FAMILY TRUST	4 GARY DR	LONDONDERRY	NH	03053
010 016 11	HICKS NATHANIEL E + JULIE L	6 GARY DR	LONDONDERRY	NH	03053
010 016 12	HAUSER ALLEN E + PATRICIA E	PO BOX 208	LONDONDERRY	NH	03053
010 016 13	TYRRELL JUDITH S	10 GARY DR	LONDONDERRY	NH	03053
010 016 14	HOULE MARGO S	12 GARY DR	LONDONDERRY	NH	03053
010 016 15	JOHNSON FREDERICK C & DIANE L.	14 GARY DR	LONDONDERRY	NH	03053
010 016 16	KEVIN BAILEY	16 GARY DR	LONDONDERRY	NH	03053
010 016 17	BRATHWAITE DEXTER P + BONNIE A	18 KITT LN	LONDONDERRY	NH	03053
010 016 18	SCHMIDT ALAN D + DONNA M	20 KITT LN	LONDONDERRY	NH	03053
010 016 19	CARTER ROBERT G + DEBORAH A	22 KITT LANE	LONDONDERRY	NH	03053
010 016 20	TISHLER DONALD + LINDA & TISHLER, LINDSEY	24 KITT LN	LONDONDERRY	NH	03053
010 016 21	NORTON ROBERT H + ALICE B	26 KITT LN	LONDONDERRY	NH	03053
010 016 22	RICE DAVID W JR & JEAN	28 KITT LN	LONDONDERRY	NH	03053
010 016 23	GALLO PETER F	30 KITT LN	LONDONDERRY	NH	03053
010 016 24	JEFFREY M. BARNARD	32 KITT LN	LONDONDERRY	NH	03053
010 016 25	AUGERI MATTHEW A + LEANNE	34 KITT LN	LONDONDERRY	NH	03053
010 016A 0	SHATTO JACQUES M	2 HARDY RD	LONDONDERRY	NH	03053
010 018 0	CTI TOWERS, INC.	38 POND ST, SUITE 305	FRANKLIN	MA	02038
010 018 1	BEAULIEU JOSEPH & SUSAN FRANZ	51 HOVEY RD	LONDONDERRY	NH	03053
010 018 2	MAUCERI DAVID A + JULIANNE M	1 DRAGONFLY WAY	LONDONDERRY	NH	03053
010 019 0	BOLES, CURTIS M. & LINDA E. 2011 REVOCABLE TRUSTS	57 HOVEY RD	LONDONDERRY	NH	03053
010 023A 0	CHAPDELAIN KEVIN M	43 TROLLEY CAR LANE	LONDONDERRY	NH	03053
010 023A A	DERRY TOWN OF, C/O TOWN ADMINISTRATOR	14 MANNING ST	DERRY	NH	03038
010 028 0	D'URSO, LORRENE & ERIN	42 TROLLEY CAR LN	LONDONDERRY	NH	03053
010 029C 1A	CARTER CHARLES + LINDA	34A TROLLEY CAR LN	LONDONDERRY	NH	03053
010 029C 1B	COSTA LISETTE D	34B TROLLEY CAR LN	LONDONDERRY	NH	03053
010 029C A	BROWN ROCHELLE	36A TROLLEY CAR LN	LONDONDERRY	NH	03053
010 029C B	LANGLEY, JAMES E	36B TROLLEY CAR LN	LONDONDERRY	NH	03053
010 030 0	DOUCETTE DENNIS + CAROLYN L	28 TROLLEY CAR LN	LONDONDERRY	NH	03053
010 030 3	DICKENSON DONNA E	24 TROLLEY CAR LN	LONDONDERRY	NH	03053
010 033	OLIVERIO, CHRIS M. & RHINE, TRACY A.	1 TROLLEY CAR LN	LONDONDERRY	NH	03053
010 034	TOWN OF LONDONDERRY	268B MAMMOTH RD	LONDONDERRY	NH	03053
010 037C 7A	15A TROLLEY CAR LLC	4 PARK DRIVE	BEDFORD	NH	03310
010 037C 7B	SALVAGE JACQUELINE L & JAMES M. HOWE	15 B TROLLEY CAR LN	LONDONDERRY	NH	03053
010 038 0	DANIELS JOSEPH E	PO BOX 342	LONDONDERRY	NH	03053
010 038 2	MILLER DORIS J	16 HOVEY RD	LONDONDERRY	NH	03053
010 038 3	ROY GAIL M REVOCABLE TRUST	32 HOVEY RD	LONDONDERRY	NH	03053
010 038 4	BOLES JEFFREY M + RACHEL B	28 HOVEY ROAD	LONDONDERRY	NH	03053
010 038 5	STODDARD JOHN	26 HOVEY RD	LONDONDERRY	NH	03053
010 039 0	TROMBLEY EDWARD J & TIMOTHY TROMBLEY	24 HOVEY RD	LONDONDERRY	NH	03053
010 039 1	GOODWIN JASON M	20 HOVEY RD	LONDONDERRY	NH	03053

Parcel ID	Owner(1)	Owner Address	City	State	Zip
010 040 0	HALL CHESTER H JR	PO BOX 75	LONDONDERRY	NH	03053
010 040 1	IRONWOOD REAL EST HOLDINGS LLC	57 NOYES RD	LONDONDERRY	NH	03053
010 042 1	RICHARD D. & DAWN N. SHEA	1 HOVEY ROAD	LONDONDERRY	NH	03053
010 042 5	HILLARD RICHARD J JR & LORI E. BELLINO	9 HOVEY RD	LONDONDERRY	NH	03053
010 042 6	ONYX PROPERTY DEVELOPMENT INC	ONE JEFFERSON DR	LONDONDERRY	NH	03053
010 042 7	ONYX PROPERTY DEVELOPMENT INC	ONE JEFFERSON DR	LONDONDERRY	NH	03053
010 043 0	VELIE JAMES D & KIMBERLY A.	38 PILLSBURY RD	LONDONDERRY	NH	03053
010 044 0	SARTORI RONALD F & NANCY H.	42 PILLSBURY RD	LONDONDERRY	NH	03053
010 045 1	COMES BRUCE R + CATHARINE	8 ASH ST	LONDONDERRY	NH	03053
010 045 2	RUSSELL GARY H + NANCY E	6 ASH ST	LONDONDERRY	NH	03053
010 045 3	KURTIS R. TANGUAY	4 ASH ST	LONDONDERRY	NH	03053
010 045 4	DANAHY DANIEL A	2 ASH ST	LONDONDERRY	NH	03053
010 049 0	SEVEN HILLS DEVELOPMENT LLC	PO BOX 1220	DERRY	NH	03038
010 053 0	NEW HAMPSHIRE STATE OF, DEPT OF PW & HWYS	85 LOUDON RD	CONCORD	NH	03301
010 054 0	NEW ENGLAND INDUST PROP INC	12 LEXINGTON ST	LEWISTON	ME	04240
010 055 0	KC PROPCO LLC C/O KNOWLEDGE LEARNING CORP	PO BOX 6760	PORTLAND	OR	97228
010 056 0	NEW HAMPSHIRE STATE OF, DEPT OF PW & HWYS	7 HAZEN DRIVE	CONCORD	NH	03301
010 060 0	SILVA LORI ANN	2 BUYCK AV	LONDONDERRY	NH	03053
010 061 0	THERIEN ANDREW J & VALLEE, MICHELLE M	PO BOX 633	LONDONDERRY		03053
010 063 0	MORRISSETTE HAROLD J JR	20 ASH ST	LONDONDERRY	NH	03053
010 064	DONAGHEY, WILLIAM & CANDY	18 ASH ST	LONDONDERRY	NH	03053
010 065 0	ROSA JOSEPH A + MARION M	16 ASH ST	LONDONDERRY	NH	03053
010 066 0	TILLEY DANIEL K	14 ASH ST	LONDONDERRY	NH	03053
010 067 0	MACK DANIEL + CHARLENE	12 ASH ST	LONDONDERRY	NH	03053
010 068 0	WANAT LARRY D + SHEILA M	10 ASH ST	LONDONDERRY	NH	03053
010 072 0	INNIE IRENE P FAMILY TRUST	3 ASH ST	LONDONDERRY	NH	03053
010 083 0	PINKNEY DEBBIE	21 ASH ST	LONDONDERRY	NH	03053
010 084 0	NAULT DENISE M	23 ASH ST	LONDONDERRY	NH	03053
010 085 0	27 ASH ST LLC	27 ASH ST	LONDONDERRY	NH	03053
010 086 0	NEW HAMPSHIRE STATE OF	PO BOX 483, 1 HAZEN DR	CONCORD	NH	03301
010 087 0	£33 LONDONDERRY ROAD LLC C/O LOUISE VON DAMM	64-17 28TH AVE	ASTORIA	NY	11103
010 138 0	CRACKER BARREL COUNTRY STORE LLC	6920 POINTE INVERNESS	FT. WAYNE	IN	46804
010 142 0	PENNICHUCK EAST UTILITY INC	25 MANCHESTER ST PO BOX 1947	MERRIMACK	NH	03054
010 148 0	LIONEL R. LABONTE	335 RTE 125	BRENTWOOD	NH	03833
010 148 1	LIONEL R. LABONTE & GRETA A. LABONTE	335 RTE 125	BRENTWOOD	NH	03833
010 150 0	STATE OF NEW HAMPSHIRE	P.O. BOX 483 7 HAZEN DR	CONCORD	NH	03302
010 151 0	NEW HAMPSHIRE STATE OF, DEPT OF PW & HWYS	85 LOUDON RD	CONCORD	NH	03301
010 154 0	LONDONDERRY TOWN OF	268B MAMMOTH RD	LONDONDERRY	NH	03053
013 001 11	FERREIRA JOHN L JR & BARBARA A.	58 HOVEY RD	LONDONDERRY	NH	03053
013 001 12	DUMONT RONALD A + CRISTETA A	47 TROLLEY CAR LANE	LONDONDERRY	NH	3053
013 001 14	LONDONDERRY TOWN OF	268B MAMMOTH RD	LONDONDERRY	NH	03053
013 088 1	DORAIAKANNAN KUMARAVELAN M	3 COTE LN	LONDONDERRY	NH	03053
013 088 2	ROSSI DAVID + MICHELLE	18 SNOWCAP WAY	LONDONDERRY	NH	03053

Parcel ID	Owner(1)	Owner Address	City	State	Zip
013 088 4	MANDRIOLI JOHN SCOTT	60 SEASONS LN	LONDONDERRY	NH	03053
013 088 5	TONKIN-BEATON ELIZABETH T & SCOTT BEATON	62 SEASONS LN	LONDONDERRY	NH	03053
013 088 6	JAMES W. & CYNTHIA G. ALLEY	64 SEASONS LN	LONDONDERRY	NH	03053
013 088 7	WALL GARY & CONNIE	66 SEASONS LN	LONDONDERRY	NH	03053
013 088 8	ROTHERMUND VICTORIA L + MARC	58 BOSTON ST	MIDDLETON	MA	01949
013 090 0	DEMERS MARCEL E + RITA A	30 COTEVILLE RD	LONDONDERRY	NH	03053
013 094 0	PECK JAMES D JR & PECK, TIMOTHY	40 COTEVILLE RD	LONDONDERRY	NH	03053
013 095 0	SULLIVAN KIM J + PATRICK F	37 COTEVILLE RD	LONDONDERRY	NH	03053
013 095A 0	HYRAX DERRY PARTNERS LLC C/O RICH MAY	176 FEDERAL ST	BOSTON	MA	02210
DERRY:					
	31011 SEVEN HILLS DEVELOPMENT LLC	PO BOX 1220	DERRY	NH	03038
	31011 SEVEN HILLS DEVELOPMENT LLC	PO BOX 1220	DERRY	NH	03038
	35003 HYRAX DERRY PARTNERS LLC, C/O RICH MAY	176 FEDERAL ST	BOSTON	MA	02210
	35101 HYRAX DERRY PARTNERS LLC, C/O RICH MAY	176 FEDERAL ST	BOSTON	MA	02210
31005-006	DERRY BUSINESS CENTER LLC	11 CORPORATE DRIVE	BELMONT	NH	03220
31005-007	PEARL E. SCIUTO	5 TSIENNETO RD U30	DERRY	NH	03038
31005-023	GLENN D. & ROSALIND WANTE	19 ASH ST	DERRY	NH	03038

4.2 MASTER PLAN TRAFFIC IMPACT ASSESSMENT

4.3 INFRASTRUCTURE MEMORANDUM

4.4 MASTER FISCAL IMPACT ANALYSIS

These three sets of documents are provided as separately bound reports.

Section 4.2 Master Plan Traffic Impact Assessment includes the *Master Plan Traffic Impact Assessment*, dated February 6, 2013 and the *Exit 4A Sensitivity Analysis* dated April 10, 2013, both by TEC.

Section 4.3 Infrastructure Memorandum includes the *Utility Memo*, dated May 15, 2013, by CMA.

Section 4.4 Master Fiscal Impact Analysis contains the *Woodmont Commons Fiscal Impact Analysis*, dated May 17, 2013 and the *Technical Memorandum, Woodmont Commons Fiscal Impact, Londonderry School District*, dated May 20, 2013, both by Development Planning & Financing Group, Inc.

All three sections are to be used as described in *Section 2.0 PUD Regulations and Standards* as baseline reports for comparison to later supplemental reports that may be provided with PUD Subdivision and/or PUD Site Plan submittals.

4.5 DEVELOPMENT AGREEMENT

The Development Agreement is provided as a separately bound report.

4.6 SAMPLE RESTRICTIVE COVENANT

This section includes a form of restrictive covenant to be executed and recorded by each Land Owner.

Return to: Town of Londonderry Planning Department
268B Mammoth Road
Londonderry, NH 03053

RESTRICTIVE COVENANT

[GRANTOR], a New Hampshire [Entity] having offices at [Address] (“Covenantor”), for consideration received and as a condition of approval for the Woodmont Commons Master Plan approved by the Planning Board of the Town of Londonderry, New Hampshire on *Month Day, 2013* for a Planned Unit Development known as Woodmont Commons (“Approval”), hereby enters into this Restrictive Covenant relating to said real property in Londonderry, New Hampshire and further identified as Town of Londonderry Parcel Identification Nos. _____ (“the Restricted Properties”).

As a condition of the Approval, Covenantor irrevocably agrees that the development, redevelopment, operation and maintenance of the Restricted Properties shall now and forever conform to the Woodmont Commons Master Plan, as the same may be amended by the Town of Londonderry Planning Board from time to time.

The covenants, conditions, reservations and restrictions of this Restrictive Covenant shall run with and bind the Restricted Properties in perpetuity and shall inure to the benefit of the Town of Londonderry, New Hampshire.

Prior to the Covenantor’s conveyance of any property within Woodmont Commons, each deed conveying land within Woodmont Commons recorded subsequent to the recordation of this Declaration shall state that the respective conveyance is subject to this Declaration, but that the failure to include such statement, or to include a sufficiently suitable statement, in a deed, shall not affect the enforceability of the Declarations with respect to the property conveyed or invalidate the respective conveyance. Further, no projects, rezoning petition, Master Plan adjustment, variation, alteration or amendment, application for variance, special exception or conditional use approval shall be submitted to the Town, or its land use boards, or proposed in any manner, without the written consent of the Master Developer, which shall not be unreasonably withheld.

Use and development of the Restricted Properties is subject to regulations applicable to Woodmont Commons as stated in the Master Plan, a copy of which is available for inspection at the Planning

Department of the Town of Londonderry (the "Planning Department) at the Londonderry Town Hall, 268 B Mammoth Road, Londonderry, New Hampshire 03053.

Enforcement of this Restrictive Covenant shall be by any appropriate proceeding in law or equity in any court or administrative tribunal having jurisdiction against any person, persons, firm, company, trust, corporation, or other entity violating or attempting to violate or circumvent any covenant, condition, reservation or restriction contained herein, either to restrain a violation or to recover damages, or both. Failure to enforce any covenant, condition, reservation or restriction contained herein shall in no event be deemed a waiver of the right to do so thereafter. In the event of litigation or administrative proceedings to enforce these covenants, conditions, reservations and restrictions, or to recover damages, the prevailing party shall be entitled to recover court costs and reasonable attorney's fees.

Meaning and intending to restrict real properties conveyed to Covenantor by deed of _____ dated _____, 20____, and recorded in the Rockingham County Registry of Deeds in Book _____, Page _____.

Executed this _____ day of _____, 2013.

[Land Owner]

BY _____, ITS _____

STATE OF NEW HAMPSHIRE
COUNTY OF _____

This instrument was acknowledged before me on _____ by _____ as a _____ of _____.

Notary Public

Print Name _____
My Commission Expires _____

4.7 SAMPLE FORMS

The following two forms are examples of the forms described in *Section 2.6 Forms and Records*.

4.7.1 Sample Submittal Form

Woodmont Commons PUD	
Subarea: _____	Project Name: _____
Project Type _____	Project ID: _____
PUD Subdivision <input type="checkbox"/>	Date Submitted: _____
PUD Site Plan <input type="checkbox"/>	Date Approved: _____

Town of Londonderry Planning Department
 268B Mammoth Road
 Londonderry, NH 03053

SUBMITTAL FORM

The Developer certifies that this *PUD Subdivision/PUD Site Plan/PUD Subdivision and PUD Site Plan* submittal is in conformance with the PUD Master Plan.

The initial inquiry was conducted on _____.
 (date)

The Developer accepted the formal application submission on _____.
 (date)

The impact of this project on the PUD and Subarea Maximum Development is as follows:

	Land Use Categories	Allocated Development for This Project	Total Maximum PUD Development	New Total Allocated To Date	Total Maximum Subarea Development	New Total Allocated to Date
	Agricultural		No limit			
Residential	New Primary Residence		1,300 units			
	New Accessory Units		130 units			
	Existing Units		9 units			
Institutional	Hospital		250,000 GSF or 300 beds			
	Nursing Homes and Assisted Living		250,000 GSF			
	Civic		No limit			
Accommodation	Hotels		3 hotels/550 rooms			
	PUD Bed and Breakfast		No limit			
Commercial Uses	New Commercial Uses		882,500 GSF			
	Office Building Space		700,000 GSF			
	Existing Commercial Uses		272,000 GSF			

4.7.1 Sample Submittal Form

Woodmont Commons PUD	
Subarea: _____	Project Name: _____
Project Type _____	Project ID: _____
PUD Subdivision <input type="checkbox"/>	Date Submitted: _____
PUD Site Plan <input type="checkbox"/>	Date Approved: _____

The impact of this project on the Open Space and Perimeter Buffer minimums is as follows:

Open Space and Buffer Categories	Allocated for This Project	Total Minimum PUD Development	New Total Allocated To Date	Total Minimum Subarea Development	New Total Allocated to Date
Conserved Green Space		89			
Shared Open Space		64			
Perimeter Buffer		38			

Notes:

Signed this _____ day of _____, 2013.

[Developer]

BY _____
 _____, ITS _____

4.7.2 Sample Tracking Form

Summary Page: Total PUD and Subarea Totals

Woodmont Commons

Sample Tracking Form: Summary Sheet

Last Updated:

Allowable Densities Table

LAND USE CATEGORIES		UNIT OF MEASURE	Total PUD Maximum Development	Total PUD Maximum Allocated To Date	Remaining Allocation Capacity	WC-1-GL
Total Land Area		Acres	-	-	-	-
Agricultural		All Types	N/A	No limit		
Residential	New Primary Residence	Units	1,300	-	1,300	-
	New Accessory Units	Units	130	-	130	-
	Existing Units	Units	9	-	9	-
	Maximum Total	Units	1,439	-	1,439	-
Institutional	Hospital	Beds/GSF	250,000	-	250,000	-
	Nursing Homes and Assisted Living	GSF	250,000	-	250,000	-
	Maximum Total	GSF	500,000	-	500,000	-
Civic		All Types	N/A	No limit		
Accommodations	Hotels (all types)	No./Rooms	3/550	-	550	-
	PUD Bed and Breakfast	Rooms	No limit	-		-
Commercial Use	New Commercial Uses	GSF	882,500	-	882,500	-
	Office Building Space	GSF	700,000	-	700,000	-
	Existing Commercial Uses	GSF	272,000	-	272,000	-
	Maximum Subtotal	GSF	1,854,500	-	1,854,500	-
Flexibility Factors						15%

OPEN SPACE AND BUFFER CATEGORIES		UNIT OF MEASURE	Total PUD Minimum Area	Total PUD Minimum Allocated To Date	Remaining Allocation Capacity	
Green and Open Space	Conserved Green Space	Acres	88.5	-	88.5	-
	Shared Open Space	Acres	63.5	-	63.5	-
	Total	Acres	152.0	-	152.0	-
Perimeter Buffers		Acres	38	38	-	-

1.6

4.7.2 Sample Tracking Form

Examples of Subarea Tracking Pages

Woodmont Commons

Sample Tracking Form: Subarea WC-1-GL

Last Updated:

Allowable Densities Table		UNIT OF	Total	Total	Difference	Project 1	Project 2	Project 3	Project 4	Project 5
LAND USE CATEGORIES		MEASURE	Subarea Maximum WC-1-GL	Projects to Date						
Total Land Area		Acres	38	-	38					
Agricultural		All Types	N/A							
Residential	New Primary Residence	Units	-	-	-					
	New Accessory Units	Units	-	-	-					
	Existing Units	Units	-	-	-					
	Maximum Total	Units	-	-	-	-	-	-	-	-
Institutional	Hospital	GSF	-	-	-					
	Nursing Homes and Assisted Living	GSF	-	-	-					
	Maximum Total	GSF	-	-	-	-	-	-	-	-
Civic		All Types	N/A							
Accommodations	Hotels (all types)	No./Rooms	-	-	-					
	PUD Bed and Breakfast	Rooms	-	-	-					
Commercial Use	New Commercial Uses	GSF	50,000	-	50,000					
	Office Building Space	GSF	-	-	-					
	Existing Commercial Uses	GSF	225,000	-	225,000					
	Maximum Subtotal	GSF	275,000	-	275,000	-	-	-	-	-
Flexibility Factors			15%							

OPEN SPACE AND BUFFER CATEGORIES		UNIT OF	Total	Total	Difference	Project 1	Project 2	Project 3	Project 4	Project 5
		MEASURE	Subarea Minimum	Projects to Date						
Green and Open Space	Conserved Green Space	Acres		-	-					
	Shared Open Space	Acres	1.5	-	2					
	Total	Acres	1.5	-	1.5					
Perimeter Buffers		Acres	-	-	-					

4.7.2 Sample Tracking Form

Woodmont Commons

Sample Tracking Form: Subarea WC-8

Last Updated:

Allowable Densities Table		UNIT OF MEASURE	Total Subarea	Total Projects to	Difference	Project 1	Project 2	Project 3	Project 4	Project 5
LAND USE CATEGORIES			WC-8	Date						
Total Land Area		Acres	70	-	70					
Agricultural		All Types	N/A							
Residential	New Primary Residence	Units	190	-	190					
	New Accessory Units	Units	40	-	40					
	Existing Units	Units		-	-					
	Maximum Total	Units	230	-	230	-	-	-	-	-
Institutional	Hospital	Beds/GSF		-	-					
	Nursing Homes and Assisted Living	GSF	20,000	-	20,000					
	Maximum Total	GSF		-	20,000	-	-	-	-	-
Civic		All Types	N/A							
Accommodations	Hotels (all types)	No./Rooms		-	-					
	PUD Bed and Breakfast	Rooms		-	-					
Commercial Use	New Commercial Uses	GSF	5,000	-	5,000					
	Office Building Space	GSF		-	-					
	Existing Commercial Uses	GSF		-	-					
	Maximum Subtotal	GSF	5,000	-	5,000	-	-	-	-	-
Flexibility Factors			15%							
OPEN SPACE AND BUFFER CATEGORIES		UNIT OF MEASURE	Total Subarea	Total Projects to	Difference	Project 1	Project 2	Project 3	Project 4	Project 5
			Minimum	Date						
Green and Open Space	Conserved Green Space	Acres	10.0	-	10					
	Shared Open Space	Acres	1.0	-	1					
	Total	Acres	11.0	-	11.0					
Perimeter Buffers		Acres	1.9	-	1.9					

- 4.8 LONDONDERRY ZONING ORDINANCE, AS AMENDED THROUGH DECEMBER 2011
- 4.9 SUBDIVISION REGULATIONS, JUNE 10, 2009
- 4.10 SITE PLAN REGULATIONS, JUNE 10, 2009
- 4.11 TYPICAL DETAILS FOR SITE AND ROADWAY INFRASTRUCTURE, MAY 2009

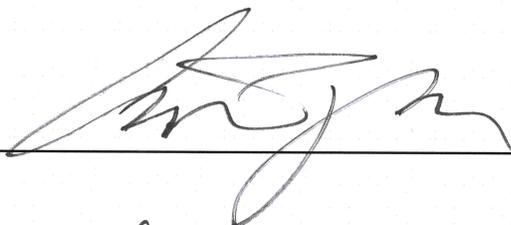
These four documents are provided as separately bound reports.

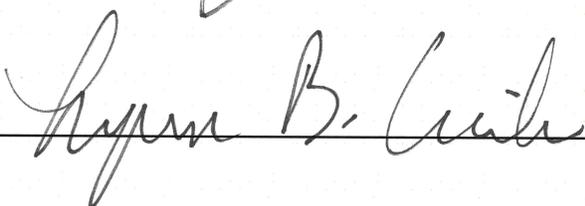
SIGNATURE PAGE

APPROVED BY THE LONDONDERRY, NH PLANNING BOARD

ON (DATE): October 2, 2013

CERTIFIED BY:

CHAIRMAN: 

SECRETARY: 

FOR PLANNING BOARD USE ONLY

